



CITY OF  
BAINBRIDGE ISLAND

PLANNING COMMISSION  
REGULAR MEETING  
THURSDAY, OCTOBER 27, 2016  
6:00 – 9:00 PM  
COUNCIL CHAMBER  
280 MADISON AVE N  
BAINBRIDGE ISLAND, WA 98110

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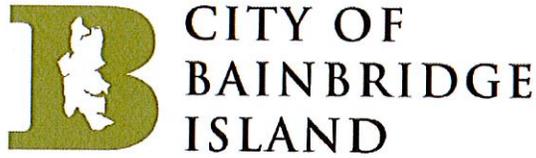
## AGENDA

- 6:00 PM**      **CALL TO ORDER**  
Call to Order, Agenda Review, Conflict Disclosure
- 6:05 PM**      **PUBLIC COMMENT**  
Accept public comment on off agenda items
- 6:10 PM**      **SITE PLAN & DESIGN REVIEW PERMIT:  
BI FIRE DEPARTMENT STATION 21- PLN11791SPR/CUP**  
Public Meeting and Recommendation
- 6:55 PM**      **SITE PLAN & DESIGN REVIEW PERMIT:  
BI FIRE DEPARTMENT STATION 22- PLN14200SPR/CUP**  
Public Meeting and Recommendation
- 7:40 PM**      **ISLAND-WIDE TRANSPORTATION PLAN**  
Discussion
- 8:10 PM**      **ORDINANCE 2016-28 LOW IMPACT DEVELOPMENT REGULATIONS**  
Study Session
- 8:55 PM**      **NEW/OLD BUSINESS**
- 9:00 PM**      **ADJOURN**

**\*\*TIMES ARE ESTIMATES\*\***

Public comment time at meeting may be limited to allow time for Commissioners to deliberate. To provide additional comment to the City outside of this meeting, e-mail us at [pcd@bainbridgewa.gov](mailto:pcd@bainbridgewa.gov) or write us at Planning and Community Development, 280 Madison Avenue, Bainbridge Island, WA 98110

**For special accommodations, please contact Jane Rasely, Planning & Community  
Development 206-780-3758 or at [jrasely@bainbridgewa.gov](mailto:jrasely@bainbridgewa.gov)**



DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT

STAFF REPORT

Date: October 18, 2016  
To: City of Bainbridge Island Planning Commission  
From: Joshua Machen, AICP  
Planning Manager  
Project: Bainbridge Island Fire Department Station 21  
Site Plan and Design Review/ Conditional Use Permit  
File Number: PLN11791B SPR/CUP

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I. INTRODUCTION

**Applicant:** Mackenzie  
C/O Michael Chen  
500 Union Street, Suite 545  
Seattle, WA 98101

**Owner:** Bainbridge Island Fire Department

**Request:** The proposed project is the redevelopment of the existing fire station on the site, Station #21. The redeveloped fire station will have two stories with approximately 30,760 square feet with living quarters, meeting spaces, offices, storage areas, and apparatus bays. The existing memorial and helicopter landing pad and fence will remain. (Attachment B, C & G)

**Location:** 8895 Madison Avenue, being portions of Sect. 22, T. 25N. R. 02E. W.M. Tax parcel numbers 222502-1-023-2000 and 4169-000-050-0206

**Environmental Review:** A SEPA Threshold Determination was issued on October 11, 2016

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**Recommendation:** Approve the application with the following conditions:

**SEPA Conditions**

1. All graded materials removed from the subject property shall be hauled to and deposited at City approved locations (Note: local regulations require that a grade/fill permit is obtained for any grading or filling of 50 cubic yards of material or more if the grading or filling occurs on sites that have not been previously approved for such activities. A SEPA

Threshold Determination is required for any fill over 100 cubic yards on sites that have not been previously received a SEPA determination).

2. Contractor is required to stop work and immediately notify the Department of Planning and Community Development and the Washington State Office of Archaeology and Historic Preservation if any historical or archaeological artifacts are uncovered during excavation or construction.
3. To mitigate the possible impact on adjacent properties from light and glare, all exterior lighting shall be hooded and shielded so that the bulb is not visible from adjacent properties. All landscape lighting shall be downcast and lighting within surface parking lots shall be no higher than 14 feet above grade. All exterior lighting shall comply with BIMC Chapter 15.34.
4. Prior to any clearing or grading on the site, chain-link construction fencing shall be installed and inspected by the City at the edge of the tree's dripline for trees being preserved as part of the development.
5. All construction activities shall comply with the construction operating hours limitations contained in BIMC Chapter 16.16. Noise produced by this development must comply with the maximum environmental noise levels established by the Washington Administrative Code 173-60 or its successor.
6. Frontage improvements along New Brooklyn Road must be completed prior to Certificate of Occupancy and/or Final Inspection. Frontage improvements include pedestrian sidewalk and bike lane.
7. A mid-block crosswalk on Madison Avenue must be designed, approved, and constructed prior to Certificate of Occupancy and/or Final Inspection.
8. All on-site stormwater facilities shall remain privately owned and maintained. The owner shall be responsible for maintenance of the storm drainage facilities for this development following construction. Annual inspection and maintenance reports shall be provided to the City. A declaration of Covenant for stormwater system operation and maintenance will be required to be recorded before issuance of occupancy permits. The approved language for the Declaration of Covenant is found in BIMC 15.21.

Project Conditions:

9. The site shall be developed in substantial conformance with the revised site plans date stamped September 29, 2016 and building elevations date stamped received October 17, 2016.
10. The 50-foot averaged full landscape screen along Highway SR305, as shown on the submitted plans, shall be maintained between the proposed development and Highway SR 305 to the west. All existing trees within the required buffers shall be retained and protected during and after construction.
11. The applicant shall obtain an approved building and/or grading permit from the Department of Planning and Community Development, prior to any construction activities on the site.

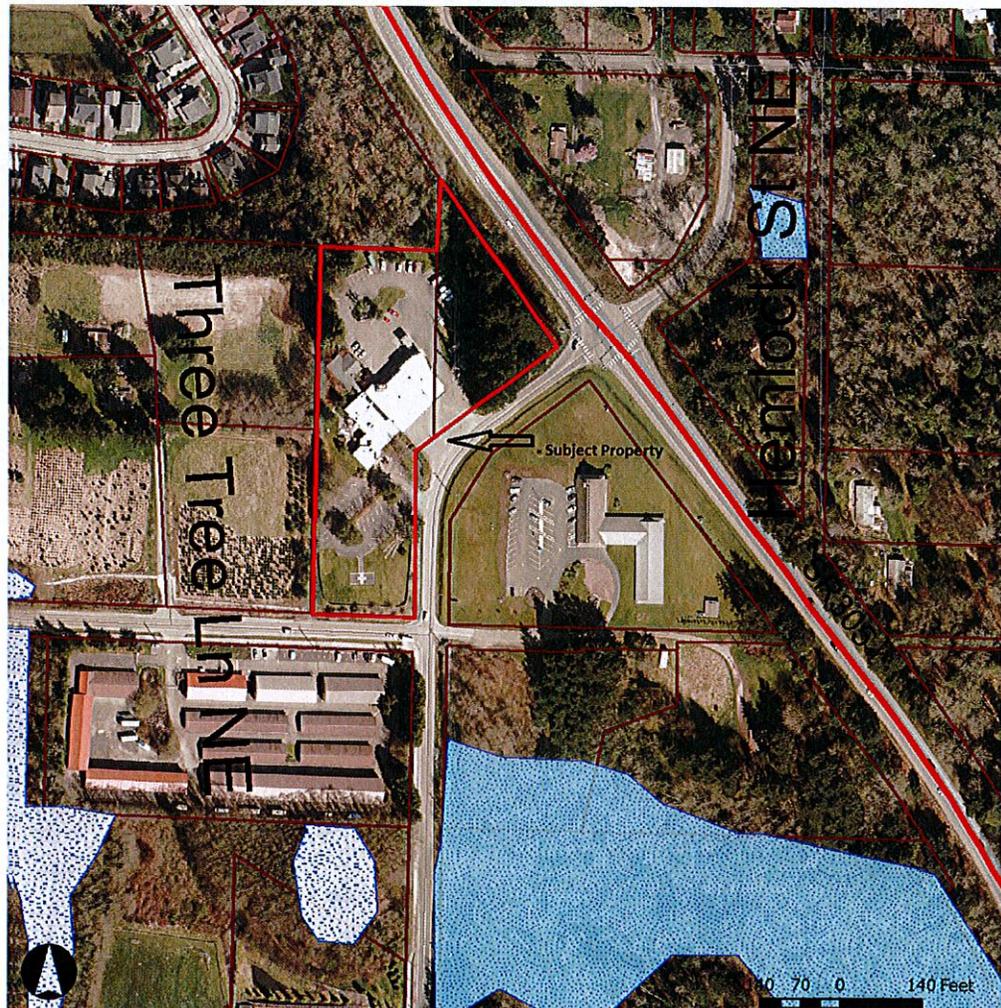
12. Sign permits shall be obtained as required by BIMC Section 15.08.
13. Parking shall be improved in substantial conformance with the approved site plan. The parking area shall be paved, all stalls shall be striped to their full dimensions and appropriate signage shall be placed at each handicap stall(s). Each parking stall shall meet the dimensional standards of BIMC Table 18.15.020-3 outside of required driving aisles or minimum sidewalk width of five feet.
14. The primary walkways throughout the development shall meet accessibility requirements including being surfaced with nonskid hard surfaces and providing a minimum of five feet of unobstructed width.
15. A minimum of 10 bicycle spaces shall be installed prior to final inspection. The racks shall allow for the wheel and frame of the bicycle to be locked. The bicycle stalls shall provide for both visitors and employees of the station.
16. All landscaping shall be installed as provided in the approved landscaping plans or a performance assurance device shall be submitted and approved, prior to final inspection of the station. The installation of landscaping shall be verified by a Landscape Professional and a landscaping declaration shall be signed by either the certified landscape professional or owner.
17. Prior to occupancy of the station or the release of a landscaping performance assurance, a landscaping maintenance assurance device for the required landscaping shall be provided to COBI for a period of three years. All landscaping and buffers shall be maintained for the life of the project.
18. An International Society of Arboriculture (ISA) valuation for all trees, where the critical root zone of a tree required to be retained may be impacted by clearing, grading construction, development, or maintenance, shall be submitted to issuance of the building permit for the station.
19. In order to define the circulation system and pedestrian separation, raised curbs shall be used to separate landscaping and raised walkways from parking stalls and drive aisles.
20. Exterior trash receptacles/recycling facilities shall be fully screened with solid walls and gates (no chain-link fencing). The screening enclosures shall be architecturally consistent with the adjacent station. All enclosures shall be constructed and inspected prior to final inspection.
21. At the time of building permit submittal, detailed lighting plans demonstrating compliance with the lighting standards shall be submitted for review and approval by the COBI.
22. All mechanical equipment shall either be located underground, incorporated into landscaping or integrated within the building or roof form of the building. Parapet walls may be used to screen roof top mechanical equipment as long as equipment is completely obscured from view and the parapet does not substantially detract from the building architecture.
23. Proposed fire station must be equipped with fire sprinklers and fire alarms as per the adopted code.

## Staff Analysis

### II. FINDINGS OF FACT

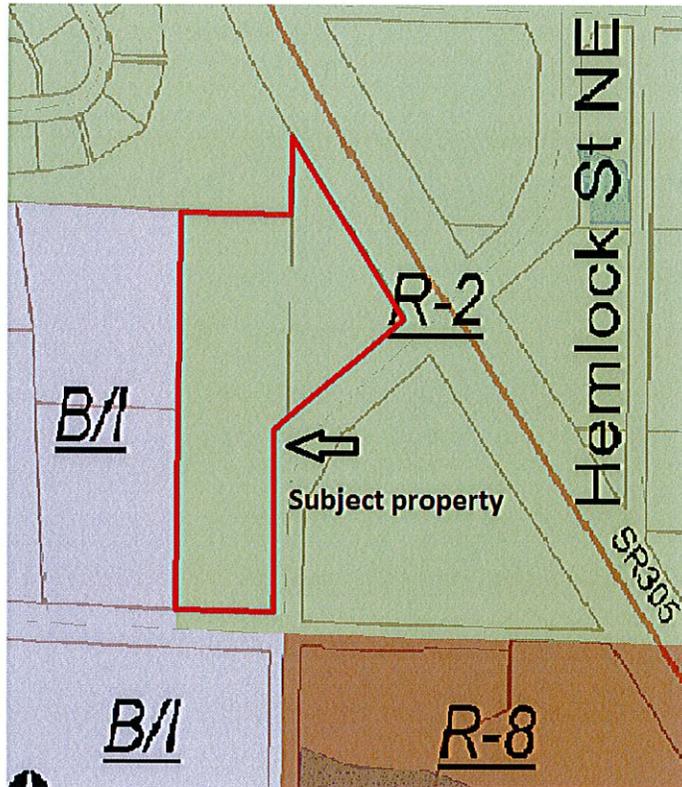
#### A. Site Characteristics

1. ASSESSOR'S RECORD INFORMATION:  
222502-1-023-2000 and 4169-000-050-0206, Bainbridge Island Fire District, 3.91 Acres
2. TERRAIN:  
The property relatively flat. The property has just a few trees around the perimeter of the property and a large triangle cluster at the corner of SR305 and Madison Avenue.



3. SITE DEVELOPMENT/EXISTING USE:  
The site contains an existing fire station and related improvements, including parking, a memorial, helipad and living facilities.

4. ACCESS:  
Vehicular access to the site is from Madison Avenue
5. PUBLIC SERVICES:
  - a. Police - Bainbridge Island Police Department
  - b. Fire - Bainbridge Island Fire District
6. EXISTING ZONING/ COMPREHENSIVE PLAN DESIGNATION:  
R-2, 2 units per acre residential zone  
OSR-2, Open Space Residential, two units per acre



7. SURROUNDING ZONING/COMPREHENSIVE PLAN DESIGNATION:
  - a. North: R-2
  - b. East: R-2
  - c. South: B/I
  - d. West: B/I
8. SURROUNDING USES:
  - a. North: Single-family Residential Development
  - b. East: Highway 305/Single Family
  - c. South: Mini Storage/Church
  - d. West: Bainbridge Artisan Network (under construction)

B. History

1. A pre-application conference was held December 1, 2015
2. The Design Review Board reviewed the pre-application proposal on December 21, 2015 and made several recommendations on the design (Attachment U).
3. A public participation meeting was held at the City on January 11, 2016. Comments and responses from that meeting are included as Attachment A.
4. A formal application for the Site Plan and Design Review and Conditional Use Permit were submitted on January 15, 2016 (Attachments B & C).
5. On February 22, 2016, the application was taken back to the Design Review Board, the board reviewed the revised drawings and recommended approval of the application (Attachment V).
6. Revisions to the application and submission documents were submitted on March 4, 2016.
7. A notice of technically complete application was issued on March 9, 2016, but requested additional information to be provided prior to the complete processing of the applications.
8. A Notice of Application/SEPA comment period was published and mailed out March 11, 2016.
9. Revisions to the application were received on April 29, 2016
10. On May 27<sup>th</sup> a memo from the City's Development engineer indicated the application was still deficient proper documentation regarding stormwater.
11. On June 7, 2016 a Preliminary Technical Information Report (drainage report) was submitted to the City.
12. On June 22, 2016 the Development Engineer provided a memo indicating that the revisions received from the applicant in April and the drainage report received in June were still deficient and that additional information and revisions were required.
13. On September 12<sup>th</sup> then again on September 23<sup>rd</sup> revised stormwater plans and technical documents were submitted.
14. The City Development Engineer reviewed the project and provided final comments and recommended conditions on October 6, 2016 (Attachment T).

C. Public Comment

No public comment was received during the official public comment period.

D. Comprehensive Plan Analysis

**OS 4.2 The R-2 District is intended to recognize an existing development pattern of two units per acre.** The subject development is proposed within the residential 1/2-acre zone. The Fire station is an essential public facility and is replacing an existing fire station. The changes to the station are being properly reviewed through the Site Plan and Design Review process as well as the Conditional Use Permit process to ensure compliance with the code requirements as well as compatibility with neighboring development.

**SD 1.3** The City shall require new development to provide both on-site improvements and off-site improvements necessary to avoid adverse downstream water quality and quantity impacts. **The applicant has submitted an engineered comprehensive stormwater drainage plan that**

**addresses downstream water quality and quantity impacts. The submitted plans were reviewed by and found to meet local and state regulations (Attachments T).**

**SD 1.4** Where appropriate and feasible, infiltration of stormwater is preferred over surface discharge to downstream systems. The return of uncontaminated precipitation to the soil at natural rates near where it falls should be encouraged through the use of detention ponds, grassy swales and infiltration facilities. **The stormwater drainage plan submitted with the application includes several of these features, including but not limited to rain gardens, and infiltration/dispersal systems that will allow water to return to the soil.**

E. Land Use Code Analysis

1. **BIMC Chapter 16.16 Noise Regulations**

The proposal is subject to noise regulations related to the construction hours and maximum environmental noise levels from the proposed uses on the property. Since the construction for this proposal will occur within 100 feet of residentially zoned properties, construction activities are limited to certain hours and days of the week in accordance with BIMC 16.16.025. The maximum allowed environmental noise levels are those allowed by state law, which include specific exemptions for emergencies.

2. **BIMC Chapter 18.06.0020C**

The purpose of the R-2 zone is to provide residential neighborhoods in an environment with special island character consistent with other land uses such as agriculture and forestry, and the preservation of natural systems and open space, at a somewhat higher density than the R-1 district. **The proposed reconstructed fire station is consistent with the purpose of this district as it is an essential public facility that directly serves the residential neighborhoods of the island in addition to the commercial zones. The subject property is well suited for a fire station as it only has residential uses on one side and that neighborhood is buffered from the station by a large open space area within the subdivision and a 25-foot buffer on the subject property.**

3. **BIMC Chapter 18.09.020 Permitted Use Table**

The permitted use table lists the multiple uses that are that are permitted and conditionally permitted within the R-2 zone. Included in those allowed by a conditional use permit are governmental facilities.

4. **BIMC Chapter 18.12 Dimensional Standards**

The proposed development is within the R-2 zone, therefore the maximum lot coverage for is 20 percent. **The proposed lot coverage for this development is approximately 13.1 percent overall (approximately 22,300 square feet of building footprint).**

The setback from the SR 305 right-of-way is a minimum of 50 feet in order to provide the required landscape buffer. The front yard setbacks along Madison Avenue and New Brooklyn Road are 25 feet, the proposed building is a minimum of 46 feet from the street. The required side setbacks are 5' minimum, however the perimeter buffers are larger. The north property line has a buffer of 25-feet and along the west property line the buffer is a minimum of 10 feet with an average of 20-feet.

The building height limitations for the R-2 zone is 30 feet, however non-residential structures that meet the conditional use decision criteria may go up to 35 feet. **The propose building has a height of 34 feet.**

5. **BIMC Chapter 18.15 Development Standards and Guidelines**

a. **18.15.010 Landscaping and Screening**

Since the proposal is located within the R-2 zone, the project has a required 25-foot average perimeter full-screen landscape buffer along the single-family use to the north and a 20-foot average perimeter full-screen landscape buffer to the business industrial zoned properties to the west. The development is also required to provide a 50-foot partial screen buffer to the SR305 right-of-way and a 25-foot partial screen along the Madison Avenue and New Brooklyn Road frontages. **The proposed development is providing the full 25 foot full-screen landscape buffer to the residential property along the north property line and is providing an averaged 20-foot landscape buffer along the west property line to accommodate fire apparatus access driveway (minimum proposed width of 13-feet). The proposal also provides existing landscape buffering along Madison Avenue and New Brooklyn Road, but much of the frontage is dedicated to access driveways and existing fire apparatus exit driveway.**

The parking lot landscaping requirement for the proposed development entails providing one tree for every four parking stalls as most of the parking areas are located to the side of the buildings. The project has a 43 parking stalls proposed; therefore 11 parking lot related trees will need to be planted. The applicant has proposed planting close to 129 new trees, which is in excess of the minimum required for the parking lot landscaping, the required buffers and the tree unit requirements. Also as required the landscaping plan properly designates landscape areas at the end of the parking stalls and aisles (Attachments P, Q & R).

The total site tree requirements for non-residential development within the R-2 zone is 40 tree units per acre. Each existing tree that is being preserved is given a tree unit value based on its size, then all proposed trees to be planted are also given a tree unit value. The property is 2.88 acres outside the required buffers; therefore, a total of 115.2 tree units would typically be required outside of the significant trees being preserved within required perimeter buffers or the roadside buffers. **The proposal is retaining 96.8 tree units and is proposing to plant 129 trees. While some of the replanted trees are required for the buffers and the parking lot tree planting requirements, the proposed retained trees in addition to the planted trees far exceed the minimum tree unit requirements. As conditioned, all landscaping and buffers need to be maintained throughout the life of the project and the proposed landscaping must be installed or an assurance device provided prior to any occupancy of the buildings (Condition 17).**

Retained trees within buffers and those needed to be retained to meet the tree unit requirements require protection during construction. **The applicant has proposed tree protection strategies in compliance with the municipal code requirements including but not limited to construction fencing, dripline protection, soil amendment, and root pruning (Attachment Y). In addition, the City required and the applicant provided a Certified Arborist analysis of any tree proposed to be retained whose dripline/critical**

root zone would be affected by the construction. Her review, analysis and recommendations are included in Attachment Z.

b. **18.15.020 Parking and Loading**

The proposed building is proposed as a fire station facility; therefore, the parking regulations require enough spaces to meet the peak demand as verified by the Director. **As proposed, there are 43 parking spaces proposed, which exceeds the peak parking demand forecasted by the Bainbridge Island Fire Department, 28 spaces. While the Director recognizes the benefit of additional spaces, only the Planning Commission may approve parking spaces above the required spaces, which is the peak demand in this special case (Attachment AA).**

As required by this section of code the locations of the parking within this development are primarily to the sides or behind the proposed buildings (Attachment G). As conditioned, each parking stall is to meet the dimensional standards of Table 18.15.020-3 outside of required driving aisles or minimum sidewalk widths (Conditions 13).

c. **18.15.030 Mobility and access**

In order to define the circulation system and pedestrian separation, raised curbs shall be used to separate landscaping and raised walkways from parking stalls and drive aisles (Condition 19). In accordance with the regulations regarding mobility and access, all internal walkways shall be surfaced with nonskid hard surfaces and provide at least five feet of unobstructed width (Condition 14).

Bicycle spaces are required for the proposed station. A minimum of 9 bicycle spaces are required for this development. The site plan properly designates 10 available spaces that will serve both employees and the public (Condition 15).

d. **18.15.040 Outdoor Lighting**

In order to preserve and enhance the view of the dark sky and promote health, safety and security, outdoor lighting is to be shielded and maintained so there is no light trespass. All lighting shall be downcast and shielded such that it masks the horizontal surface of the light source. At the time of building permit submittal, detailed lighting plans demonstrating compliance with the lighting standards shall be submitted for review and approval by the City (Condition 21).

6. **BIMC Chapter 18.18 Design Guidelines**

The proposed project is subject to; the commercial and mixed-use design guidelines for all zoning districts. The application was first reviewed by the Design Review Board on December 21, 2015 during the pre-application conference phase of review. The Design Review Board made several suggestions during that meeting and the applicant responded by making several modifications to the design including but not limited to addressing the proposed entrance to the building, the visual approach from the south and the roofline (Attachment U).

The Design Review Board (DRB) again reviewed the proposal after the applicant had submitted the formal Site Plan and Design Review Application on February 22, 2016. The Board reviewed the design line checklist and found the design compliant with the guidelines but wanted to see the landscaping plans at the meeting on March 7, 2016. (Attachment V). At the March 7, 2016 meeting the landscape architect for the project presented the landscape plan and the DRB finalized their recommendation for approval of the station as designed (Attachment W ).

**7. BIMC Chapter 2.16.040 Site Plans and Design Review**

The Bainbridge Island Fire Station 21 was properly submitted as a major site plan and design review application along with a major conditional use permit. Since this is a major conditional use permit and site plan and design review application, the applicant first applied for a pre-application conference and had the preliminary plans reviewed by the Design Review Board. The applicants met with the Design Review Board once during the pre-application phase of the project, on December 21, 2015. The application was also required to have a Public Participation Meeting, which was held January 11, 2016. The DRB reviewed the application again as part of their formal submittal on February 22, 2016 (Attachment V) As a Major Site Plan and Design Review, the application is properly before the Planning Commission for review in a public meeting. The Planning Commission is to provide a recommendation to the Hearing Examiner, who will issue a decision on the consolidated project. The staff recommends the Planning Commission recommend approval of the application with the conditions listed in this project report.

*Decision Criteria*

The Director and Planning Commission shall base their respective recommendations or decision on site plan and design review applications on the following criteria:

- a. The site plan and design is in conformance with applicable code provisions and development standards of the applicable zoning district; **As summarized in this staff report and in the materials prepared by the applicant, the application, with conditions, is in conformance with the Bainbridge Island Municipal Code.**
- b. The locations of the buildings and structures, open spaces, landscaping, pedestrian, bicycle and vehicular circulation systems are adequate, safe, efficient and in conformance with the non-motorized transportation plan; **The proposed site plan provides a building layout, parking and circulation system that are an efficient use of the available land while providing the required setbacks and screening from the adjacent properties and open spaces.**
- c. The Kitsap County Health District has determined that the site plan and design meets the following decision criteria:
- e. The proposal conforms to current standards regarding domestic water supply and sewage disposal; or if the proposal is not to be served by public sewers, then the lot has sufficient area and soil, topographic and drainage characteristics to permit an on-site sewage disposal system. **The Health District reviewed the project and had no comment.**
  - i. If the Health District recommends approval the application with respect to those items in subsection i., the health district shall so advise the director. **No comment.**

- ii. If the health district recommends disapproval of the application, it shall provide a written explanation to the director. **The Health District has not denied the application.**
  
- d. The city engineer has determined that the site plan and design meets the following decision criteria:
  - i. The site plan and design conforms to regulations concerning drainage in Chapters 15.20 and 15.21 BIMC; and
  
  - ii. The site plan and design will not cause an undue burden on the drainage basin or water quality and will not unreasonably interfere with the use and enjoyment of properties downstream; and
  
  - iii. The streets and pedestrian ways as proposed align with and are otherwise coordinated with streets serving adjacent properties; and
  
  - iv. The streets and pedestrian ways as proposed are adequate to accommodate anticipated traffic; and
  
  - v. If the site will rely on public water or sewer services, there is capacity in the water or sewer system (as applicable) to serve the site, and the applicable service(s) can be made available at the site; and
  
  - vi. The site plan and design conforms to the “City of Bainbridge Island Engineering Design and Development Standards Manual,” unless the city engineer has approved a variation to the road standards in that document based on his or her determination that the variation meets the purposes of BIMC Title 18.

**The Development Engineer has reviewed the submitted civil plans and has found them to be consistent with these requirements. A detailed response and analysis is contained in her memo (Attachment T).**

- e. The site plan and design is consistent with all applicable design guidelines in BIMC Title 18, unless strict adherence to a guideline has been modified as a housing design demonstration project pursuant to BIMC 2.16.020.Q; **The proposed development was reviewed by the Design Review Board during the pre-application stage of the development and during the Site Plan and Design Review process (Attachments U,V & W). The applicant has incorporated the applicable Design Review Board’s recommendations into the final design of the buildings and layout of the site. As now proposed, the design of the building form, wall articulation, roof form, landscaping and overall layout is consistent with the intent of the applicable design guidelines.**

- f. No harmful or unhealthful conditions are likely to result from the proposed site plan; **The proposed development is for a use conditionally allowed by the Municipal Code within the R-2 zoning district.**
  - g. The site plan and design is in conformance with the Comprehensive Plan and other applicable adopted community plans; **The proposed plan provides for the replacement of an essential public facility.**
  - h. Any property subject to site plan and design review that contains a critical area or buffer, as defined in Chapter 16.20 BIMC, conforms to all requirements of that chapter; **No critical areas are identified on the subject property.**
  - i. Any property subject to site plan and design review that is within shoreline jurisdiction, as defined in Chapter 16.12 BIMC, conforms to all requirements of that chapter; **The subject property is not within the shoreline jurisdiction.**
  - j. If the applicant is providing privately owned open space and is requesting credit against dedications for park and recreation facilities required by BIMC 17.20.020.C, the requirements of BIMC 17.20.020.D have been met; **The requirements of BIMC 17.20.020 are not applicable to this application because the proposal contains no new dwelling units.**
  - k. The site plan and design has been prepared consistent with the purpose of the site design review process and open space goals; **The proposed site plan was prepared consistent with the overall purpose and goals of the Site Plan and Design Review process. The process has provided a means for guiding the development in a logical, safe, attractive and expedient manner.**
  - l. For applications in the B/I zoning district, the site plan and development proposal include means to integrate and re-use on-site storm water as site amenities. **The subject property is not located within the B/I zoning district.**
8. **BIMC Chapter 2.16.110 Major Conditional Use Permit**  
 The applicants have also properly applied for a major Conditional Use Permit to allow a fire station within the residential R-2 zoning district. Fire Stations are considered government facilities; therefore, they are required to be reviewed under the major Conditional Use Permit process. Since this Conditional Use Permit is part of a consolidated project review with the Site Plan and Design Review application, it was also subject to a pre-application conference, public participation meeting and Design Review Board recommendation. The role of the Planning Commission is to make a recommendation to the Hearing Examiner.

Decision Criteria

A conditional use may be approved or approved with conditions if:

The conditional use is harmonious and compatible in design, character and appearance with the intended character and quality of development in the vicinity of the subject property and with the physical characteristics of the subject property; provided, that in the case of a housing design demonstration project any differences in design, character or appearance that are in furtherance of the purpose and decision criteria of BIMC 2.16.020.Q. shall not result in denial of a Conditional Use Permit for the project; **The proposed development is for the renovation/redevelopment of an existing fire station, which is an essential public facility. While a couple of the surrounding uses are single-family residences, the fire station has been in existence and operation for many years. No public comment or concerns were raised during the public comment period. The other adjacent developments are a mini storage facility and the new Bainbridge Artisans Resource Network (BARN) building.**

- a. The conditional use will be served by adequate public facilities including roads, water, fire protection, sewage disposal facilities and storm drainage facilities; **As discussed in this staff report, with conditions, the proposed conditional use is adequately served by public roads, water, sewer, fire protection and proper storm drainage facilities will be constructed.**
- b. The conditional use will not be materially detrimental to uses or property in the vicinity of the subject property; **Allowing the reconstruction/expansion of the fire station will not be materially detrimental to uses in the vicinity. The use will continue with little change from the way it has operated for many years. Proper access, parking, circulation, landscaping are being provided.**
- c. The conditional use is in accord with the Comprehensive Plan and other applicable adopted community plans, including the Non-Motorized Transportation Plan; **As discussed in Section II. C of this report, the proposed conditional use is consistent with the goals and policies of the Comprehensive Plan.**
- d. The conditional use complies with all other provisions of the BIMC, unless a provision has been modified as a housing design demonstration project pursuant to BIMC 2.16.020.Q; **As detailed in this staff report, the conditional use complies, as does the building covered by the Site Plan Review, with all provisions of the BIMC.**
- e. All necessary measures have been taken to eliminate or reduce to the greatest extent possible the impacts that the proposed use may have on the immediate vicinity of the subject property;
- f. Noise levels shall be in compliance with BIMC [16.16.020](#) and 16.16.040.A; **While the project is required by law to be in compliance with BIMC 16.16.020, under emergency situations, it is likely that the noise standards will be exceeded by ambulances or fire trucks leaving the facility, also the facility will continue to provide the helipad for medivac.**
- g. The vehicular, pedestrian, and bicycle circulation meets all applicable city standards, unless the city engineer has modified the requirements of BIMC 18.15.020.B.4 and B.5, allows alternate driveway and parking area surfaces, and confirmed that those surfaces meet city requirements for handling surface water and pollutants in accordance with Chapters [15.20](#) and [15.21](#) BIMC; **The Site Plan Review application and the Conditional Use Permit application have been reviewed by the city's development engineer and as conditioned the vehicular, pedestrian, and bicycle circulation meets all applicable City standards and pollution controls. A detailed analysis prepared by the**

**Development Engineer was provided to staff in memo (Attachment T). Pedestrian improvements are being required along New Brooklyn Road, by the City requiring the extension of the sidewalk, and by requiring a mid-block crossing on Madison Avenue (Conditions 6 & 7).**

- h. The city engineer has determined that the conditional use meets the following decision criteria:
  - i. The conditional use conforms to regulations concerning drainage in Chapters [15.20](#) and [15.21](#) BIMC; and
  - ii. The conditional use will not cause an undue burden on the drainage basin or water quality and will not unreasonably interfere with the use and enjoyment of properties downstream; and
  - iii. The streets and pedestrian ways as proposed align with and are otherwise coordinated with streets serving adjacent properties; and
  - iv. The streets and pedestrian ways as proposed are adequate to accommodate anticipated traffic; and
  - v. If the conditional use will rely on public water or sewer services, there is capacity in the water or sewer system (as applicable) to serve the conditional use, and the applicable service(s) can be made available at the site; and
  - vi. The conditional use conforms to the "City of Bainbridge Island Engineering Design and Development Standards Manual," unless the city engineer has approved a variation to the road standards in that document based on his or her determination that the variation meets the purposes of BIMC Title [17](#).

**The city's Development Engineer has reviewed the Conditional Use Permit and Site Plan and Design Review application for consistency with each of the above criteria and has found the design and specifications to be consistent and in compliance with the above criteria or has recommended proper conditions to address the concerns (Attachment T).**

- i. If a major conditional use is processed as a housing design demonstration project pursuant to BIMC 2.16.020.Q, the above criteria will be considered in conjunction with the purpose, goals, policies, and decision criteria of BIMC 2.16.020.Q. **The proposal is not a housing design demonstration project.**
- j. Additional Decision Criteria for Institutions in Residential Zones. Applications to locate any of those uses categorized as educational facilities, governmental facilities, religious facilities, health care facilities, cultural facilities, or clubs in Table 18.09.020 in residential zones shall be processed as major conditional use permits and shall be required to meet the following criteria, in addition to those in subsection D of this section:
  - 1. All sites must front on roads classified as residential suburban, collector, or arterial on the Bainbridge Island functional road classification map. **The subject property is fronting on Madison Avenue which is a collector road.**
  - 2. If the traffic study shows an impact on the level of service, those impacts have been mitigated as required by the city engineer. **The submitted trip generation data did not show a change to the level of service requiring mitigation. The project is increasing the PM peak hour trips by five trips and increasing trips overall by less than 49, therefore a traffic study was not required.**

3. If the application is located outside of Winslow study area, the project shall provide vegetated perimeter buffers in compliance with BIMC [18.15.010](#). **The proposal is within the Winslow study area, but will still be providing buffers in accordance with 18.15.010.**
4. The proposal meets the requirements of the commercial/mixed use design guidelines in BIMC [18.18.030.C](#). **The proposal is consistent with the commercial/mixed use design guidelines as far as they were applicable. The DRB reviewed the project and has provided a recommendation of approval (Attachments U, V & W).**
5. The scale of proposed construction including bulk and height and architectural design features is compatible with the immediately surrounding area. **As a fire station, the construction has unique architectural style, however it will be compatible with the immediate surrounding properties which include a church, a mini storage facility and the new Bainbridge Artisans Resource Network (BARN) building.**
6. If the facility will have attendees and employees numbering fewer than 50 or an assembly seating area of less than 50, the director may waive any or all the above requirements in this subsection E, but may not waive those required elsewhere in the BIMC. **No requirements were waived.**
7. Lot coverage does not exceed 50 percent of the allowable lot coverage in the zone in which the institution is located, except that existing public schools and governmental facilities, as defined in BIMC Title [18](#), shall be allowed 100 percent of the lot coverage established in the underlying zoning district in which it is located unless conditions are required to limit the lot coverage to mitigate impacts of the use. **This construction is replacing an existing governmental facility therefore it is allowed 100 percent of the underlying lot coverage. The proposal is to use only 13% lot coverage when 20 % would be allowed.**

9. **BIMC Chapter 2.16.170 Consolidated Project Review**

When an applicant has two or more land use applications, they may request that the applications be processed under a single consolidated project review process. **The proposed project requires both Site Plan and Design Review in addition to a Conditional Use Permit. The two applications are being reviewed under the consolidated project review provisions.**

F. Environmental Review:

A SEPA Mitigated Determination of Nonsignificance was issued by the SEPA Official on October 11, 2016.

III. **CONCLUSIONS**

***Staff's Conclusion***

10. As conditioned, the proposed project is consistent with the Comprehensive Plan, the applicable sections of the Bainbridge Island Municipal Code including: BIMC Chapter 16.16 Noise Regulations; BIMC Chapter 18.06.0020C R-2 Zone; BIMC 18.09.020 Permitted Use Table; 18.12 Dimensional Standards; BIMC 18.15.010 Landscaping and Screening; BIMC 18.15.020 Parking and loading; BIMC 18.15.030 Mobility and access; BIMC 18.15.040 Outdoor Lighting; and BIMC 18.18 Design Guidelines; BIMC 2.16.040 Site Plans and Design Review; and BIMC Chapter

2.16.170 Consolidated Project review. Appropriate notice of application was made, and a SEPA Mitigated Determination of Non-significance was issued. The application is properly before the Planning Commission in a public meeting.

**IV. Attachments:**

- A. Public Participation Meeting Summary and comment matrix
- B. Site Plan and Design Review Application, Submitted January 15, 2016
- C. Conditional Use Permit Application, Submitted January 15, 2016
- D. Vicinity Map/Aerial Photo of Site
- E. Project Summary/Introduction/Description of Proposal by Architect
- F. Existing Site Conditions Map
- G. Site Plan Map
- H. First Floor Plan
- I. Second Floor Plan
- J. Elevation Drawings
- K. Building Section
- L. Street Perspective Drawing
- M. High Angle Perspective Drawing
- N. Tree Retention Plan
- O. Plant Schedule
- P. Planting Plan (South)
- Q. Planting Plan (North)
- R. Planting Plan (East)
- S. Landscape Buffer Diagram
- T. Development Engineer Project Review Memo
- U. Design Review Board Minutes, December 21, 2015
- V. Design Review Board Minutes, February 22, 2016
- W. Design Review Board Minutes, March 7, 2016
- X. Bainbridge Island Fire Department Memo/Recommendation
- Y. Landscape Plan Supplemental Information
- Z. Arborist Analysis of trees potential impacts by development
- AA. Letter from Mackenzie regarding parking demand

Redevelop the existing fire station 21 located at 8895 Madison Avenue North as a two-story, approximately 30,760 sf building with living quarters, meeting spaces, offices, storage areas and an apparatus bay, with associated site improvements. The existing memorial is to be preserved; the flagpoles are to be relocated. The existing helicopter landing pad and fence in the southern portion of the site are to be preserved.

Redevelop the existing fire station 22 located at 7934 Bucklin Hill Road as a two-story, approximately 16,808 sf building with living quarters, meeting spaces, offices, storage areas and an apparatus bay with associated site improvements. The existing flag poles will be relocated.

ISSUE/QUESTION	RESPONSE FROM APPLICANT OR CITY
Was the facility considered to have a joint police/fire building?	It was originally considered but as of 2014 there was a vote to not combine the two.
Does this plan prohibit a combined facility?	Yes, this is only large enough for fire. The land is too small for a combined facility.
What is the acreage of Station 21?	3.91 acres.
Have you looked into geothermal?	Yes, but the size and cost make it prohibitive.
Are both buildings being replaced or remodeled?	Replaced.
How big are the existing buildings compared to the proposed?	Building 21 is approximately 1/3 <sup>rd</sup> larger than the existing.
How many parking spaces for the public is there at Station 21? Is there more or less than existing?	The same amount is proposed.
Would you consider a safe route from bus stop on 305 to the Barn property through the Fire station?	Our concern is for safety due to the helipad. We will look into a possible pedestrian connection.
Why are you proposing 9 bays?	We are designing for 50 years and anticipate extra bays for engines and EMS services.

CITY OF BAINBRIDGE ISLAND

**SITE PLAN AND DESIGN REVIEW APPLICATION**

FORM MUST BE COMPLETED IN INK, PREFERABLY BLUE.  
PENCIL WILL NOT BE ACCEPTED.



<p><u>DATE STAMP FOR CITY USE ONLY</u></p> <p><b>City of Bainbridge Island</b></p> <p><b>JAN 15 2016</b></p> <p><b>Planning and Community Development</b></p>	<p style="text-align: center;"><u>TO BE FILLED OUT BY APPLICANT</u></p> <p>PROJECT NAME: BAINBRIDGE ISLAND FIRE DEPARTMENT, STATION 21</p> <p>TAX ASSESSOR'S NUMBER: <u>222502-1-023-2000</u> <u>4169-000-050-0206</u></p> <hr/> <p>PROJECT STREET ADDRESS OR ACCESS STREET: 8895 MADISON AVENUE N</p> <p>ENVIRONMENTAL CHECKLIST SUBMITTED : <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO</p> <hr/> <p style="text-align: center;"><u>FOR CITY USE ONLY</u></p> <p>FILE NUMBER: _____</p> <p>PROJECT NUMBER: _____</p> <p>DATE RECEIVED: _____</p> <p>APPLICATION FEE: _____</p> <p>TREASURER'S RECEIPT NUMBER: _____</p>
<b>SUBMITTAL REQUIREMENTS</b>	
APPLICATION	<i>One original (which must contain an original signature) and six copies</i> must be provided. Whenever possible, originals must be <i>signed in blue</i> . Please identify the original document.
SUPPORTING DOCUMENTS	<i>One original (which must contain an original signature)</i> , where applicable, and <i>six copies</i> (if an original is not applicable, <i>seven copies</i> must be provided).
FULL-SIZE DRAWINGS	<i>Seven copies</i> of the required drawings must be provided. Drawings <i>must be folded and 18" x 24"</i> in size. <i>No construction drawings or other sized drawings</i> will be accepted unless specifically requested.
REDUCED DRAWINGS	<i>Five copies</i> of the drawings reduced to 11" x 17" must be provided.
SUBMITTING APPLICATIONS	Applications <i>must be submitted in person</i> by either the owner or the owner's designated agent. Should an agent submit the application, a <i>notarized Owner/Agent Agreement</i> must accompany the application. Please call (206) 780-3762 to make an appointment to submit your application.
FEEs	Please call the Department of Planning & Community Development for submittal fee information.
ATTACHED SUBMITTAL CHECKLIST	Please refer to attached Submittal Checklist for further information. <b>NOTE:</b> when submitting this application, please do not copy or include the Submittal Checklist sheets attached to the back of this application.
<p><b>APPLICATIONS WILL NOT BE ACCEPTED</b> unless these basic requirements are met and the submittal packet is deemed counter complete.</p>	

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT  
280 MADISON AVENUE NORTH • BAINBRIDGE ISLAND, WA • 98110-1812  
PHONE: (206) 842-2550 • FAX: (206) 780-2055 • EMAIL: [planning@bainbridgewa.gov](mailto:planning@bainbridgewa.gov)

CITY OF BAINBRIDGE ISLAND

SITE PLAN AND DESIGN REVIEW APPLICATION

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A. GENERAL INFORMATION

1. Name of property owner: Bainbridge Island Fire Department  
Address: 8895 Madison Avenue N, Bainbridge Island, WA 98110  
Phone: (206) 842-7633 Fax: \_\_\_\_\_  
E-mail: Lcarpenter@bifd.org

Name of property owner: \_\_\_\_\_  
Address: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
E-mail: \_\_\_\_\_

Name of property owner: \_\_\_\_\_  
Address: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
E-mail: \_\_\_\_\_

*If the owner(s) of record as shown by the county assessor's office is (are) not the agent, the owner's (owners') signed and notarized authorization(s) must accompany this application.*

2. Applicant/agent: Michael Chen, Mackenzie  
Address: Logan Building, 500 Union Street, Suite 545, Seattle, WA 98101  
Phone: (206) 749-9993, Extension 513 Fax: (206) 749-5565  
E-mail: MChen@mcknze.com

3. Name of land surveyor: Adam & Goldsworthy, Inc.  
Address: 1015 NE Hostmark Street, Suite 103, Poulsbo, WA 98370  
Phone: (360) 779-4299 Fax: (360) 779-4213  
E-mail: info@agols.com

4. Planning department personnel familiar with site: Nan Gladstein, Josh Machen

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CITY OF BAINBRIDGE ISLAND

SITE PLAN AND DESIGN REVIEW APPLICATION

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5. Description of proposal: Redevelopment of existing fire station as two-story, approximately 30,760-square foot building with living quarters, meeting spaces, offices, storage areas, and an apparatus bay, with associated site improvements. Existing helicopter pad and fence in southern portion of site to be preserved.

6. Driving directions to site: The site is accessible from Madison Avenue N to the east.

7. Please give the following existing parcel information:

Assessor's Parcel Number	Parcel Owner	*Lot Area
222502-1-023-2000	City of Bainbridge Island	2.8 ac.
4169-000-050-0206	City of Bainbridge Island	1.1 ac.
Use additional sheet if necessary		Total of all parcels: 3.91

\* As defined in Bainbridge Island Municipal Code 18.12.050

8. Legal description (or attach): 22252E THE E 208FT OF S1/2 SE1/4 NE1/4 PER SC NO.68005 TGW WLY 30FT OF TH PTN OF VAC CO RD PER CJ 13 PAGE 129

9. Current comprehensive plan, zoning and shoreline designations and use of subject parcel(s):

Lot Number	Comp Plan Designation	Zoning Designation	Shoreline Designation	Current Use
Lot 222502-1-023-2000	OSR-2	R-2		Existing fire station
Lot 4169-000-050-0206	OSR-2	R-2		Existing fire station
Lot				
Lot				

10. Current comprehensive plan, zoning and shoreline designations and use of adjacent properties:

Property	Comp Plan Designation	Zoning Designation	Shoreline Designation	Current Use

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North	OSR-2	R-2		Residential subdivision
South	CZ-2	B/I		Commercial storage
East	OSR-2	R-2		Church
West	B/I	B/I		Residential

11. Common name of adjacent water area or wetlands area: N/A

12. Does the site contain an environmentally sensitive area as defined in Critical Areas Ordinance (*Bainbridge Island Municipal Code Chapter 16.20*)?  yes  no  unknown

If yes, check as appropriate:

<input type="checkbox"/> wetland*	<input type="checkbox"/> geologically hazardous area**
<input type="checkbox"/> wetland buffer*	<input type="checkbox"/> zone of influence**
<input type="checkbox"/> stream*	<input type="checkbox"/> slope buffer**
<input type="checkbox"/> stream buffer*	<input type="checkbox"/> fish and wildlife habitat area

\* If your site includes a wetland or wetland buffer, a wetland report is required with your application.

\*\* If your site includes a geologically hazardous area or is within the zone of influence as defined in *Bainbridge Island Municipal Code 16.20*, a geotechnical report may be required with your application.

13. Are there underlying/overlying agreements on the property?  yes  no  unknown  
If yes, check as appropriate and provide a copy of the decision document:

<input checked="" type="checkbox"/> CUP Conditional Use Permit	<input checked="" type="checkbox"/> SPR Site Plan Review
<input type="checkbox"/> MPD Master Planned Development	<input type="checkbox"/> SPT Short Plat
<input type="checkbox"/> PUD Planned Unit Development	<input type="checkbox"/> SSDP Shoreline Permit
<input type="checkbox"/> REZ Contract Rezone	<input type="checkbox"/> SUB Prior Subdivision
<input type="checkbox"/> RUE Reasonable Use Exception	<input type="checkbox"/> VAR Zoning Variance
	<input type="checkbox"/> Other:

Under which jurisdiction was the approval given?

City of Bainbridge Island  Kitsap County Approval date: Varies

14. Is there any other information which is pertinent to this project?  yes  no

If yes, please explain: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

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CITY OF BAINBRIDGE ISLAND

**SITE PLAN AND DESIGN REVIEW APPLICATION**

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**B. TECHNICAL INFORMATION**

1. Name of water purveyor: City of Bainbridge Island  
 If a private well, what class? N/A

2. Type of sewage disposal:  on-site septic  off-site septic  sewer  
 Sewer district:  City of Bainbridge Island  Sewer District 7

3. General description of the existing terrain: The site is relatively flat, fully developed, with no known geological concerns.

4. Soil survey classification: Geotechnical information on the site is available from Aspect Consulting; soil types found on the site include sand, gravel, and silt.

5. Flood plain designation:  X  AE

6. Access (street functional road classifications):

Street Type	Required ROW Width	Street Name	Existing ROW Width
primary arterial	150 feet	Highway 305	150 feet (assumed)
secondary arterial	60 feet	Madison Avenue N & NE New Brooklyn Road	60 feet
collector	50 feet		
residential urban	40 feet		
residential suburban	30 feet		
private	20 - 30 feet		

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7. Sidewalks are adjacent to the parcel:  yes  no

If yes, existing sidewalks are \_\_\_\_\_ feet wide.

Sidewalk installation is proposed as part of the development project:  yes  no

Proposed sidewalks:  adjacent to the parcel and are to be \_\_\_\_\_ feet wide.

internal to the proposal and are to be 5-7 feet wide.

8. Intended use of the land, as well as the sequence and timing of the proposed development:

The land is proposed for redevelopment under the same use as a fire station.

Pending the required approvals, construction is expected to start in the fall of 2016 and to be complete in 2018.

9. Floor area ratio: 0.18

10. Dimensions of proposed structures: Proposed GSF: 30,760 SF; proposed lot coverage: 22,270 SF/13.1%

11. Height of proposed buildings or structures: 34'-0" (maximum, highest overhang/roofline)

12. Square footage of all spaces:	storage:	<u>3,289 square feet</u>
retail: <u>N/A</u>	residential:	<u>4,920 square feet</u>
office: <u>8,640 square feet</u>	other:	<u>13,977 square feet (apparatus bay and support spaces)</u>

13. Number of stories proposed: 2

14. Square feet per story: (1) 22,270 square feet (2) 8,490 square feet (3) N/A

15. Setback requirements:	north: <u>5-15 feet</u>	south: <u>5-15 feet</u>
	east: <u>25 feet</u>	west: <u>15 feet</u>

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SITE PLAN AND DESIGN REVIEW APPLICATION

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16. Number of parking stalls required: 40
17. Number of parking stalls proposed: 40
18. Amount of square footage of proposed paved areas: 51,874 square feet
19. Square footage of building area: 22,270 square feet
20. Percent of site to be covered by impervious surfaces: 43.5%  
(If the proposal results in more than 1,000 square feet of additional impervious surface, a drainage plan shall be required.)
21. Percentage of site to be covered by landscaping: 56.5%
22. Percentage of parking area to be covered by landscaping: 10.3%
23. Percentage of site to remain undeveloped: 56.5%
24. Is the applicant proposing any terms, conditions, covenants and agreements or other documents regarding the intended development: (If yes, attach copies)  
 yes  no  unknown
25. List any other permits for this project from state, federal or local governmental agencies for which you have applied or will apply, including the name of the issuing agency, whether the permit has been applied for, and if so, the date the application was approved or denied, and the application or permit number:  
Conditional Use Permit Approval, SEPA Determination, Building Permit:  
City of Bainbridge Island  
NPDES Permit: Washington State Department of Ecology
26. Will the completed project result in 800 or more square feet of impervious surface (building footprint + driveways + parking)?  yes  no  unknown
27. Will the project result in clearing more than six significant trees or 2,500 square feet of ground?  yes  no  unknown

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28. Do storm water systems exist on the site?  yes  no  unknown

If yes, were they constructed after 1982?  yes  no  unknown

If yes, what type of storm water system exists on the site?

infiltration  open ditching  closed conveyance  detention

29. Will the completed project result in excavating of or filling in:

less than 50 cubic yards.  more than 50 cubic yards but less than 100 cubic yards.  more than 100 cubic yards.

I hereby certify that I have read this application and know the same to be true and correct.

*[Handwritten Signature]*

\*Signature of owner or authorized agent

*January 14, 2016*

Date

*Michael Chen*

Please Print

\*If signatory is not the owner of record, the attached "Owner/Agent Agreement" must be signed and notarized.

# SUBMITTAL REQUIREMENTS FOR SITE PLAN AND DESIGN REVIEW APPLICATION

**Submittal Documents.** Application for site plan and design review may be made after the completion of a pre-application conference. Applications must be submitted by the owner or others authorized by the owner in person at the City of Bainbridge Island, Department of Planning and Community Development. A complete application shall include the items listed below (unless waived in writing by the Director or Project Manager).

- An application form provided by the City with the notarized signatures of all property owners or an owner/applicant agreement with the notarized signatures of all property owners.
- An application fee in the amount specified by the City.
- A complete and detailed written statement of the intended use of the land, the sequence and timing of the proposed development and the applicant's intentions with regard to the proposed ownership of land areas, dwelling units, if any, and commercial buildings within the development, both during and after construction. The statement shall include the following:
  - Intended use of the land, including the phasing of street improvements, including road construction, acreage of road area and percentage it represents of the total land area;
  - The availability and phasing of water system extensions;
  - The method of sewage disposal to include the availability and phasing of system extensions; and
  - Phasing of on- and off-site requirements such as sidewalks, bike paths, or improvements of adjacent streets;
- A site plan in compliance with the requirements in "Drawing Content" section below;
- Architectural drawings, including floor plan, building elevation and building sections;
- Analysis of how the project meets design guidelines, if applicable;
- Storm drainage design;
- The proposed method of ensuring permanent retention and maintenance of circulation system, storm drainage system, sewage disposal system, parks, open space or other common private or public facilities;
- Landscaping Plan
- The terms, conditions, covenants, and agreements under which the subject property is bound, if any;
- An environmental checklist when required by the State Environmental Policy Act and BIMC 16.04;
- Land dedicated for park and recreational facilities, if applicable;
- For developments within the Business/Industrial (B/I) district, designated open space areas required pursuant to BIMC 18.06.060.C.1; and
- Any other graphic materials required to adequately describe how the proposal meets specific regulations and guidelines, such as color palettes, material boards, and computerized models;
- Other plans and information deemed necessary by the director for evaluation of the merits of the proposal; and
- For installation of outdoor lighting fixtures, evidence of compliance with the requirements of chapter 18.15.040, Outdoor Lighting, on a form provided by the department of planning and community development.

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## Drawing Format

**Required size: 18" x 24"**

**Minimum scale: 1" = 100'**

**Border: 1" minimum all sides**

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# SUBMITTAL REQUIREMENTS FOR SITE PLAN AND DESIGN REVIEW APPLICATION

## Identification Information (to be included on each page of each drawing)

- \_\_\_ 1. Project title
- \_\_\_ 2. Space for the Dept. of Planning and Community Development's application number
- \_\_\_ 3. Name of property owner(s).
- \_\_\_ 4. Sheet title (road, drainage, grading, utility, temporary erosion and sedimentation control, site constraints, etc.)
- \_\_\_ 5. Revision block
- \_\_\_ 6. Quarter Section, Section, Township and Range in which property is located.
- \_\_\_ 7. Date drawings were prepared.
- \_\_\_ 8. Page numbers and total number of pages.
- \_\_\_ 9. Name, address, phone number, and E-mail address of professional who prepared the drawing.
- \_\_\_ 10. North arrow with north at top or left side of sheet
- \_\_\_ 11. Graphic scale.
- \_\_\_ 12. Elevation datum and benchmark

## Drawing Content

- \_\_\_ 1. Vicinity map showing the proposed project site, easements and major city streets. Map shall, at a minimum, show adjacent streets and lots in sufficient manner to reasonably locate the site.
- \_\_\_ 2. Location, name, width, and ownerships of all existing and proposed boundaries, streets, roads, rights-of-way, or easements on or adjacent to the subject property.
- \_\_\_ 3. Circulation plans on and off the site, including pedestrian, bicycle and transit access, delineated parking spaces, location of locking bicycle space, etc.
- \_\_\_ 4. Location of all existing structures and improvements on or adjacent to the subject property.
- \_\_\_ 5. Location of proposed improvements.
- \_\_\_ 6. Utilities plans.
- \_\_\_ 7. Location of all existing vegetation, including all trees over six inches in diameter, on the subject property.
- \_\_\_ 8. Topography at appropriate contour intervals.
- \_\_\_ 9. Location of all existing watercourses, slopes, critical areas and other natural features on the subject property.
- \_\_\_ 10. Any other plans deemed necessary for evaluation.



CITY OF BAINBRIDGE ISLAND

CONDITIONAL USE PERMIT APPLICATION

FORM MUST BE COMPLETED IN INK, PREFERABLY BLUE.  
PENCIL WILL NOT BE ACCEPTED.



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	<p>PROJECT NAME: BAINBRIDGE ISLAND FIRE DEPARTMENT, STATION 21</p> <p>TAX ASSESSOR'S NUMBER: <u>222502-1-023-2000</u> <u>4169-000-050-0206</u></p> <hr/> <p>PROJECT STREET ADDRESS OR ACCESS STREET: <u>8895 MADISON AVENUE N</u></p> <p>ENVIRONMENTAL CHECKLIST SUBMITTED: <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO</p>
	<p><b><u>FOR CITY USE ONLY</u></b></p>
	<p>FILE NUMBER:</p> <p>PROJECT NUMBER:</p> <p>DATE RECEIVED:</p> <p>APPLICATION FEE:</p> <p>TREASURER'S RECEIPT NUMBER:</p>

SUBMITTAL REQUIREMENTS	
APPLICATION	<i>One original (which must contain an original signature) and six copies</i> must be provided. Whenever possible, originals must be <i>signed in blue</i> . Please identify the original document.
SUPPORTING DOCUMENTS	<i>One original (which must contain an original signature)</i> , where applicable, <i>and six copies</i> (if an original is not applicable, <i>seven copies</i> must be provided).
FULL-SIZE DRAWINGS	<i>Seven copies</i> of the required drawings must be provided. Drawings <i>must be folded and 18" x 24"</i> in size. <i>No construction drawings or other sized drawings</i> will be accepted unless specifically requested.
REDUCED DRAWINGS	<i>Two copies (five if commercial)</i> of the drawings reduced to 11" x 17" must be provided.
SUBMITTING APPLICATIONS	Applications <i>must be submitted in person</i> by either the owner or the owner's designated agent. Should an agent submit the application, a <i>notarized Owner/Agent Agreement</i> must accompany the application. Please call (206) 780-3762 to make an appointment to submit your application.
FEES	Please call the Department of Planning & Community Development for submittal fee information.
ATTACHED SUBMITTAL CHECKLIST	Please refer to attached Submittal Checklist for further information. <b>NOTE:</b> when submitting this application, please do not copy or include the Submittal Checklist sheets attached to the back of this application.

**APPLICATIONS WILL NOT BE ACCEPTED  
unless these basic requirements are met and the submittal packet is deemed counter complete.**

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT  
280 MADISON AVENUE NORTH • BAINBRIDGE ISLAND, WA • 98110-1812  
PHONE: (206) 842-2555 ibridgewa.gov

CITY OF BAINBRIDGE ISLAND

CONDITIONAL USE PERMIT APPLICATION

FORM MUST BE COMPLETED IN INK, PREFERABLY BLUE.

PENCIL WILL NOT BE ACCEPTED.



A. GENERAL INFORMATION

Please indicate which of the following is the reason for which you are applying:

\_\_\_\_\_ Height  Use

1. Name of property owner: Bainbridge Island Fire Department
Address: 8895 Madison Avenue N, Bainbridge Island, WA 98110
Phone: (206) 842-7686 Fax:
E-mail:

Name of property owner:
Address:
Phone: Fax:
E-mail:

Name of property owner:
Address:
Phone: Fax:
E-mail:

If the owner(s) of record as shown by the county assessor's office is (are) not the agent, the owner's (owners') signed and notarized authorization(s) must accompany this application.

2. Authorized agent: Michael Chen, Mackenzie
Address: Logan Building, 500 Union Street, Suite 545, Seattle, WA 98101
Phone: (206) 749-9993, Extension 513 Fax: (206) 749-5565
E-mail: MChen@mcknze.com

3. Person responsible for payment: Luke Carpenter, Assistant Chief, Bainbridge Island Fire Department
Address: 8895 Madison Avenue N, Bainbridge Island, WA 98110
Phone: (206) 451-2033 Fax:
E-mail: Lcarpenter@bifd.org

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4. Project contact: Michael Chen, Mackenzie  
 Address: Logan Building, 500 Union Street, Suite 545, Seattle, WA 98101  
 Phone: (206) 749-9993, Extension 513 Fax: (206) 749-5565  
 E-mail: MChen@mcknze.com

5. Name of land surveyor: Adam & Goldsworthy, Inc.  
 Address: 1015 NE Hostmark Street, Poulsbo, WA 98370  
 Phone: (360) 779-4299 Fax: (360) 779-4213  
 E-mail: info@agols.com

6. Planning department personnel familiar with site: Nan Gladstein, Josh Machen

7. Description of proposal: Redevelopment of existing fire station as two-story, approximately 30,760-square foot building with living quarters, meeting spaces, offices, storage areas, and an apparatus bay, with associated site improvements. Existing memorial to be preserved, flagpoles to be relocated. Existing helicopter landing pad and fence in southern portion of site to be preserved.

8. Driving directions to site: The site is accessible from Madison Avenue N to the east.

9. Please give the following existing parcel information:

Assessor's Parcel Number	Parcel Owner	*Lot Area
222502-1-023-2000	City of Bainbridge Island	2.8 ac.
4169-000-050-0206	City of Bainbridge Island	1.1 ac.
Use additional sheet if necessary	Total of all parcels:	3.91

\* As defined in Bainbridge Island Municipal Code 18.12.050

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10. Legal description (or attach): 22252E THE E 208FT OF S1/2 SE1/4 NE1/4 PER  
SC NO.68005 TGW WLY 30FT OF TH PTN OF VAC CO RD PER CJ 13  
PAGE 129

11. Current comprehensive plan, zoning and shoreline designations and use of subject parcel(s):

Lot Number	Comp Plan Designation	Zoning Designation	Shoreline Designation	Current Use
Lot 222502-1-023-2000	OSR-2	R-2		Existing fire station
Lot 4169-000-050-0206	OSR-2	R-2		Existing fire station
Lot				
Lot				

12. Current comprehensive plan, zoning and shoreline designations and use of adjacent properties:

Property	Comp Plan Designation	Zoning Designation	Shoreline Designation	Current Use
North	OSR-2	R-2		Residential subdivision
South	CZ-2	B/I		Commercial storage
East	OSR-2	R-2		Church
West	B/I	B/I		Residential

13. Common name of adjacent water area or wetlands area: \_\_\_\_\_ N/A

14. Does the site contain an environmentally sensitive area as defined in Critical Areas Ordinance (*Bainbridge Island Municipal Code Chapter 16.20*)?  yes  no  unknown

If yes, check as appropriate:

<input type="checkbox"/> wetland*	<input type="checkbox"/> geologically hazardous area**
<input type="checkbox"/> wetland buffer*	<input type="checkbox"/> zone of influence**
<input type="checkbox"/> stream*	<input type="checkbox"/> slope buffer**
<input type="checkbox"/> stream buffer*	<input type="checkbox"/> fish and wildlife habitat area

\* If your site includes a wetland or wetland buffer, a wetland report is required with your application.

\*\* If your site includes a geologically hazardous area or is within the zone of influence as defined in *Bainbridge Island Municipal Code 16.20*, a geotechnical report is required with your application.

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15. Are there underlying/overlying agreements on the property?  yes  no  unknown  
 If yes, check as appropriate and provide a copy of the decision document: Please see list of land use decisions included with Conditional Use Permit Narrative.

<input checked="" type="checkbox"/> CUP Conditional Use Permit	<input checked="" type="checkbox"/> SPR Site Plan Review
<input type="checkbox"/> MPD Master Planned Development	<input type="checkbox"/> SPT Short Plat
<input type="checkbox"/> PUD Planned Unit Development	<input type="checkbox"/> SSDP Shoreline Permit
<input type="checkbox"/> REZ Contract Rezone	<input type="checkbox"/> SUB Prior Subdivision
<input type="checkbox"/> RUE Reasonable Use Exception	<input type="checkbox"/> VAR Zoning Variance
	<input type="checkbox"/> Other:

Under which jurisdiction was the approval given?

City of Bainbridge Island  Kitsap County

Approval date: Varies

16. Is there any other information which is pertinent to this project?  yes  no

If yes, please explain: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

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**B. TECHNICAL INFORMATION**

1. Name of water purveyor: City of Bainbridge Island  
If a private well, what class? N/A

2. Type of sewage disposal:  on-site septic  off-site septic  sewer  
Sewer district:  City of Bainbridge Island  Sewer District 7

3. General description of the existing terrain: The site is relatively flat, fully developed, with no known geological concerns.

4. Soil survey classification: Geotechnical information on the site is available from Aspect Consulting; soil types found on the site include sand, gravel, and silt.

5. Flood plain designation:  X  AE

6. Access (street functional road classifications):

Street Type	Required ROW Width	Street Name	Existing ROW Width
primary arterial	150 feet	Highway 305	150 feet (assumed)
secondary arterial	60 feet	Madison Avenue N & NE New Brooklyn Road	60 feet
collector	50 feet		
residential urban	40 feet		
residential suburban	30 feet		
private	20 - 30 feet		

7. Sidewalks are adjacent to the parcel:  yes  no  
If yes, existing sidewalks are \_\_\_\_\_ feet wide.  
Sidewalk installation is proposed as part of the development project:  yes  no

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Proposed sidewalks: [ ] adjacent to the parcel and are to be [ ] feet wide.
[ ] internal to the proposal and are to be 5-7 feet wide.

8. Intended use of the land, as well as the sequence and timing of the proposed development:
The land is proposed for redevelopment under the same use as a fire station.
Pending the required approvals, construction is expected to start in the fall of
2016 and to be complete in 2018.

9. Dimensions of proposed structures: Proposed GSF: 30,760 SF; proposed lot
coverage: 22,270 SF/13.1%

10. Height of proposed buildings or structures: 34'-0" (maximum, highest
overhang/roofline)

11. Square footage of all spaces:
retail: N/A storage: 3,289 square feet
office: 8,640 square feet residential: 4,920 square feet
other: 13,977 square feet (apparatus bay and support spaces)

12. Number of stories proposed: 2

13. Square feet per story: (1) 22,270 square feet (2) 8,490 square feet (3) N/A

14. Setback requirements:
north: 5-15 feet south: 5-15 feet
east: 25 feet west: 15 feet

15. Number of parking stalls required: 40

16. Number of parking stalls proposed: 40

17. Amount of square footage of proposed paved areas: 51,874 square feet

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18. Square footage of building area: 22,270 square feet

19. Percent of site to be covered by impervious surfaces: 43.5%  
(If the proposal results in more than 1,000 square feet of additional impervious surface, a drainage plan shall be required.)

20. Percentage of site to be covered by landscaping: 56.5%

21. Percentage of parking area to be covered by landscaping: 10.3%

22. Percentage of site to remain undeveloped: 56.5%

23. Is the applicant proposing any terms, conditions, covenants and agreements or other documents regarding the intended development: (If yes, attach copies)  yes  no  unknown

24. Is the proposal part of a phased development plan? (If so, an outline of the future plans must be submitted.)  
N/A

25. List any other permits for this project from state, federal or local governmental agencies for which you have applied or will apply, including the name of the issuing agency, whether the permit has been applied for, and if so, the date the application was approved or denied, and the application or permit number:  
Site Plan and Design Review Approval, SEPA Determination, Building Permit:  
City of Bainbridge Island  
NPDES Permit: Washington State Department of Ecology

26. Will the completed project result in 800 or more square feet of impervious surface (building footprint + driveways + parking)?  yes  no  unknown

27. Will the project result in clearing more than six significant trees or 2,500 square feet of ground?  yes  no  unknown

28. Do storm water systems exist on the site?  yes  no  unknown

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If yes, were they constructed after 1982?

yes  no  unknown

If yes, what type of storm water system exists on the site?

infiltration  open ditching  closed conveyance  detention

29. Will the completed project result in excavating of or filling in:

less than 50 cubic yards.  more than 50 cubic yards but less than 100 cubic yards.  more than 100 cubic yards.

C. Conditional Use Permit Criteria

In accordance with Chapter 2.16.050 and 2.16.110 of the City of Bainbridge Island Municipal Code, the applicant must answer the following questions:

1. In what manner is the requested conditional use harmonious and compatible in design, character and appearance with the existing or intended character and quality of development in the vicinity of the subject property and with the physical characteristics of the subject property?

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

2. How has the proposed conditional use made adequate provision for the following facilities:

- a) roads; Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.
- b) water availability;
- c) fire protection;
- d) sewage disposal facilities, and
- e) storm drainage facilities.

3. Describe how the proposed conditional use will **not** be materially detrimental to uses or property in the vicinity of the subject property:

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

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**CONDITIONAL USE PERMIT APPLICATION**

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4. Relate how the proposed conditional use will meet all the criteria otherwise applicable to the zone in which it is to be developed:

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

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5. How is the conditional use in conformance with the Comprehensive Plan?

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

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6. How does the conditional use comply with all of the other provisions of the city code?

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

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7. Describe how the conditional use will not adversely affect the area or alter the area's predominantly residential nature?

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

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8. How is the subject property suitable for the proposed conditional use and will it be detrimental to surrounding land uses or sensitive areas? Consideration shall include items such as topography, streets and adjacent land uses:

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

9. Describe how all necessary measures have been taken to eliminate the impacts that issuance of the conditional use permit may have on the area in which it is to be located:

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

**I hereby certify that I have read this application and know the same to be true and correct.**

*[Handwritten Signature]*  
\*Signature of owner or authorized agent

1/13/16  
Date

L. Carpenter  
Please Print

*\*If signatory is not the owner of record, the attached "Owner/Agent Agreement" must be signed and notarized.*

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**CONDITIONAL USE PERMIT APPLICATION**

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**SUBMITTAL DOCUMENTS**

Application for conditional use permit may be made after the completion of a pre-application conference. Applications must be submitted by the owner or others authorized by the owner in person at the City of Bainbridge Island, Department of Planning and Community Development. A complete application shall include the items listed below (unless waived in writing by the Director or Project Manager).

- An application form provided by the City with the notarized signatures of all property owners or an owner/applicant agreement with the notarized signatures of all property owners.
- An application fee in the amount specified by the City.
- A complete and detailed written statement of the intended use of the land and the sequence and timing of the proposed development;
- A site plan in compliance with the requirements in "Drawing Content" section below;
- The terms, conditions, covenants, and agreements under which the subject property is bound, if any;
- An environmental checklist when required by the State Environmental Policy Act and BIMC 16.04;
- Land dedicated for park and recreational facilities, if applicable;
- Other plans and information deemed necessary by the director for evaluation of the merits of the proposal; and
- Institutional development in residential zones shall submit the following items:
  - A traffic report, showing the effects on level of service (LOS) on affected roads;
  - The appropriate approvals of sewer and water supply;
  - A fencing plan or alternative methods to protect the public health, safety and welfare;
  - Vehicular, pedestrian, and bicycle access and site circulation; and
  - A site and building design proposal that meets the design principles and guidelines found in BIMC 18.18.030.C, Commercial/Mixed Use design guidelines.
- For installation of outdoor lighting fixtures, evidence of compliance with the requirements of chapter 18.15.040, Outdoor Lighting, on a form provided by the department of planning and community development.

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**Drawing Format**

**Required Size: 18" x 24"**

**Minimum scale: 1" = 100'**

**Border: 1" minimum all sides**

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CITY OF BAINBRIDGE ISLAND

**CONDITIONAL USE PERMIT APPLICATION**

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**Identification Information** (to be included on each page of each drawing)

1. Project title.
2. Space for the Dept. of Planning and Community Development's application number.
3. Name of property owner(s).
4. Sheet title (road, drainage, grading, utility, temporary erosion and sedimentation control, site constraints, etc.)
5. Revision block
6. Quarter Section, Section, Township and Range in which property is located.
7. Date drawings were prepared.
8. Page numbers and total number of pages.
9. Name, address, phone number, and E-mail address of professional who prepared the drawing.
10. North arrow with north at top or left side of sheet
11. Graphic scale.
12. Elevation datum and benchmark

**Drawing Content**

1. Vicinity map showing the proposed project site, easements and major city streets. Map shall, at a minimum, show adjacent streets and lots in sufficient manner to reasonably locate the site.
2. Location, name, width, and ownerships of all existing and proposed boundaries, streets, roads, rights-of-way, or easements on or adjacent to the subject property.
3. Circulation plans on and off the site, including pedestrian, bicycle and transit access, delineated parking spaces, location of locking bicycle space, etc.
4. Location of all existing structures and improvements on or adjacent to the subject property.
5. Location of proposed improvements.
6. Utilities plans.
7. Location of all existing vegetation, including all trees over six inches in diameter, on the subject property.
8. Landscaping plan. (See BIMC 18.15.010 for details.)
9. Contours at a maximum interval of five (5) feet.
10. Location of all existing watercourses, slopes, wetlands, required buffers, critical areas regulated under BIMC 16.20, and other natural features on the subject property.
11. Any other plans deemed necessary for evaluation.

CITY OF BAINBRIDGE ISLAND

## CONDITIONAL USE PERMIT APPLICATION

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**Additional requirements for educational, cultural, governmental, religious or health care facilities in residential zones.** BIMC 2.16.110.E requires educational, cultural, governmental, religious or health care facilities in residential zones to be processed as major (as opposed to minor) conditional use permit applications. When an application is made for one of these facilities, the following material must be included as part of the submittal:

- A traffic report showing the effects of the proposed project on level of service (LOS) on affected roads and including proposed mitigation measures for degradation of the LOS.
- Evidence that the proposed project will front on a road classified as Residential Suburban or above on the Bainbridge Island Functional Road Classification Map.
- Evidence that noise levels shall be in compliance with 16.16.020 and 16.16.040.A.
- Approved water and sewer service agreements.
- A fencing plan or alternative methods to protect the public health, safety, and welfare must be submitted at the time of application.
- Evidence that the proposal will be limited to no more than fifty percent (50%) of the allowable lot coverage in the zone in which it is to be located.
- A site plan showing perimeter buffers, if the proposal is to be sited in a residential district outside Winslow.



Aerial Image – Project Site



ATTACHMENT D

**I. PROJECT SUMMARY**

**Applicant:** Bainbridge Island Fire Department  
8895 Madison Avenue N  
Bainbridge Island, WA 98110

**Owner:** Bainbridge Island Fire Department  
8895 Madison Avenue N  
Bainbridge Island, WA 98110

**Site Address:** 8895 Madison Avenue N  
Bainbridge Island, WA 98110

**Assessor Site Acreage:** 3.91 acres

**Zoning:** 2 Units per Acre Zone (20,000 square feet; R-2)

**Comprehensive Plan:** 2 Units per Acre Zone (20,000 square feet; OSR-2)

**Adjacent Zoning:** R-2 to the north and east and Business/Industrial District (B/I) to the south and west

**Existing Structures:** Fire station, living quarters, generator building, circulation and parking areas, helicopter landing pad, and other site improvements

**Request:** Site Plan and Design Review Approval

**Project Contact:** Mackenzie, Applicant's Representative  
Michael Chen, Senior Planner  
Logan Building  
500 Union Street, Suite 545  
Seattle, WA 98101  
(206) 749-9993, Extension 513  
MChen@mcknze.com

**ATTACHMENT E**

## II. INTRODUCTION

### Description of Request

The applicant, Bainbridge Island Fire Department (BIFD), seeks approval of site plan and design review for the proposed redevelopment of the existing fire station, Station 21, at 8895 Madison Avenue N in Bainbridge Island, Washington.

As articulated in the City of Bainbridge Island Comprehensive Plan, one of the City's primary goals is to "[p]rovide adequate public facilities [...] which maximize public safety and minimize adverse environmental impacts." In a Strategic Plan Update, the BIFD identified critical needs at Station 21 and recommended a formal needs assessment, which the applicant's representative, Mackenzie, was involved in producing. The proposed redevelopment of Station 21 will ensure not only that the buildings meet the BIFD's operational needs, but also that the redeveloped fire station will serve the needs of the Bainbridge Island community for decades to come.

Community members will be involved in the redevelopment of the fire station through participation in the community meeting to be held in the initial stages of the review process. The applicant supports this initiative to develop a meaningful process for citizen participation and looks forward to engaging the community's vision for Station 21.

### Existing Site & Surrounding Land Use

The subject site is located northwest of Madison Avenue N, southwest of Highway 305, and north of NE New Brooklyn Road in Bainbridge Island. The site comprises two tax lots, 4169-000-050-0206 and 222502-1-023-2000.

There are approximately four existing buildings on the site, including a fire station, living quarters, a generator building, and an accessory building; there are a memorial, a helicopter landing pad, a fence, and vehicle circulation areas on the site, as well.

The site and the tax lots to the north and east are zoned 2 Units per Acre Zone (20,000 square feet; R-2), and the tax lots to the south and west are zoned Business/Industrial District (B/I). The site comprises approximately 3.91 acres. The site is currently in use by the BIFD as the existing Station 21.

Existing land use decisions on the site include the following:

- Kitsap County Conditional Use Permit 936A (4/26/1977)
- Zoning Variance VAR10-05-93-1
- Conditional Use Permit CUP05-14-99-1
- Conditional Use Permit CUP11791 (11/21/2002)

### Description of Proposed Development

The land is proposed for redevelopment under the same use as a fire station. Pending the required approvals, construction is expected to start in the fall of 2016 and to be complete in 2018. The subject property is and will continue to be owned by the BIFD. No permanent dwelling units or commercial buildings will be provided within the development either during or after construction.

**III. NARRATIVE & COMPLIANCE**

The land is proposed for redevelopment under the same use as a fire station. The site is currently served by Madison Avenue N to the east. The existing size of Madison Avenue N meets the required right-of-way width of 60 feet. No public street improvements or road construction are proposed for this development. The existing driveway will be redeveloped as two separate driveways, both approximately 26 feet wide, one at the southern end of the existing driveway and one at the northern end of the existing driveway. Landscape area will separate the two newly created driveways from the emergency access area to be used by the fire apparatus and passenger vehicles.

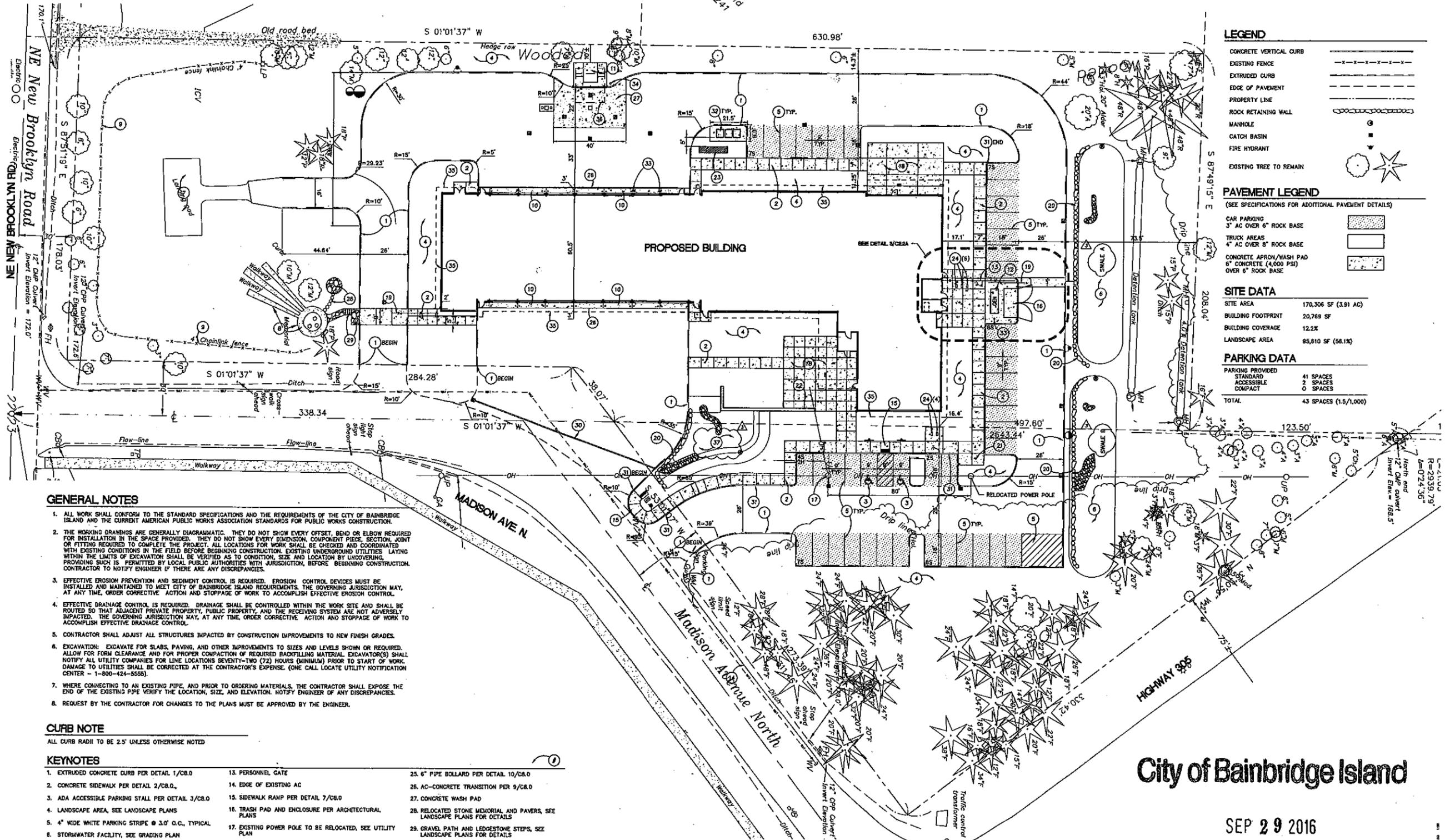
The site is currently served by public water utility infrastructure provided by the City of Bainbridge Island that is available and adequate to meet the needs of the proposed development; therefore, no new water system extensions are proposed. The site is also currently served by public sewer utility infrastructure provided by the City of Bainbridge Island that is available and adequate to meet the needs of the proposed development; therefore, no new sewer system extensions are proposed.

Internal sidewalks ranging in width from five to seven feet are proposed with the development. These sidewalks will be constructed with the redevelopment of the existing fire station and with the associated site improvements. No off-site improvements are proposed. Pending the required approvals, construction is expected to start in the fall of 2016 and to be complete in 2018.

**IV. CONCLUSION**

The applicant submits this narrative and the attached Exhibits in seeking approval of site plan and design review for the proposed redevelopment of the existing fire station located at 8895 Madison Avenue N. This application demonstrates that the proposal is in compliance with the relevant standards provided by the City of Bainbridge Island.





**LEGEND**

CONCRETE VERTICAL CURB	---
EXISTING FENCE	---
EXTRUDED CURB	---
EDGE OF PAVEMENT	---
PROPERTY LINE	---
ROCK RETAINING WALL	---
MANHOLE	○
CATCH BASIN	○
FIRE HYDRANT	○
EXISTING TREE TO REMAIN	★

**PAVEMENT LEGEND**  
(SEE SPECIFICATIONS FOR ADDITIONAL PAVEMENT DETAILS)

CAR PARKING 3" AC OVER 6" ROCK BASE	▨
TRUCK AREAS 4" AC OVER 8" ROCK BASE	▨
CONCRETE APRON/WASH PAD 8" CONCRETE (4,000 PSI) OVER 6" ROCK BASE	▨

**SITE DATA**

SITE AREA	170,306 SF (3.91 AC)
BUILDING FOOTPRINT	20,769 SF
BUILDING COVERAGE	12.2%
LANDSCAPE AREA	95,810 SF (58.1%)

**PARKING DATA**

PARKING PROVIDED	41 SPACES
STANDARD	41 SPACES
ACCESSIBLE	0 SPACES
COMPACT	0 SPACES
<b>TOTAL</b>	<b>43 SPACES (1.5/1,000)</b>

**GENERAL NOTES**

- ALL WORK SHALL CONFORM TO THE STANDARD SPECIFICATIONS AND THE REQUIREMENTS OF THE CITY OF BAINBRIDGE ISLAND AND THE CURRENT AMERICAN PUBLIC WORKS ASSOCIATION STANDARDS FOR PUBLIC WORKS CONSTRUCTION.
- THE WORKING DRAWINGS ARE GENERALLY DIAGRAMMATIC. THEY DO NOT SHOW EVERY OFFSET, BEND OR ELBOW REQUIRED FOR INSTALLATION IN THE SPACE PROVIDED. THEY DO NOT SHOW EVERY DIMENSION, COMPONENT PIECE, SECTION, JOINT OR FITTING REQUIRED TO COMPLETE THE PROJECT. ALL LOCATIONS FOR WORK SHALL BE CHECKED AND COORDINATED WITH EXISTING CONDITIONS IN THE FIELD BEFORE BEGINNING CONSTRUCTION. EXISTING UNDERGROUND UTILITIES LAYING WITHIN THE LIMITS OF EXCAVATION SHALL BE VERIFIED AS TO LOCATION, SIZE AND LOCATION BY UNCOVERING, PROVIDING SUCH IS PERMITTED BY LOCAL PUBLIC AUTHORITIES WITH JURISDICTION, BEFORE BEGINNING CONSTRUCTION. CONTRACTOR TO NOTIFY ENGINEER IF THERE ARE ANY DISCREPANCIES.
- EFFECTIVE EROSION PREVENTION AND SEDIMENT CONTROL IS REQUIRED. EROSION CONTROL DEVICES MUST BE INSTALLED AND MAINTAINED TO MEET CITY OF BAINBRIDGE ISLAND REQUIREMENTS. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE EROSION CONTROL.
- EFFECTIVE DRAINAGE CONTROL IS REQUIRED. DRAINAGE SHALL BE CONTROLLED WITHIN THE WORK SITE AND SHALL BE ROUTED SO THAT ADJACENT PRIVATE PROPERTY, PUBLIC PROPERTY, AND THE RECEIVING SYSTEM ARE NOT ADVERSELY IMPACTED. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE DRAINAGE CONTROL.
- CONTRACTOR SHALL ADJUST ALL STRUCTURES IMPACTED BY CONSTRUCTION IMPROVEMENTS TO NEW FINISH GRADES.
- EXCAVATION: EXCAVATE FOR SLABS, PAVING, AND OTHER IMPROVEMENTS TO SIZES AND LEVELS SHOWN OR REQUIRED. ALLOW FOR FORM CLEARANCE AND FOR PROPER COMPACTION OF REQUIRED BACKFILLING MATERIAL. EXCAVATOR(S) SHALL NOTIFY ALL UTILITY COMPANIES FOR LINE LOCATIONS SEVENTY-TWO (72) HOURS (MINIMUM) PRIOR TO START OF WORK. DAMAGE TO UTILITIES SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE. (ONE CALL LOCATE UTILITY NOTIFICATION CENTER - 1-800-424-5555).
- WHERE CONNECTING TO AN EXISTING PIPE, AND PRIOR TO ORDERING MATERIALS, THE CONTRACTOR SHALL EXPOSE THE END OF THE EXISTING PIPE TO VERIFY THE LOCATION, SIZE, AND ELEVATION. NOTIFY ENGINEER OF ANY DISCREPANCIES.
- REQUEST BY THE CONTRACTOR FOR CHANGES TO THE PLANS MUST BE APPROVED BY THE ENGINEER.

**CURB NOTE**

ALL CURB RADII TO BE 2.5' UNLESS OTHERWISE NOTED

**KEYNOTES**

- |   |   |  |
|---|---|--|
| 1. EXTRUDED CONCRETE CURB PER DETAIL 1/CB.0   | 13. PERSONNEL GATE  | 25. 6" PIPE BOLLARD PER DETAIL 10/CB.0   |
| 2. CONCRETE SIDEWALK PER DETAIL 2/CB.0  | 14. EDGE OF EXISTING AC   | 26. AC-CONCRETE TRANSITION PER 9/CB.0  |
| 3. ADA ACCESSIBLE PARKING STALL PER DETAIL 3/CB.0   | 15. SIDEWALK RAMP PER DETAIL 7/CB.0   | 27. CONCRETE WASH PAD  |
| 4. LANDSCAPE AREA, SEE LANDSCAPE PLANS  | 16. TRASH PAD AND ENCLOSURE PER ARCHITECTURAL PLANS   | 28. RELOCATED STONE MEMORIAL AND PAVERS, SEE LANDSCAPE PLANS FOR DETAILS           |
| 5. 4" WIDE WHITE PARKING STRIPE @ 3.0' O.C., TYPICAL  | 17. EXISTING POWER POLE TO BE RELOCATED, SEE UTILITY PLAN                                   | 29. GRANUL PATH AND LEDGESTONE STEPS, SEE LANDSCAPE PLANS FOR DETAILS              |
| 6. STORMWATER FACILITY, SEE GRADING PLAN  | 18. PLAZA AREA, SEE GRADING PLAN FOR DETAILS  | 30. TRENCH DRAIN - SEE UTILITY PLAN  |
| 7. EXISTING HELICOPTER PAD TO REMAIN  | 19. MOTORIZED ROLLING CHAIN-LINK GATE   | 31. STANDARD CURB PER DETAIL 11/CB.0   |
| 8. EXISTING MEMORIAL TO REMAIN, SEE LANDSCAPE PLANS FOR DETAILS                                     | 20. ROCK WALL, SEE GRADING PLAN   | 32. MECHANICAL VRF UNITS, SEE MECHANICAL PLANS                                     |
| 9. EXISTING FENCE TO REMAIN   | 21. TRANSFORMER PAD - 6" OF 3,500 PSI CONCRETE OVER 8" CRUSHED ROCK BASE WITH #4 @ 24" O.C. | 33. POST WITH ELECTRICAL OUTLET - SEE ELECTRICAL PLANS, PROVIDE BOLLARD PROTECTION |
| 10. CONCRETE APRON, SEE PAVING LEGEND   | 22. FLAG POLE WITH NEW CONCRETE FOOTING PER MANUFACTURER RECOMMENDATIONS                    | 34. POST HYDRANT PER DETAIL 7/CB.2   |
| 11. CONCRETE FUELING PAD WITH RELOCATED FUELING STATION PER DETAIL 2/CB.2                           | 23. PROPANE TANK  | 35. BUILDING ROOF LINE ABOVE   |
| 12. EMERGENCY GENERATOR PAD - 6" OF 3,500 PSI CONCRETE OVER 8" CRUSHED ROCK BASE WITH #4 @ 24" O.C. | 24. BIKE RACKS  | 36. RELOCATED CANOPY   |
|   |   | 37. EAST POND - LANDSCAPE FEATURE  |

City of Bainbridge Island

SEP 29 2016  
Planning and  
Community Development

ATTACHMENT G

MECHANICAL/ELECTRICAL  
INTERFACE ENGINEERING  
708 SW 3RD, SUITE 400  
PORTLAND, OR 97204

LANDSCAPE  
FISCHER BOUMA PARTNERSHIP  
310 MADISON AVENUE  
SOUTH SUITE A  
BAINBRIDGE ISLAND, WA  
98110



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Project  
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STATION 21**  
8885 MADISON AVE. NE  
BAINBRIDGE, WA 98110

Project  
**BAINBRIDGE ISLAND FIRE DEPT  
STATION 21**  
8895 MADISON AVE. NE



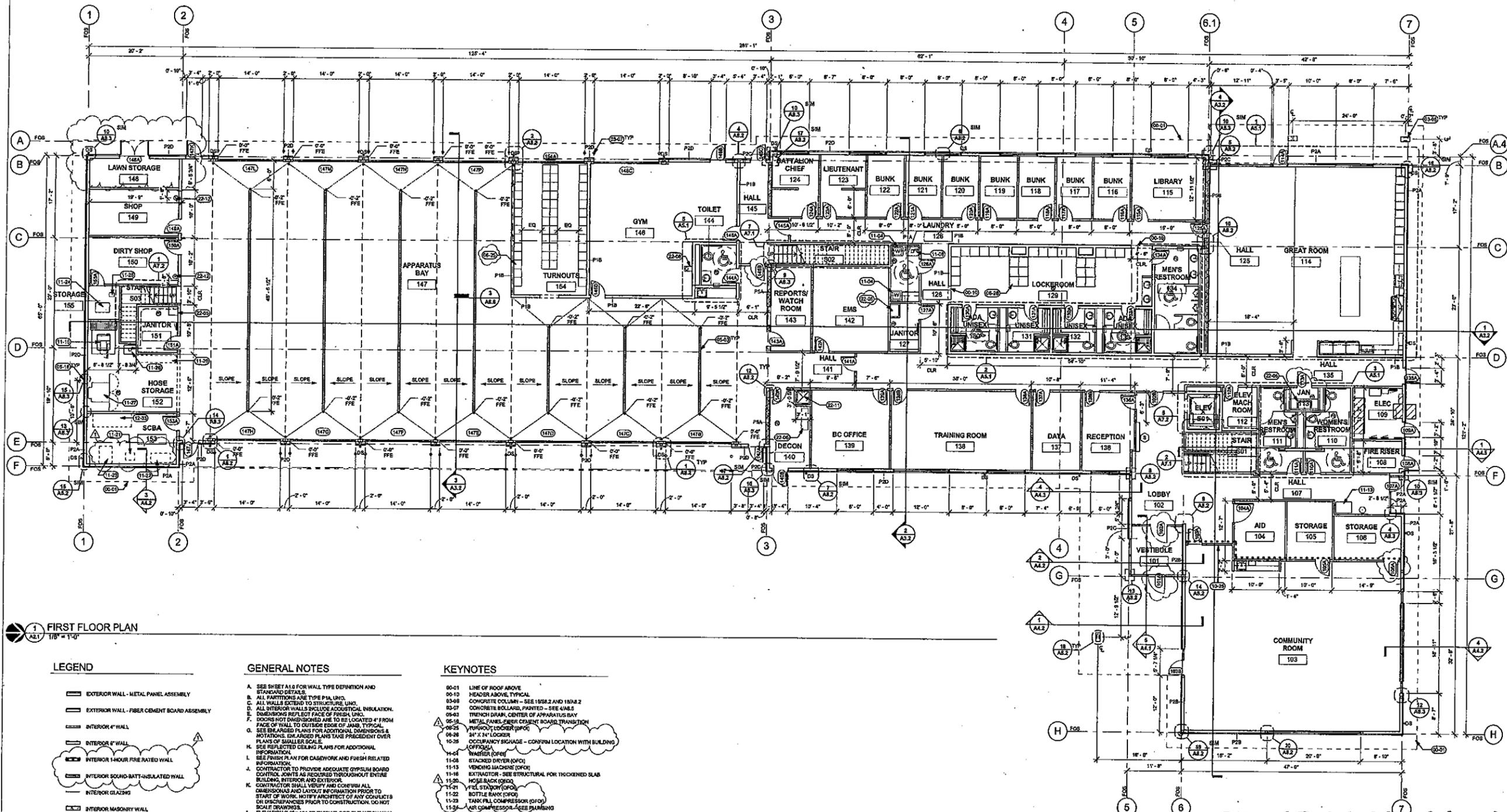
REVISIONS:

NO.	DESCRIPTION	DATE
1	X	8/12/2016
2	X	9/19/2016
3	X	IN PROGRESS

DATE: 9/14/16  
DRAWN BY: BTS  
CHECKED BY: TWM

SHEET TITLE:  
**SITE PLAN**

SHEET  
**C2.1**  
REVISED 08/12/16  
JOB NO. 2150124.00



**1 FIRST FLOOR PLAN**  
1/8" = 1'-0"

**LEGEND**

- EXTERIOR WALL - METAL PANEL ASSEMBLY
- EXTERIOR WALL - FIBER CEMENT BOARD ASSEMBLY
- INTERIOR 4' WALL
- INTERIOR 6' WALL
- INTERIOR 1-HOUR FIRE RATED WALL
- INTERIOR SOUND-BATT-INSULATED WALL
- INTERIOR GLAZING
- INTERIOR MASONRY WALL
- GRIDLINES
- KEYNOTES
- FACE OF STUD
- NON-OCCUPIABLE SPACE  
(ceiling height less than 7'-0")

**GENERAL NOTES**

- A. SEE SHEET A1.0 FOR WALL TYPE DEFINITION AND STANDARD DETAILS.
- B. ALL PARTITIONS ARE TYPE P1A, UNO.
- C. ALL WALLS EXTEND TO STRUCTURE UNO.
- D. ALL INTERIOR WALLS INCLUDE ACOUSTICAL INSULATION.
- E. DIMENSIONS REFLECT FACE OF FINISH UNO.
- F. DOORS NOT DIMENSIONED ARE TO BE LOCATED 4" FROM FACE OF WALL TO OUTSIDE EDGE OF JAMB, TYPICAL.
- G. SEE ENLARGED PLANS FOR ADDITIONAL DIMENSIONS & NOTATIONS. ENLARGED PLANS TAKE PRECEDENCE OVER PLANS OF SMALLER SCALE.
- H. SEE REFLECTED CEILING PLANS FOR ADDITIONAL INFORMATION.
- I. SEE FINISH PLAN FOR CASWORK AND FINISH RELATED INFORMATION.
- J. CONTRACTOR TO PROVIDE ADEQUATE GYPSUM BOARD CONTROL JOINTS AS REQUIRED THROUGHOUT ENTIRE BUILDING, INTERIOR AND EXTERIOR.
- K. CONTRACTOR SHALL VERIFY AND CORRECT ALL DIMENSIONS AND LAYOUT INFORMATION PRIOR TO START OF WORK. NOTIFY ARCHITECT OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO CONSTRUCTION. DO NOT SCALE DRAWINGS.
- L. ELEVATION OF 4" = 1/8" FINISH FLOOR ELEVATION MARK FLOOR INDICATED IN CIVIL DRAWINGS.

**KEYNOTES**

- 00-01 LINE OF ROOF ABOVE
- 00-10 HEADER ABOVE, TYPICAL
- 03-08 CONCRETE COLUMN - SEE 1805.2 AND 1806.2
- 03-07 CONCRETE BOLLARD, PAINTED - SEE 446A
- 05-03 TRENCH DRAIN, CENTER OF APPARATUS BAY
- 05-18 METAL PANEL FIBER CEMENT BOARD TRANSITION (W/GRIP) LOCKER (OFO)
- 06-28 24" X 4" LOCKER
- 10-25 OCCUPANCY SIGNAGE - CONFIRM LOCATION WITH BUILDING (OFO)
- 11-04 WASHER (OFO)
- 11-08 STACKED DRYER (OFO)
- 11-13 VENDING MACHINE (OFO)
- 11-18 EXTRACTOR - SEE STRUCTURAL FOR THICKENED SLAB
- 11-20 HOSE RACK (OFO)
- 11-21 VREL STATION (OFO)
- 11-22 BOTTLE BANK (OFO)
- 11-23 TANK FILL COMPRESSOR (OFO)
- 11-24 AIR COMPRESSOR (SEE DIMENSIONING)
- 11-25 AIR DRYER - SEE PLUMBING
- 11-26 DRYER (OFO)
- 11-27 HOSE DRYER CABINET (OFO)
- 12-03 WORKBENCH (OFO)
- 22-16 MOP SINK
- 22-26 UTILITY SINK
- 22-28 DRINKING FOUNTAIN
- 22-11 DECON SHOWER
- 22-12 EMERGENCY EYE WASH

**City of Bainbridge Island**

OCT 17, 2016  
**Planning and  
Community Development**

ATTACHMENT H

MECHANICAL/ELECTRICAL  
INTERFACE ENGINEERING  
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PORTLAND, OR 97204

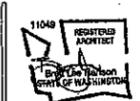
LANDSCAPE  
FISCHER BOUMA PARTNERSHIP  
310 MADISON AVENUE  
SOUTH, SUITE A  
BAINBRIDGE ISLAND, WA  
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BAINBRIDGE, WA 98110

**BAINBRIDGE ISLAND FIRE DEPT  
STATION 21**  
8995 MADISON AVE. NE



Revision Schedule	
Revision Delta	Issue Date
1	8/12/2016

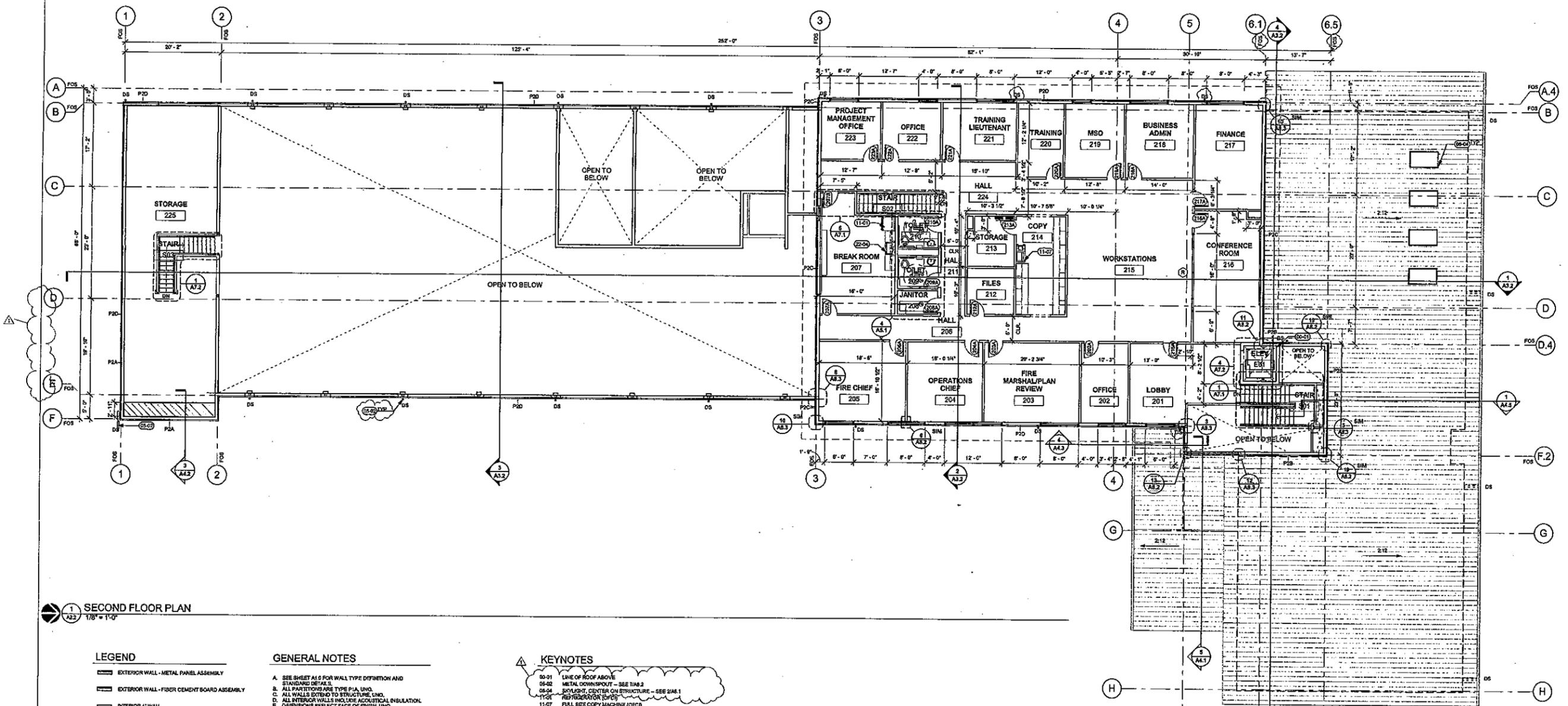
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CHECKED BY: BLH

SHEET TITLE:  
**FIRST FLOOR PLAN**

SHEET  
**A2.1**

REVISED 08/12/16 JOB NO. 2150124.00

PERMIT SET 06/30/16



1 SECOND FLOOR PLAN  
1/8" = 1'-0"

**LEGEND**

- EXTERIOR WALL - METAL PANEL ASSEMBLY
- EXTERIOR WALL - FIBER CEMENT BOARD ASSEMBLY
- INTERIOR 4' WALL
- INTERIOR 6' WALL
- INTERIOR 1-HOUR FIRE RATED WALL
- INTERIOR SOUND-BATT INSULATED WALL
- INTERIOR GLAZING
- INTERIOR MASONRY WALL
- GRIDLINES
- KEYNOTES
- FACE OF STUD
- NON-OCCUPIABLE SPACE (head height less than 7'-0")

**GENERAL NOTES**

- A. SEE SHEET A1.0 FOR WALL TYPE DEFINITION AND STANDARD DETAILS.
- B. ALL PARTITIONS ARE TYPE P1A, UNO.
- C. ALL WALLS EXTEND TO STRUCTURE, UNO.
- D. ALL INTERIOR WALLS INCLUDE ACoustICAL INSULATION.
- E. DIMENSIONS REFLECT FACE OF FINISH, UNO.
- F. DOORS NOT DIMENSIONED ARE TO BE LOCATED 4" FROM FACE OF WALL TO OUTSIDE EDGE OF JAMB, TYPICAL.
- G. SEE ENLARGED PLANS FOR ADDITIONAL DIMENSIONS & NOTATIONS. ENLARGED PLANS TAKE PRECEDENCE OVER PLANS OF SMALLER SCALE.
- H. SEE REFLECTED CEILING PLANS FOR ADDITIONAL INFORMATION.
- I. SEE FINISH PLAN FOR CASEWORK AND FINISH RELATED INFORMATION.
- J. CONTRACTOR TO PROVIDE ADEQUATE GYPSUM BOARD CONTROL JOISTS AS REQUIRED THROUGHOUT ENTIRE BUILDING, INTERIOR AND EXTERIOR.
- K. CONTRACTOR SHALL VERIFY AND CORRECT ALL DIMENSIONS AND LAYOUT INFORMATION PRIOR TO START OF WORK. NOTIFY ARCHITECT OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO CONSTRUCTION. DO NOT SCALE DRAWINGS.
- L. ELEVATION 8'-0" = 81.50' FINISH FLOOR ELEVATION MAIN FLOOR INDICATED BY CIVIL DRAWINGS.

**KEYNOTES**

- 00-01 LINE OF ROOF ABOVE
- 05-02 METAL DOWNSPOUT - SEE TAB 2
- 06-04 SPOULDER CENTER ON STRUCTURE - SEE S24.1
- 11-01 REFRIGERATOR (207)
- 11-07 FULL SIZE COPY MACHINE (207)
- 22-04 KITCHEN SINGLE SINK (REFER TO PLUMBING)

City of Bainbridge Island

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Planning and  
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ATTACHMENT I

MECHANICAL/ELECTRICAL  
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Project  
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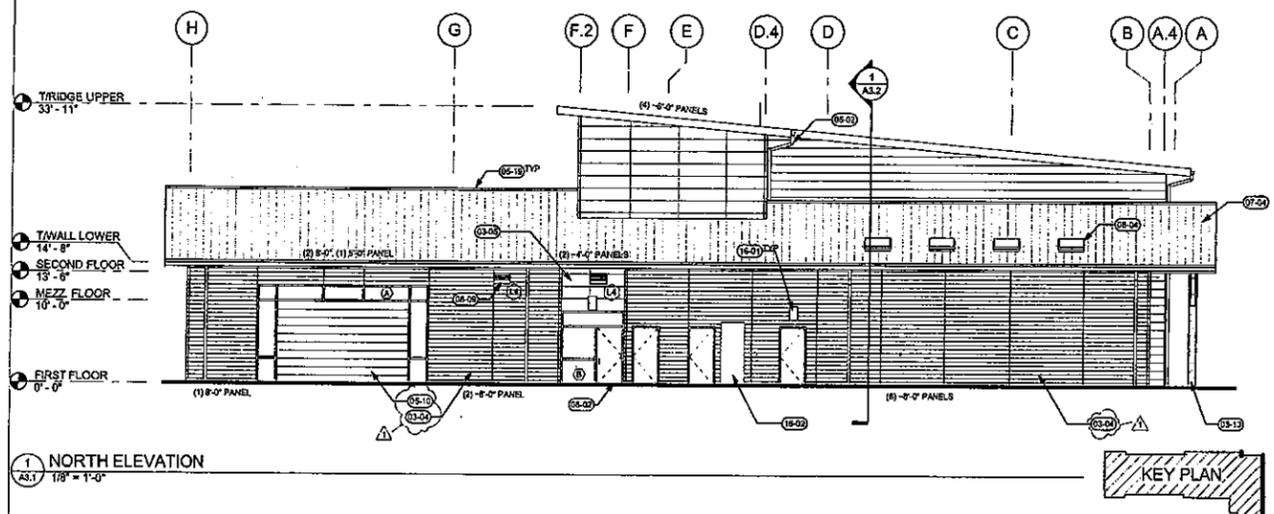
Revision Schedule	
Revision Date	Issue Date
1	8/12/2016

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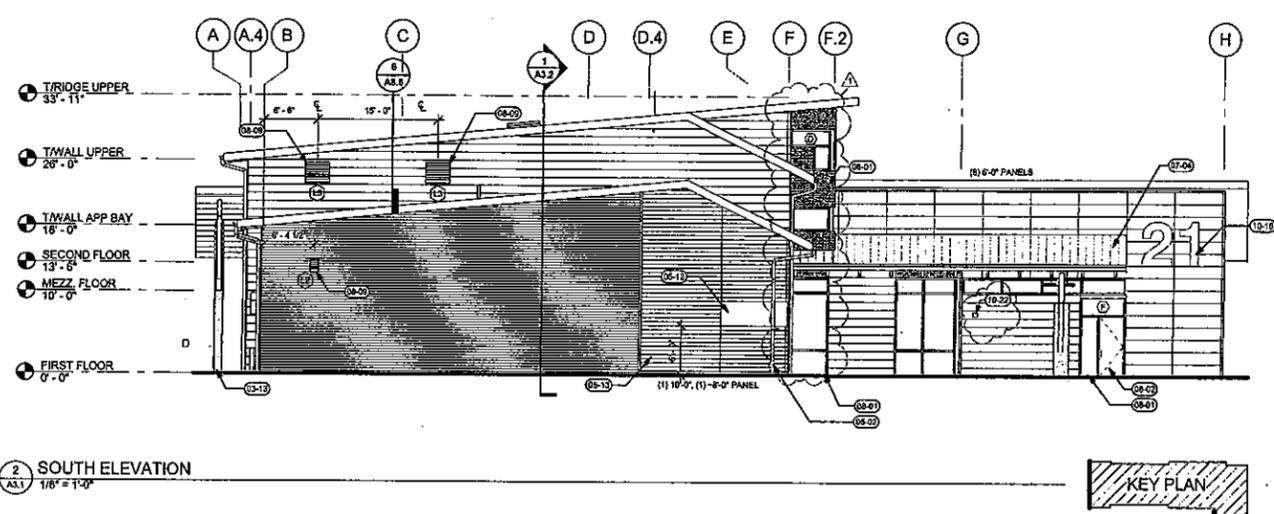
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**SECOND FLOOR PLAN**

SHEET  
**A2.2**

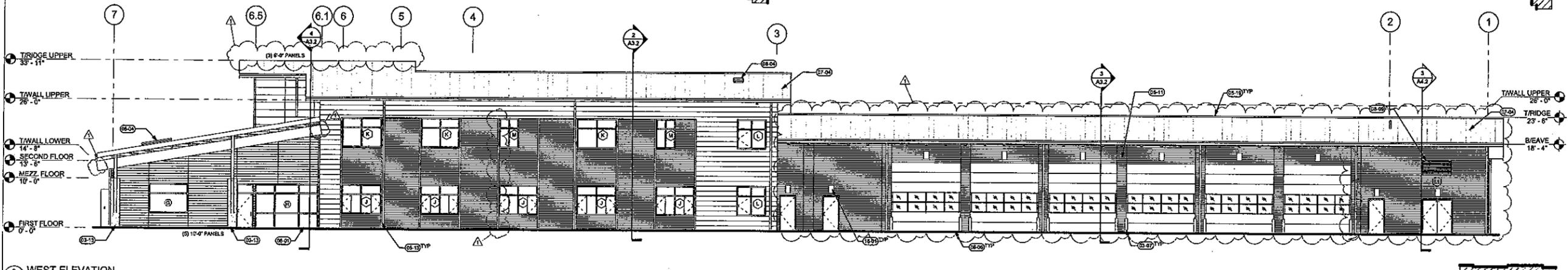
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PERMIT SET 06/30/16



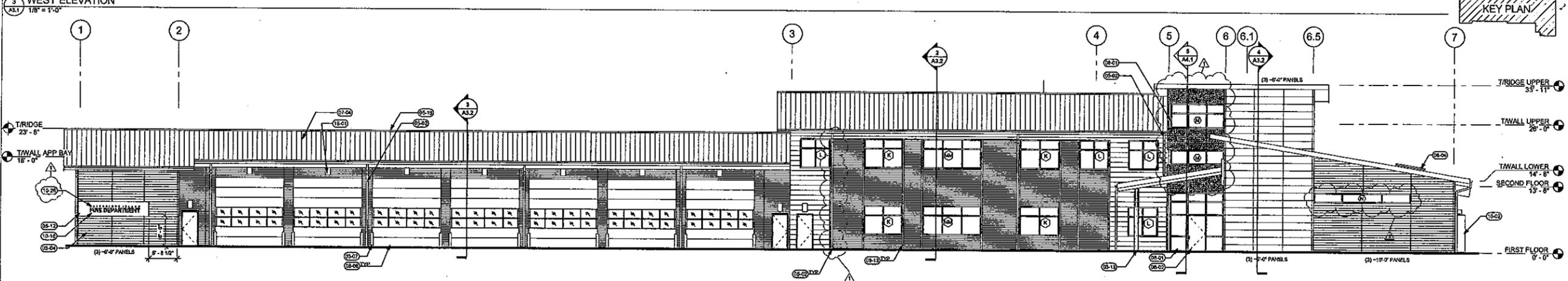
1 NORTH ELEVATION  
1/8" = 1'-0"



2 SOUTH ELEVATION  
1/8" = 1'-0"



3 WEST ELEVATION  
1/8" = 1'-0"



4 EAST ELEVATION  
1/8" = 1'-0"

**GENERAL NOTES**

A. SEE ELEVATIONS FOR EXTERIOR WINDOW TYPE DESIGNATION  
 B. ELEVATION 0'-0" = 181.87 FINISH FLOOR ELEVATION INDICATED IN CIVIL DRAWINGS. FINISH GRADE VARIES AT BUILDING PERIMETER CONDITIONS. REFERENCE CIVIL DRAWINGS FOR ADDITIONAL INFORMATION. CONTRACTOR SHALL VERIFY AND CORRECT ALL DIMENSIONS AND LAYOUT INFORMATION. NOTIFY ARCHITECT OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO CONSTRUCTION. DO NOT SCALE DRAWINGS.  
 C. REFER TO ENLARGED ELEVATIONS WHERE INDICATED.  
 D. SEE ARCHITECTURAL SCHEDULE FOR WINDOW TYPE DESIGNATION.  
 E. SEE EXTERIOR ELEVATIONS FOR LOWER TYPE DESIGNATION.

- KEYNOTES**
- 03-04 EXTERIOR FIBER CEMENT BOARD ASSEMBLY FCB-1 - SEE P20A1.0
  - 03-05 EXTERIOR FIBER CEMENT BOARD ASSEMBLY FCB-2 - SEE P20A1.0
  - 03-06 CONCRETE BOLLARD, PAINTED - SEE 03A.8
  - 03-07 CONCRETE BOLLARD, PAINTED - SEE 03A.8
  - 03-13 CONCRETE COLUMN AT OVERHANG - SEE DETAIL 18A.8.2
  - 05-02 METAL DOWNSPOUT - SEE TAB.2
  - 05-10 EXTERIOR METAL PANEL ASSEMBLY MP-1 - SEE P20A1.0
  - 05-11 EXTERIOR METAL PANEL ASSEMBLY MP-2 - SEE P20A1.0
  - 05-12 EXTERIOR METAL PANEL ASSEMBLY MP-3 - SEE P20A1.0
  - 05-13 BRASS METAL
  - 05-19 RIDGE CAP, SEE DETAIL 19A.1
  - 07-04 METAL ROOF ASSEMBLY - SEE 19A.1
  - 09-01 ALUMINUM-FRAMED STOREFRONT SYSTEM - SEE WINDOW SCHEDULE
  - 09-02 EXTERIOR STOREFRONT DOORS
  - 09-04 SKYLIGHT, CENTER ON STRUCTURE - SEE 21A.1
  - 09-08 OVERHEAD SECTIONAL DOOR - SEE DOOR SCHEDULE
  - 09-09 HVAC VENT - SEE MECHANICAL
  - 10-10 EXTERIOR SIGNAGE - SEE TAB.8
  - 10-22 EMERGENCY ACCESS KNOCK BOX, VERIFY MOUNTING HEIGHT W/ OWNER PRIOR TO INSTALLATION RECESS AND FLASH AS PREPARED, SEE JUNCTION BOX
  - 10-23 EXTERIOR DEPARTMENT SIGNAGE - SEE TAB.8
  - 10-24 LIGHT FIXTURE - SEE ELECTRICAL
  - 10-25 CT CABINET - SEE ELECTRICAL

**LEGEND**

	EXTERIOR METAL PANEL ASSEMBLY, MP-1		EXTERIOR FIBER CEMENT BOARD, FCB-1
	EXTERIOR METAL PANEL ASSEMBLY, MP-2		EXTERIOR FIBER CEMENT BOARD, FCB-2
	EXTERIOR METAL PANEL ASSEMBLY, MP-3		CONCRETE

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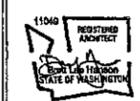
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Project  
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 8895 MADISON AVE. NE



Revision Schedule

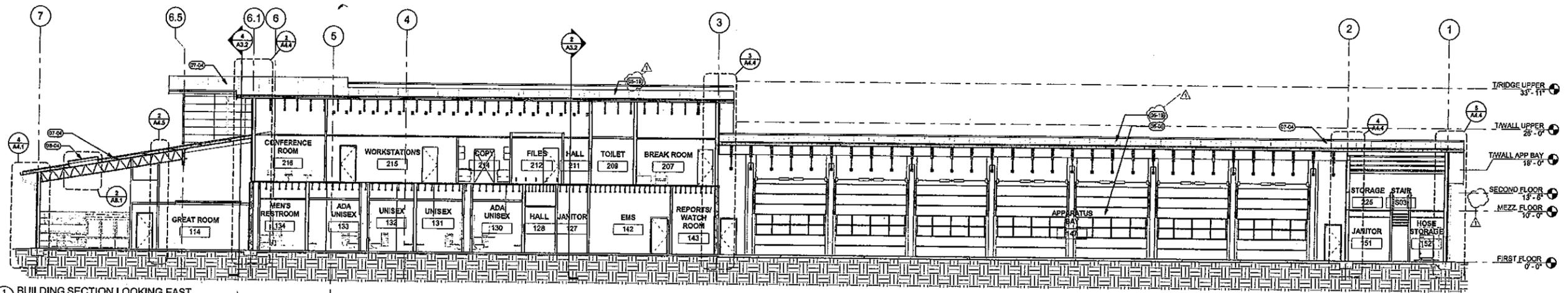
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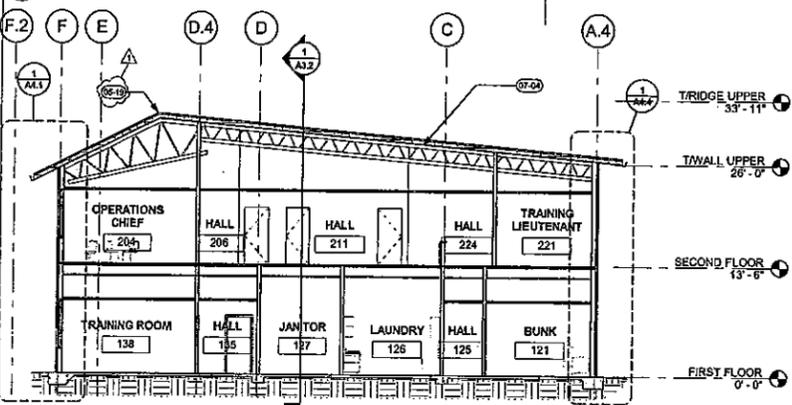
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**BUILDING ELEVATIONS**

SHEET  
**A3.1**

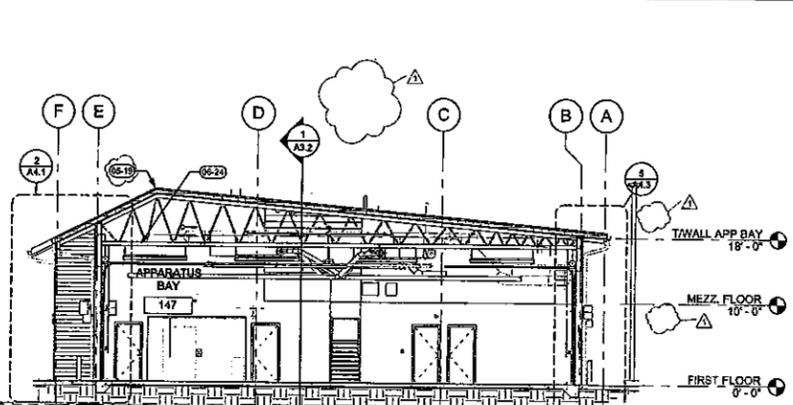
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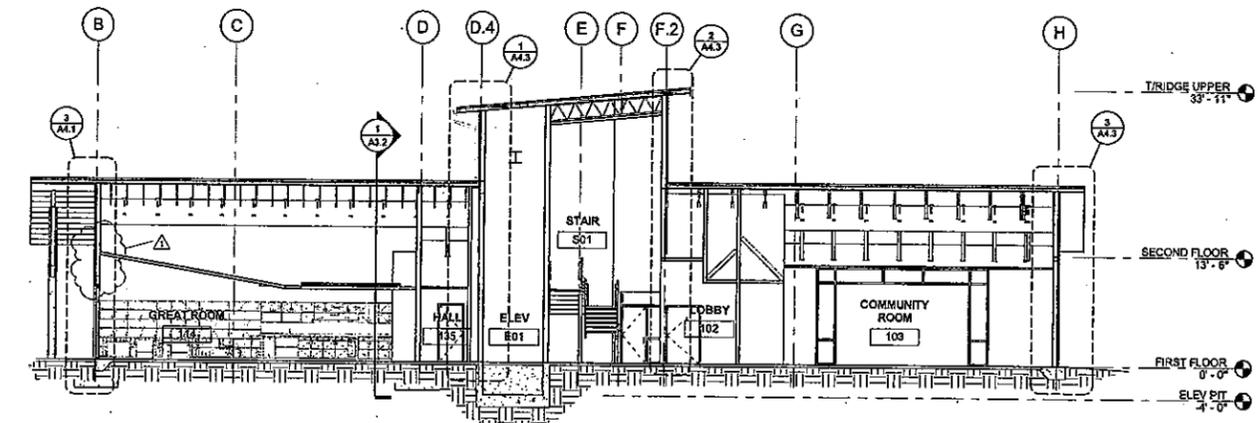
1 BUILDING SECTION LOOKING EAST  
1/8" = 1'-0"



2 WALL SECTION  
1/8" = 1'-0"



3 BUILDING SECTION LOOKING SOUTH  
1/8" = 1'-0"



4 BUILDING SECTION LOOKING NORTH  
1/8" = 1'-0"

LEGEND

- EXTERIOR METAL PANEL ASSEMBLY, MP-1
- EXTERIOR METAL PANEL ASSEMBLY, MP-2
- BOARD FORMED CONCRETE
- EXTERIOR FIBER CEMENT BOARD, FCB-1
- EXTERIOR FIBER CEMENT BOARD, FCB-2

- WINDOW TYPE, SEE A3.2
- INTERIOR WINDOW TYPE, SEE A3.2

GENERAL NOTES

- A. FRESH FLOOR ELEVATION (FF) MAIN FLOOR 0'-0" EQUALS FF ELEVATION AS INDICATED ON CIVIL PLANS
- B. ALL GLAZING SYSTEMS ARE ALUMINUM CASHEMENT UNLESS NOTED OTHERWISE
- C. ALL WALLS EXTEND TO STRUCTURE UNO
- D. SEE A3.2 FOR GLAZING SCHEDULE
- E. SEE A3.1 FOR TYPICAL SEALANT JOINT

KEYNOTES

- 03-07 CONCRETE BOLLARD, PAINTED - SEE #18.6
- 06-19 RIDGE CAP, SEE DETAIL 15A.1
- 08-24 APP BAY TRUSS - SEE STRUCTURAL UNLESS NOTED OTHERWISE
- 07-04 METAL ROOF ASSEMBLY - SEE 11A.1
- 08-04 SKYLIGHT, CENTER ON STRUCTURE - SEE 21A.1
- 08-06 OVERHEAD SECTIONAL DOOR - SEE DOOR SCHEDULE

City of Bainbridge Island

OCT 17 2016

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706 SW 3RD, SUITE 400  
PORTLAND, OR 97204

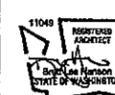
LANDSCAPE  
FISCHER BOUMA PARTNERSHIP  
310 MADISON AVENUE  
SOUTH, SUITE A  
BAINBRIDGE ISLAND, WA  
98110



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BAINBRIDGE ISLAND FIRE  
DEPARTMENT  
8895 MADISON AVE. NE  
BAINBRIDGE, WA 98110

BAINBRIDGE ISLAND FIRE DEPT  
STATION 21  
8895 MADISON AVE. NE



Revision Schedule	
Revision Date	Issue Date
1	08/22/2016

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SHEET TITLE  
**BUILDING SECTIONS**

**A3.2**

DRAWN BY: COOJDR  
CHECKED BY: BLH

REVISED 08/12/16 JOB NO. 2150124.00

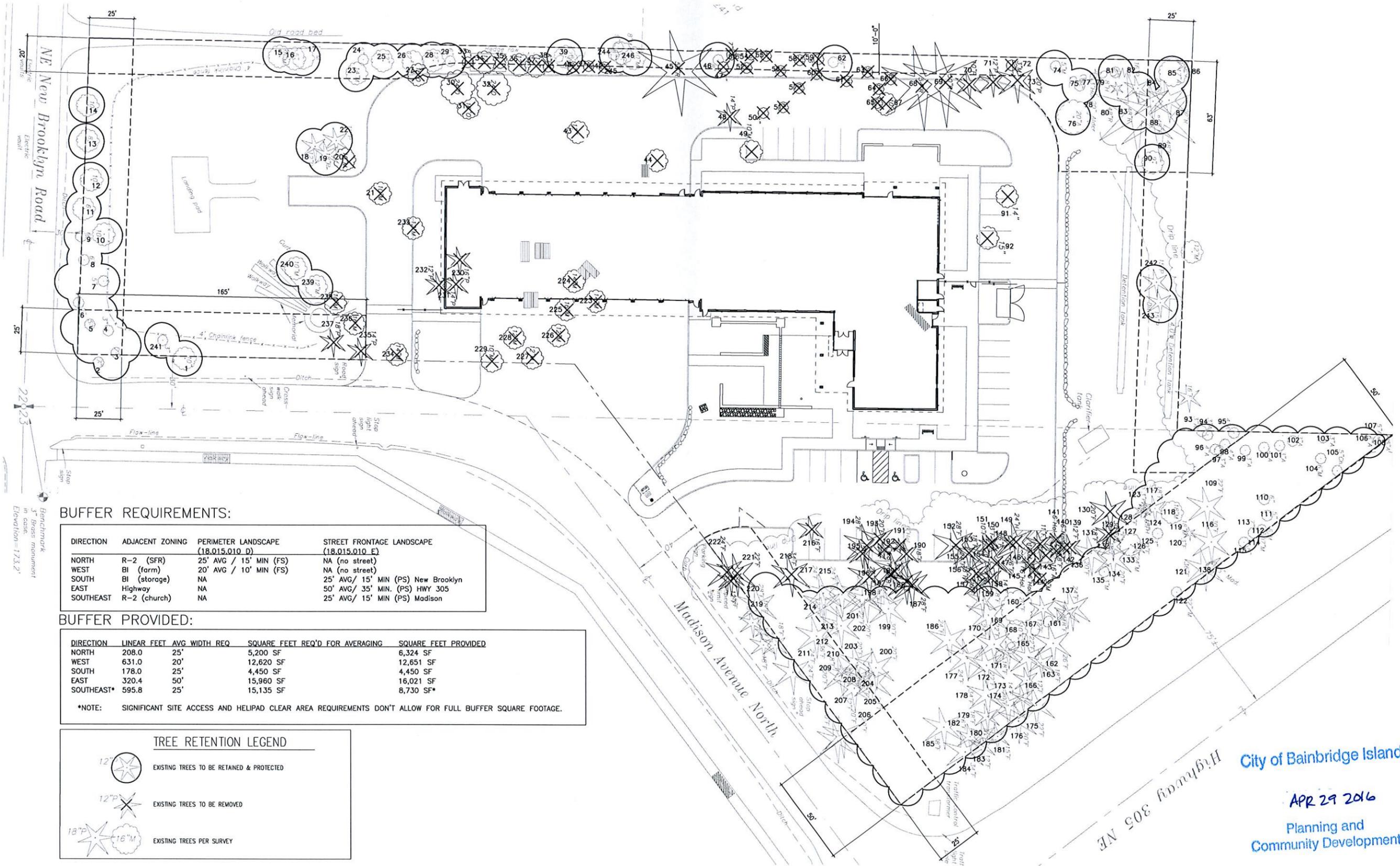
PERMIT SET 08/30/16



ATTACHMENT L



ATTACHMENT M



ATTACHMENT N

**BUFFER REQUIREMENTS:**

DIRECTION	ADJACENT ZONING	PERIMETER LANDSCAPE (18.015.010 D)	STREET FRONTAGE LANDSCAPE (18.015.010 E)
NORTH	R-2 (SFR)	25' AVG / 15' MIN (FS)	NA (no street)
WEST	BI (farm)	20' AVG / 10' MIN (FS)	NA (no street)
SOUTH	BI (storage)	NA	25' AVG/ 15' MIN (PS) New Brooklyn
EAST	Highway	NA	50' AVG/ 35' MIN. (PS) HWY 305
SOUTHEAST	R-2 (church)	NA	25' AVG/ 15' MIN (PS) Madison

**BUFFER PROVIDED:**

DIRECTION	LINEAR FEET	AVG WIDTH REQ	SQUARE FEET REQ'D FOR AVERAGING	SQUARE FEET PROVIDED
NORTH	208.0	25'	5,200 SF	6,324 SF
WEST	631.0	20'	12,620 SF	12,651 SF
SOUTH	178.0	25'	4,450 SF	4,450 SF
EAST	320.4	50'	15,960 SF	16,021 SF
SOUTHEAST*	595.8	25'	15,135 SF	8,730 SF*

\*NOTE: SIGNIFICANT SITE ACCESS AND HELIPAD CLEAR AREA REQUIREMENTS DON'T ALLOW FOR FULL BUFFER SQUARE FOOTAGE.

**TREE RETENTION LEGEND**

- EXISTING TREES TO BE RETAINED & PROTECTED
- EXISTING TREES TO BE REMOVED
- EXISTING TREES PER SURVEY

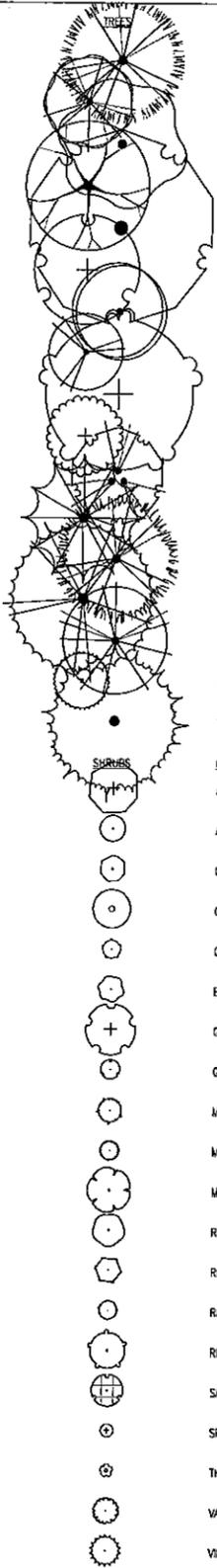
City of Bainbridge Island  
 APR 29 2016  
 Planning and  
 Community Development

**TREE RETENTION PLAN**  
 1"=20'-0"



MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204	LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110	<p><b>MACKENZIE</b>          DESIGN DRIVEN   CLIENT FOCUSED</p>	Architecture • Interiors • Planning • Engineering Client <b>BAINBRIDGE ISLAND FIRE DEPARTMENT</b> 8895 MADISON AVE. NE BAINBRIDGE ISLAND, WA 98110	Project <b>BAINBRIDGE ISLAND FIRE DEPT STATION 21</b> 8895 MADISON AVE. NE		REVISIONS: REVISION NUMBER REVISION DATE REVISION DESCRIPTION	© MACKENZIE 2016. ALL RIGHTS RESERVED. THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER WITHOUT PRIOR WRITTEN PERMISSION. DRAWN BY: JP CHECKED BY: JB	SHEET TITLE <b>TREE RETENTION PLAN</b>	SHEET <b>L1.0</b> JOB NO: 2150124.00
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PLANT SCHEDULE



CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL	SIZE
ABI GRA	5	ABIES GRANDIS	GRAND FIR	NA		6' HT.
ACE CIR	13	ACER CIRCINATUM	VINE MAPLE	B & B	2" CAL	
ACE GRI	3	ACER GRISEUM	PAPERBARK MAPLE	B & B	2" CAL	
ACE SCA	4	ACER RUBRUM 'SCARLET SENTINEL'	SCARLET SENTINEL MAPLE	B & B	2" CAL	
ACE SAC	3	ACER SACCHARUM 'GREEN MOUNTAIN TM'	GREEN MOUNTAIN SUGAR MAPLE	B & B	2" CAL	
CHI VIR	1	CHIONANTHUS VIRGINICUS	WHITE FRINGETREE	B & B	2" CAL	
COR KN3	9	CORNUS X 'KN 3B-8'	VENUS DOGWOOD	NA	2" CAL	
EXI TRE	9	EXISTING TREE	EXISTING TREE	NA		
FRA JUN	3	FRAXINUS AMERICANA 'JUNGINGER' TM	AUTUMN PURPLE WHITE ASH	B & B	2" CAL	
OXY ARB	3	OXYDENDRUM ARBOREUM	SOURWOOD TREE	B & B	2" CAL	
PAR RUB	1	PARROTIA PERSICA 'RUBY VASE'	RUBY VASE PERSIAN PARROTTA	B & B	2" CAL	
PIC ORI	7	PICEA ORIENTALIS	CAUCASIAN SPRUCE	NA	NA	4' - 6'
PIN CON	4	PINUS CONTORTA	SHORE PINE	B & B	NA	6' HT.
PSE DOU	15	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	NA	NA	6' HT.
STE PSE	4	STEWARTIA PSEUDOCAMELLIA	JAPANESE STEWARTIA	B & B	2" CAL	
THU GRE	26	THUJA OCCIDENTALIS 'GREEN GIANT'	GREEN GIANT ARBORVITAE	NA		6' HT.
THU FLI	19	THUJA PLUCATA	WESTERN RED CEDAR	NA	NA	6' HT.
CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT		
ARB UNE	27	ARBUTUS UNEDO	STRAWBERRY TREE	3 GAL		
ARB COM	17	ARBUTUS UNEDO 'COMPACTA'	DWARF STRAWBERRY TREE	3 GAL		
CLE CAL	17	CLETHRA ALNIFOLIA 'CALEB'	VANILLA SPICE SUMMERSWEET CLETHRA	3 GAL		
COR CAR	21	CORNUS STOLONIFERA 'CARDINAL'	CARDINAL REDTIG DOGWOOD	3 GAL		
COR FAR	67	CORNUS STOLONIFERA 'FARROW'	ARCTIC FIRE RED TWIG DOGWOOD	1 GAL		
ESC COM	15	ESCALLONIA X 'COMPACTA'	COMPACT ESCALLONIA	3 GAL		
GAR JAM	28	GARRYA ELLIPTICA 'JAMES ROOF'	COAST SILK TASSEL	3 GAL		
GAU SHA	188	GAULTHERIA SHALLON	SALAL	1 GAL		
MAH AQU	247	MAHONIA AQUIFOLIUM	OREGON GRAPE	1 GAL		
MAH COM	179	MAHONIA AQUIFOLIUM 'COMPACTA'	COMPACT OREGON GRAPE	1 GAL		
MYR CAL	3	MYRICA CALIFORNICA	PACIFIC WAX MYRTLE	3 GAL		
RHA RUT	74	RHAPHOLEPIS UMBELLATA 'RUTRHAPHI'	SOUTHERN MOON YEDDA HAWTHORN	3 GAL		
RHA SNO	58	RHAPHOLEPIS X DELACOURII 'SNOWCAP'	SNOWCAP INDIAN HAWTHORN	3 GAL		
RHO FRA	18	RHODODENDRON X 'FRAGRANT STAR'	FRAGRANT STAR AZALEA	2 GAL		
RIB SAN	3	RIBES SANDUNEUM	RED FLOWERING CURRANT	3 GAL		
SAL PUR	17	SALIX PURPUREA 'NANA'	DWARF ARCTIC WILLOW	3 GAL		
SPI MAG	66	SPIRAEA JAPONICA 'MAGIC CARPET'	MAGIC CARPET SPIREA	1 GAL		
THU DAN	17	THUJA OCCIDENTALIS 'DANICA'	DANICA GLOBE CEDAR	3 GAL		
VAC OVA	57	VACCINIUM OVATUM	EVERGREEN HUCKLEBERRY	1 GAL		
VIB COM	26	VIBURNUM TRILODUM 'COMPACTUM'	DWARF CRANBERRY BUSH	3 GAL		

ANNUALS/PERENNIALS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SPACING
	NAR SA	72	NARCISSUS X 'SAINT KEVERNE'	THALIA DAFFODIL	BULB	
GROUND COVERS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SPACING
	ARC LVA	927	ARCTOSTAPHYLOS LVA-URSI	KINKINICK	4" POT	36" o.c.
	ASA CAU	16	ASARUM CAUDATUM	BRITISH COLUMBIA WILD GINGER	4" POT	24" o.c.
	BLE SPI	191	BLECHNUM SPICANT	DEER FERN	4" POT	24" o.c.
	CAL SAN	248	CALLUNA VULGARIS 'SANDY'	SANDY SCOTCH HEATHER	4" POT	24" o.c.
	CAR OBN	365	CAREX OBNLPTA	SLOUGH SEDGE	PLUG	24" o.c.
	HYD MIX	19,718 SF	HYDROSEED MIX	HYDROSEED MIX	SEED	
	JUN EFF	198	JUNCUS EFFUSUS	COMMON RUSH	PLUG	36" o.c.
	LON PIL	93	LONICERA PILEATA	PRIVET HONEYSUCKLE	1 GAL	48" o.c.
	MAH REP	628	MAHONIA REPENS	CREeping MAHONIA	4" POT	36" o.c.
	POL MAZ	881	POLYSTICHUM MUNITUM	WESTERN SWORD FERN	4" POT	42" o.c.
	RUB EME	588	RUBUS CALYCNOIDES 'EMERALD CARPET'	EMERALD CARPET CREEPING RASPBERRY	4" POT	36" o.c.
	SAG SUB	47	SAGINA SUBULATA	IRISH MOSS	4" POT	18" o.c.
	SOD RPE	743 SF	SOD LAWN SOD	68% PERENNIAL RYE / 48% FESCUE	FLAT	

City of Bainbridge Island

SEP 29 2016

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PLANTING NOTES

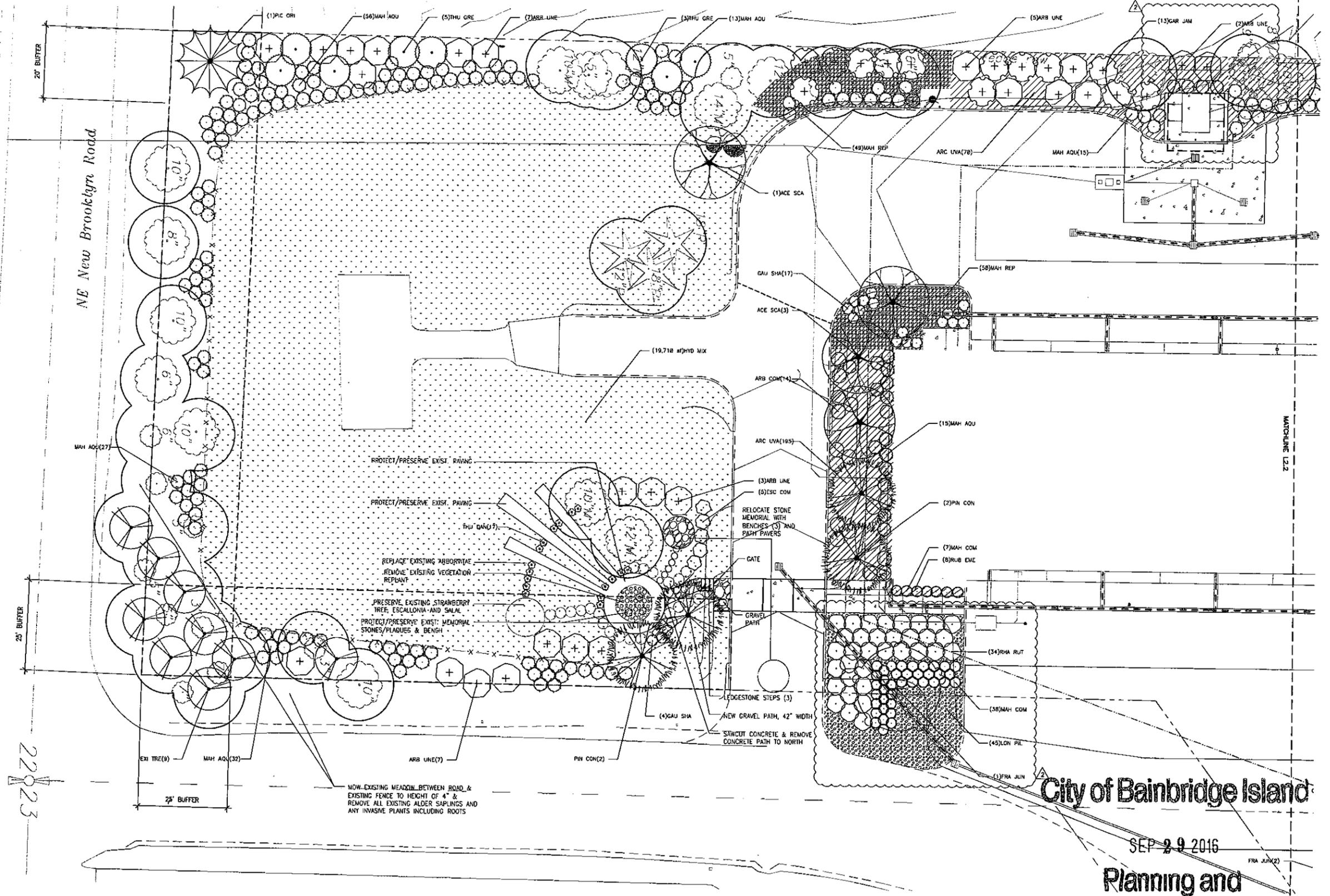
- SEE PLANT SCHEDULES FOR SPECIES, QUANTITIES, AND MINIMUM SPACING. SEE PLANTING DETAILS FOR INSTALLATION AND LAYOUT.
- CONTRACTOR SHALL ALLOW FOR THE ADDITION OF SOIL AMENDMENTS AND CONDITIONERS IN SOIL PREPARATION AND FINISH GRADING.
- ALL CONSTRUCTION MATERIAL SHALL BE EXCLUDED FROM TOPSOIL.
- LAYOUT FOR ALL PLANTING AREAS TO BE VERIFIED ON SITE BY LANDSCAPE ARCHITECT DUE TO VARYING FIELD CONDITIONS AND DENSITIES OF EXISTING VEGETATION.
- ALL PLANT MATERIAL SHALL CONFORM TO ANSI STANDARDS FOR NURSERY STOCK, LATEST EDITION.
- LANDSCAPE ARCHITECT WILL SUPERVISE THE PLANTING PROCESS. MODIFICATIONS TO THE PLANTING PLAN MAY OCCUR BASED ON FIELD CONDITIONS AND THE AVAILABILITY OF PLANT MATERIAL.
- PLANTINGS WILL BE B&B AND CONTAINERIZED NATIVE PLANT MATERIAL, AS APPROPRIATE AND SPECIFIED, FROM REGIONAL GENETIC STOCK. PLASTIC NURSERY IDENTIFICATION TAGS WILL BE ATTACHED TO THE STEM OF EACH PLANTING.
- LOCAL NURSERY STOCK WILL BE USED TO ENSURE THAT THE MATERIAL HAS ACCLIMATED TO LOCAL CONDITIONS (REDUCING PLANTING STRESS) AND IS GENETICALLY COMPARABLE WITH PLANTS IN THE LOCAL AREA.
- FINAL PLANT LISTS WILL BE CONTINGENT UPON PLANT AVAILABILITY. IF SELECTED SPECIES ARE UNAVAILABLE FROM LOCAL NURSERIES THEN UPON APPROVAL OTHER GENUS OR SPECIES WITH SIMILAR HYDROLOGICAL REQUIREMENTS MAY BE SUBSTITUTED.
- TREES WILL BE STAKED ONLY IF A PLANT CANNOT STAND ALONE IN A MODERATE WIND PER PLANTING DETAILS.
- ALL PLANTINGS WILL BE MULCHED WITH A MINIMUM OF 3 INCHES OF ORGANIC MULCH AS SPECIFIED TO DISCOURAGE WEED GROWTH, MINIMIZE SOIL EROSION, AND RETAIN MOISTURE. THE MULCH IS NOT TO MAKE CONTACT WITH THE PLANT STEM.
- DO NOT DISTURB TREES AND VEGETATION OUTSIDE LIMIT OF WORK.
- IF TREES HAVE NOT BEEN DESIGNATED AS SAVE OR REMOVE ON DEMO OR OTHER PLANS IT SHOULD BE ASSUMED THAT THEY ARE TO BE SAVED.
- USE FINE SHREDDED ORGANIC BARK MULCH IN RAIN GARDENS TO AVOID FLOTATION OF MATERIAL.
- THE ENTIRE WIDTH OF THE PLANTING ISLANDS SHALL CONTAIN ONLY SOIL, AND BY WAY OF ILLUSTRATION ONLY AND NOT LIMITATION, BE FREE OF GRAVEL, CONCRETE, CONSTRUCTION DEBRIS, OR OTHER FOREIGN MATERIALS.
- ALL PLANTING AREAS TO BE IRRIGATED - IRRIGATION PLANS ARE NOT INCLUDED. IRRIGATION WILL BE DESIGNED/BUILT BY CONTRACTOR BASED ON PERFORMANCE SPECIFICATIONS PROVIDED.
- DO NOT DIG BEFORE LOCATING UTILITIES.
- ALL LANDSCAPE AREAS DESIGNED FOR INSTALLATION OF TREES SHALL HAVE A MINIMUM SOIL DEPTH OF 18" PLUS 8" SCARIFIED SUBGRADE. MINIMUM SOIL DEPTH SHALL INCLUDE SOILS THAT MEET SOIL SPECIFICATIONS.
- FOR ALL TOPSOIL, THE CONTRACTOR SHALL OBTAIN A LANDSCAPE SOIL ANALYSIS CERTIFYING THAT THE SOIL MEETS THE SOIL SPECIFICATIONS. THE CERTIFICATION SHALL BE REVIEWED BY THE LANDSCAPE ARCHITECT AND APPROVED PRIOR TO SOIL DELIVERY.
- APPLY 3" DEPTH OF ORGANIC BARK MULCH IN ALL PLANTING AREAS. MULCH TO BE FREE OF WEED SEED, SANDUST, RESIN OR TANNIN, AND SHALL NOT CONTAIN ANY COMPOUNDS DETRIMENTAL TO PLANT GROWTH. PULL MULCH 3" AWAY FROM BASE OF PLANT.
- ALL PLANT MATERIAL SHOULD BE DISEASE FREE AND ARRIVE IN A VIGOROUS GROWING CONDITION.
- LANDSCAPE CONTRACTOR SHALL MAINTAIN THE SITE UNTIL FINAL INSPECTION AND ACCEPTANCE BY THE OWNER.
- ALL LANDSCAPE INSTALLATION TO BE REVIEWED AND APPROVED BY THE LANDSCAPE ARCHITECT.
- A PRE-CONSTRUCTION CONFERENCE PRIOR TO THE START OF ANY LANDSCAPE CONSTRUCTION IS REQUIRED.
- PLANTS SHALL BE INSPECTED PRIOR TO PLANTING TO VERIFY CONFORMANCE WITH PLANTING SIZE AND OTHER REQUIREMENTS.
- ALL LANDSCAPING SHALL BE WARRANTED FOR A PERIOD OF 1 YEAR AFTER FINAL ACCEPTANCE BY THE OWNER.
- PRIOR TO THE END OF THE WARRANTY PERIOD, THE LANDSCAPE WILL BE INSPECTED AND DEAD OR UNHEALTHY PLANTS WILL BE REQUIRED TO BE REPLACED WITH SAME SPECIES AND SIZES.

GENERAL NOTES

- REFER TO ARCHITECTURAL & CIVIL DRAWINGS FOR LIMITS OF WORK AND DEMOLITION OF EXISTING SITE ITEMS.
- REFER TO CIVIL DRAWINGS FOR LANDSCAPE GRADING AND DRAINAGE.
- REFER TO ARCHITECTURAL DRAWINGS FOR SITE HARDSCAPE DETAILS.
- EXISTING TREES TO BE PROTECTED - REFER TO PLANTING PLAN AND CIVIL DEMO PLAN FOR LOCATIONS AND CIVIL PLANS & SPECS FOR TREE PROTECTION DETAILS. CONTRACTOR IS SUBJECT TO PENALTIES FOR DAMAGE TO OR REMOVAL OF THESE TREES.

ATTACHMENT O

<p>MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204</p>	<p>LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110</p>	<p>Architecture • Interiors • Planning • Engineering</p> <p><b>M.</b></p> <p>Portland, OR 503.224.8550 Vancouver, WA 360.696.7879 Seattle, WA 206.749.9883</p> <p><b>MACKENZIE.</b> DESIGN DRIVEN • CLIENT FOCUSED</p>	<p>Client</p> <p><b>BAINBRIDGE ISLAND FIRE DEPARTMENT</b> 8895 MADISON AVE. NE BAINBRIDGE ISLAND, WA 98110</p>	<p>Project</p> <p><b>BAINBRIDGE ISLAND FIRE DEPT STATION 21</b> 8895 MADISON AVE. NE</p>	<p>REVISIONS:</p> <table border="1"> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> <tr> <td>1</td> <td></td> <td>ISSUED FOR PERMITS</td> </tr> <tr> <td>2</td> <td></td> <td>ISSUED FOR PERMITS</td> </tr> </table>	NO.	DATE	DESCRIPTION	1		ISSUED FOR PERMITS	2		ISSUED FOR PERMITS	<p>DATE: 08/16/16</p> <p>DESIGNED BY: JB</p> <p>CHECKED BY: JB</p>	<p>SCALE: AS SHOWN</p> <p>DATE: 08/16/16</p> <p>DESIGNED BY: JB</p> <p>CHECKED BY: JB</p>	<p>SHEET TITLE:</p> <p><b>PLANT SCHEDULE + NOTES</b></p>	<p>SHEET</p> <p><b>L2.0</b></p> <p>JOB NO. 2150124.00</p>
NO.	DATE	DESCRIPTION																
1		ISSUED FOR PERMITS																
2		ISSUED FOR PERMITS																



NE New Brooklyn Road

20' BUFFER  
25' BUFFER  
22' 23

MATCHLINE L2.2

City of Bainbridge Island

SEP 29 2016

Planning and  
Community Development

PLANTING PLAN - SOUTH  
1"=10'-0"

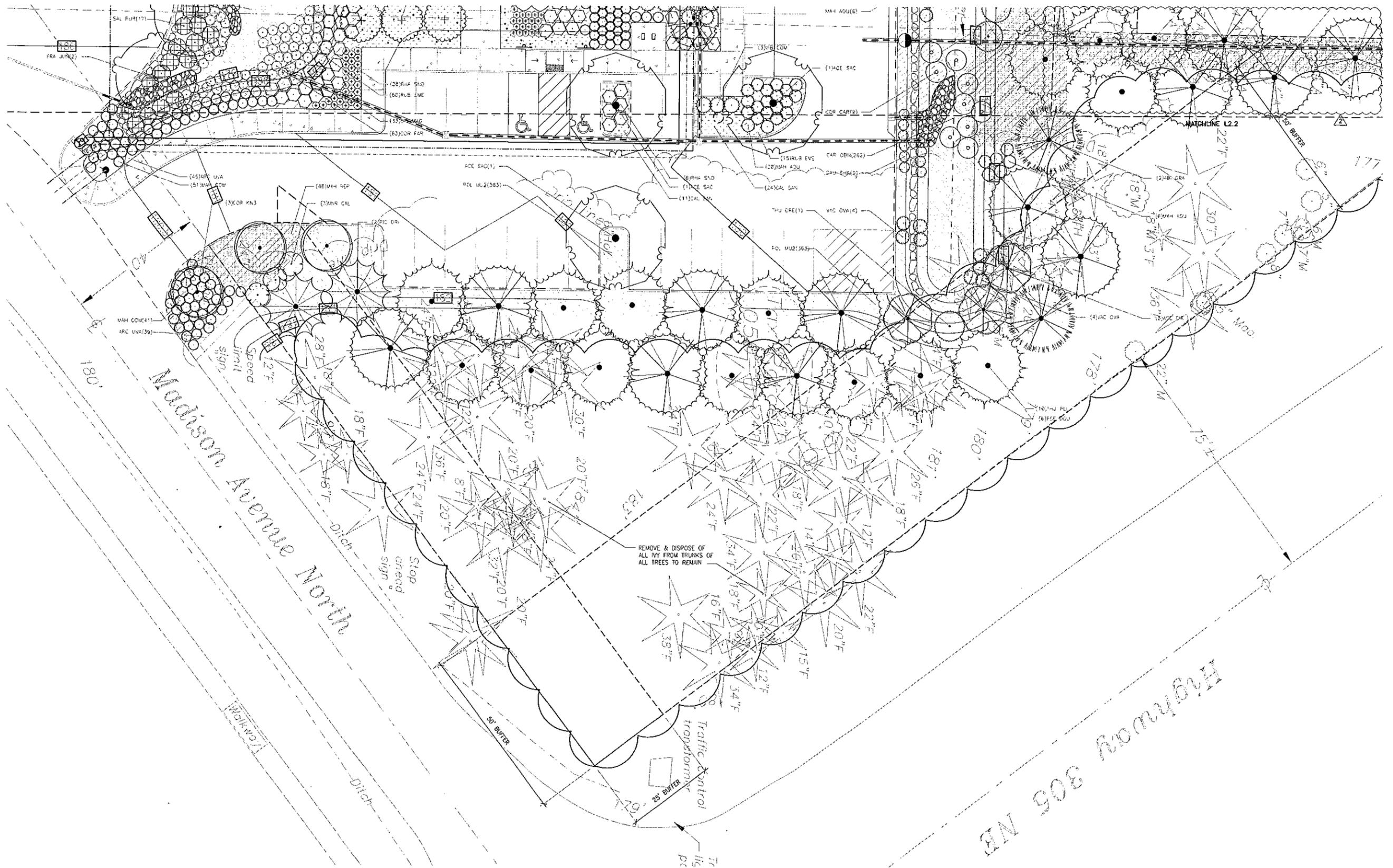
<p>MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204</p>	<p>LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110</p>	<p>www.mckm.com</p>	<p>Architecture • Interiors • Planning • Engineering</p> <p>Portland, OR 503.224.9580 Vancouver, WA 360.595.7076 Seattle, WA 206.749.5552</p> <p><b>MACKENZIE</b> DESIGN DRIVEN • CLIENT FOCUSED</p>	<p>Client <b>BAINBRIDGE ISLAND FIRE DEPARTMENT</b> 8895 MADISON AVE. NE BAINBRIDGE ISLAND, WA 98110</p>	<p>Project <b>BAINBRIDGE ISLAND FIRE DEPT STATION 21</b> 8895 MADISON AVE. NE</p>	<p>STATE OF WASHINGTON REGISTERED ARCHITECT MACKENZIE</p>	<p>REVISIONS:</p> <p>REVISION DELTA CLOSING GATE</p> <p>1. 8/23/16 REVISIONS PER SPR COMMENT 2. 8/23/16 ADJUSTMENTS &amp; REVISIONS</p>	<p>© 2016 ALL RIGHTS RESERVED THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER, WITHOUT PRIOR WRITTEN PERMISSION</p> <p>DRAWN BY: JP</p> <p>CHECKED BY: JB</p>	<p>SHEET TITLE: <b>PLANTING PLAN - SOUTH</b></p>	<p>SHEET <b>L2.1</b></p>
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REVISED 08/12/16 JOB NO. 2150124.00

PERMIT SET 06/30/16

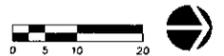
ATTACHMENT P





ATTACHMENT R

PLANTING PLAN - EAST  
1"=10'-0"



MECHANICAL/ELECTRICAL  
INTERFACE ENGINEERING  
708 SW 3RD, SUITE 400  
PORTLAND, OR 97204

LANDSCAPE  
FISCHER BOUMA PARTNERSHIP  
310 MADISON AVENUE  
SOUTH, SUITE A  
BAINBRIDGE ISLAND, WA  
98110



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Client

BAINBRIDGE ISLAND FIRE DEPARTMENT  
8895 MADISON AVE. NE  
BAINBRIDGE ISLAND, WA 98110

Project

BAINBRIDGE ISLAND FIRE DEPT  
STATION 21  
8895 MADISON AVE. NE



REVISIONS:

NO.	DATE	REVISIONS
1	04/20/16	REVISIONS PER SDR COMMENT
2	05/13/16	ADDRESS MAIL ROOM

REVISIONS: JEREMY DELLA CLEGG/DATE

DRAWN BY: JP  
CHECKED BY: JB

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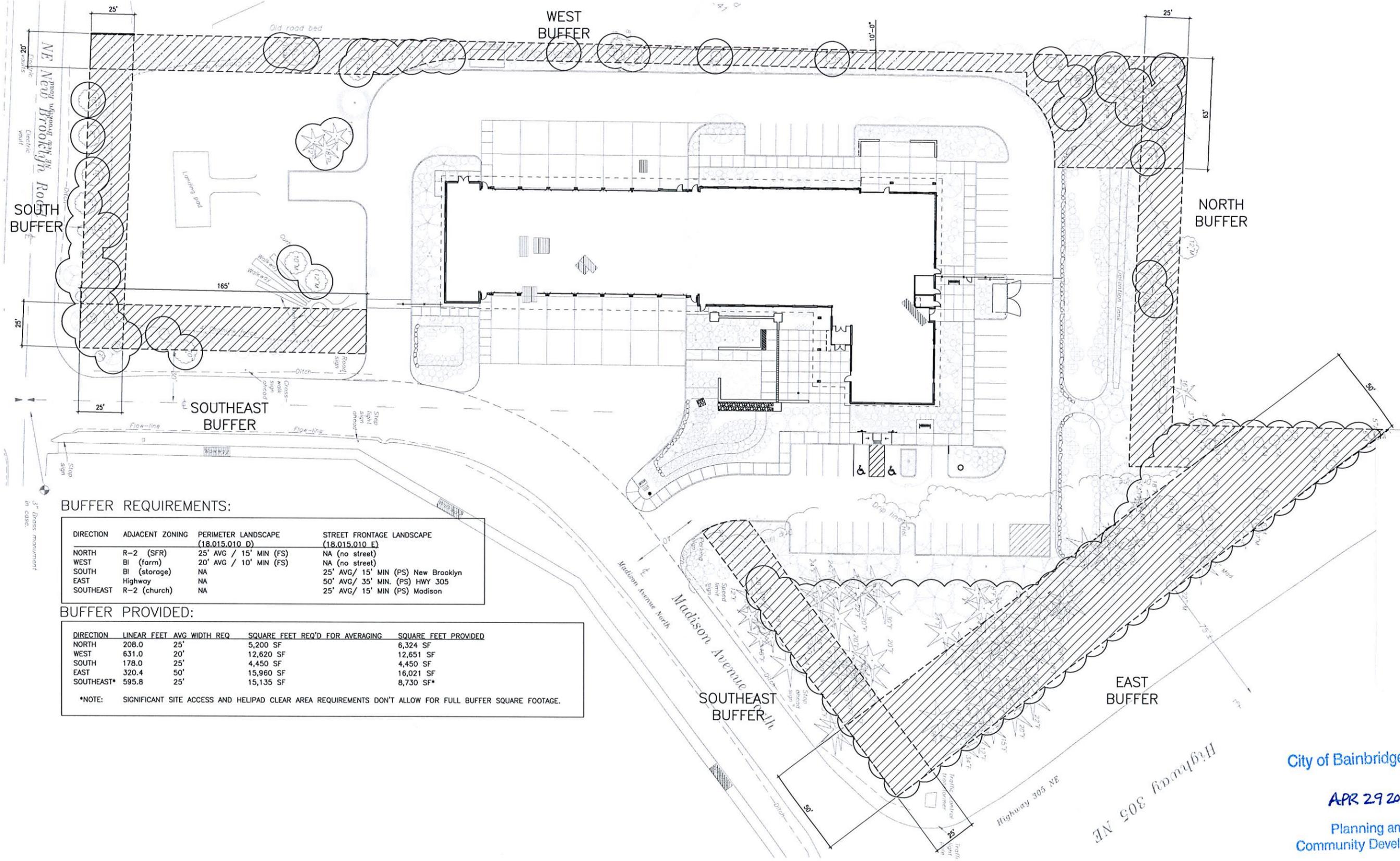
SHEET TITLE:

PLANTING PLAN - EAST

SHEET

L2.3

JOB NO. 2150124.00



**BUFFER REQUIREMENTS:**

DIRECTION	ADJACENT ZONING	PERIMETER LANDSCAPE (18.015.010 D)	STREET FRONTAGE LANDSCAPE (18.015.010 E)
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WEST	BI (farm)	20' AVG / 10' MIN (FS)	NA (no street)
SOUTH	BI (storage)	NA	25' AVG/ 15' MIN (PS) New Brooklyn
EAST	Highway	NA	50' AVG/ 35' MIN. (PS) HWY 305
SOUTHEAST	R-2 (church)	NA	25' AVG/ 15' MIN (PS) Madison

**BUFFER PROVIDED:**

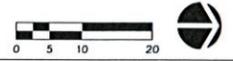
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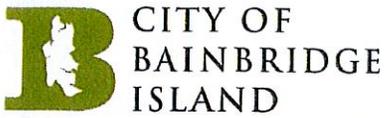
ATTACHMENT S

City of Bainbridge Island  
 APR 29 2016  
 Planning and  
 Community Development

**LANDSCAPE BUFFER DIAGRAM**



MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204	LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110	<p>Architecture • Interiors • Planning • Engineering</p> <p>Portland, OR 503.224.9560 Vancouver, WA 360.695.7879 Seattle, WA 206.749.9993</p> <p><b>MACKENZIE.</b>  <small>DESIGN DRIVEN   CLIENT FOCUSED</small></p>	Client <b>BAINBRIDGE ISLAND FIRE DEPARTMENT</b> 8895 MADISON AVE. NE BAINBRIDGE ISLAND, WA 98110	Project <b>BAINBRIDGE ISLAND FIRE DEPT STATION 21</b> 8895 MADISON AVE. NE	<p>JEFFREY S. BOUMA  <small>CERTIFICATE NO. 847</small></p>	REVISIONS: REVISION DELTA CLOSING DATE 1 24052016 REVISIONS PER SPR FLOWMENT	© MACKENZIE 2016 ALL RIGHTS RESERVED THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER, WITHOUT PRIOR WRITTEN PERMISSION. DRAWN BY: JP CHECKED BY: JB	SHEET TITLE: <b>LANDSCAPE BUFFER DIAGRAM</b>	SHEET <b>L3.0</b> JOB NO 2150124.00
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## Memorandum

Department of Planning and Community  
Development

To: Joshua Machen, Planning Manager  
From: Janelle Hitch, P.E., Development Engineer *JCH*  
Date: October 6, 2016  
Re: Fire Station 21 – Site Plan Review & Conditional Use Permit

**Related Application Number:**

PLN11791 SPR/CUP

### Project Description:

The proposed plan will replace the existing fire station with a new station building. The fire station is located at 8895 Madison Avenue and fronting SR305 and New Brooklyn Road. The site comprises two tax parcels 4159-000-050-0206 and 22502-1-023-2000.

### Comments:

I have completed a review of the above referenced project materials received by the City of Bainbridge Island (COBI). Please see the comments below for information on the proposed development.

1. The conditional use conforms to regulations concerning drainage in BIMC 15.20 and 15.21; and
2. The conditional use will not cause an undue burden on the drainage basin or water quality and will not unreasonably interfere with the use and enjoyment of properties downstream; and
3. The streets and pedestrian ways as proposed align with and are otherwise coordinated with streets serving adjacent properties, see additional condition.
4. The streets and pedestrian ways as proposed are adequate to accommodate anticipated traffic; and
5. There is capacity in the water and sewer system to serve the conditional use, and the applicable services can be made available at the site.
6. The conditional use conforms to the "City of Bainbridge Island Design and Construction Standards and Specifications Manual".

### Conditions:

1. Frontage improvements along New Brooklyn Road must be completed prior to Certificate of Occupancy and/or Final Inspection. Frontage improvements include pedestrian sidewalk and t

**ATTACHMENT T**

2. A mid-block crosswalk on Madison Avenue must be designed, approved, and constructed prior to Certificate of Occupancy and/or Final Inspection.
3. All on-site stormwater facilities shall remain privately owned and maintained. The owner shall be responsible for maintenance of the storm drainage facilities for this development following construction. Annual inspection and maintenance reports shall be provided to the City. A Declaration of Covenant for stormwater system operation and maintenance will be required to be recorded before issuance of occupancy permits. The approved language for the Declaration of Covenant is found in BIMC 15.21 Exhibit A.

**Permits Required:**

1. A NPDES permit from the Department of Ecology will be required prior to ground disturbing activities.
2. A Right-of-Way permit will be required prior to any work within the right of way. The ROW permit will be subject to separate conditions and bonding requirements.



**CITY OF BAINBRIDGE ISLAND**  
**DESIGN REVIEW BOARD**  
**Regularly Scheduled Meeting Minutes**  
**Monday, December 21, 2015 at 2:00 p.m.**  
**City Council Conference Room**  
**280 Madison Ave N**  
**Bainbridge Island, Washington 98110**

Call to Order (Attendance, Agenda, Ethics)

The Ravine Apartments PLN50039 SPR Follow-up

BIFD Station 21 – Pre-application

BIFD Station 22 – Pre-application

Montessori Country School PLN17677 PRE Pre-application

Old and New Business

Adjournment

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**Call to Order (Attendance, Agenda, Ethics)**

Chair Alan Grainger called the meeting to order at 2:01 PM. Other Design Review Board (DRB) members present were Peter Perry, Susan Bergen, Jeff Boon and Chris Gutsche. Jim McNett and Chuck Depew were absent and excused. City Staff in attendance were Planning Manager Joshua Machen, Planner Kelly Tayara and Administrative Specialist Jane Rasely who monitored recording and prepared minutes. Mr. Grainger welcomed new DRB members Mr. Gutsche and Mr. Boon.

**The Ravine Apartments PLN50039 SPR Follow-up**

Mr. Grainger gave an overview of the project to the new members. Roof slope and retaining a large tree near the building were discussed. Clients were invited in at 2:20 PM. Introductions were made all around. Devin Johnson for Johnson Squared presented the changes in roof line and the plan to save a large tree. Mr. Grainger thanked them for the changes in the roofline, saying the DRB appreciated that the bulk of the building was reduced by it. Ms. Bergen asked how they planned to save the tree, i.e., what changes were they going to make. Mr. Johnson replied they were going to move the garbage area back away from the tree and use grassy pavers instead of paving the area. Mr. Machen stated that if they were committed to saving the tree, they needed to come up with a plan for that. Mr. Johnson replied they would work with an arborist to come up with a plan for saving the tree as well as working with a Geotech engineer to plan changes to the foundation.

**Motion: I move we recommend the plan going forward.**

**Perry/Bergen: Passed unanimously 5-0**

While waiting for the BIFD architects (held up by ferry), Planner Tayara briefed the DRB about the Montessori Country School application. Mr. Gutsche asked if he needed to recuse himself for having worked with the Montessori Country School about 10 years ago. Mr. Grainger did not feel he needed to since it was so long ago, but agreed he should do so if he felt it necessary. Planner Tayara mentioned one item up for discussion was traffic and the need to keep traffic from backing up on the road during certain times of day.

**BIFD Station 21 – Pre-application**

Brett Hansen and Chauncey Drinon from Mackenzie Architects were introduced. Mr. Hansen gave an overview of their part in the design process from pre-bond stage to pre-application. He stated all the programming (design) is based on future need. Key particulars driving design were:

- Public parking moved to edge
- Pull through bays
- Medic helicopter pad
- Trees and existing storm water retention to be reused.
- Existing memorial retained
- Vegetation around helipad
- Public plaza.

There was discussion of the roof line and whether the lines would remain clean or would mechanical equipment be placed upon it. Mr. Hansen stated yes they would remain clear with the exception of ham radio antennas that would be mounted on one of the sides of the building. The number and sizes of bays were discussed as well as the size of the roof line (300 feet) and how to break up the look of it. The approach from the south when driving up Madison Avenue was also reviewed. Mr. Hansen summarized the concerns the DRB had and gave initial thoughts as to where, how and whether it was functional to respond to them. Mr. Grainger then brought up providing a connection to the existing trail behind the property. Assistant Fire Chief Luke Carpenter stated security was an issue for putting a trail through the property.

### **BIFD Station 22 – Pre-application**

Mr. Hansen gave a brief overview of the plans for Station 22 stating the same materials were being used to maintain a consistent look across the organization. Mr. Gutsche thought the design was good for the more rural setting (as opposed to Station 21) but asked if the pitch of the roof could be changed to allow for a more southern exposure to accommodate future solar panels. The DRB also expressed the desire to raise the red wall with the identifying “22” above the roof line. Mr. Grainger stated they looked forward to seeing this design at the Site Plan Review meeting.

### **Montessori Country School PLN17677 PRE Pre-application**

Introductions were made around the table with the project design team of Russ Hamlet, Brandon Hogue and Kia Micaud. Planner Tayara mentioned the public meeting for this project was being held on January 4, 2016 at the school. Mr. Hamlet expressed the vision statement of this project was to bring the two campuses together on one campus to facilitate student interaction and teacher collaboration. He continued providing an overview of the site plan showing buffers, administrative buildings and classrooms including a proposed future classroom. Ms. Micaud described landscaping, parking spaces utilizing crushed rock instead of paving, fencing, maintenance access gates and play areas. ADA accessibility was remarked upon when Mr. Grainger asked the type of materials used on the pathways that would allow wheelchair access. Mr. Hamlet discussed that deck overhangs were designed with accessibility for solar panels in mind. Storm water drainage was discussed and whether building a rain garden in the buffer would be possible. Mr. Hamlet mentioned they would come back in April or May before they applied for the building permit. Mr. Grainger spoke for the DRB saying they felt very good about this project.

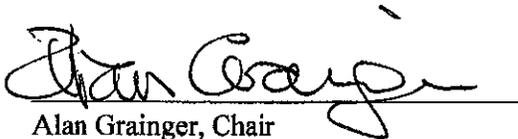
### **Old and New Business**

None.

### **Adjournment**

Meeting was adjourned at 5:05 PM.

Approved by:

  
 Alan Grainger, Chair

  
 Jane Rasely, Administrative Specialist



**CITY OF BAINBRIDGE ISLAND  
DESIGN REVIEW BOARD - REGULAR MEETING**

December 21, 2015

PLEASE PRINT

Name	Affiliation	Phone/ E-Mail	Join ListServ Yes/No
Chris Gutschik		chris@ecosmithdesign.com	yes
JEFF BOON		206-499-3354	yes
PETER PERRY	DRB		
Susan Bergen	DRB		
Alan Grainger	DRB		
EMILT SCALI	Johnson Squared	emily@johnsonsquared.com	no
BILL RAUSOM	WLG (owner)	bjran.com/8@gmail.com	no
CHAUNCEY DRINON	MACKENZIE	cdrinon@mckenze.com	
Luke Carpenter	BIFD	lcarpenter@bifd.org	
BRETT HANSON	<del>DRB</del> Mackenzie	bhanson@mckenze.com	
Kelsey Laughlin	Bronne Wheeler Engineers	kelseyebrownwheeler.com	yes
Meghan Skotheim	Meghan Country School	meghan@meghancountryschool.org	no
Kim Milcaud	Catherine Milcaud Landscape	kim.milcaud@gmail.com	no
RUSS HAMLET	SH-A		OK
Branon Hegg	SH-A		

Call to Order (Attendance, Agenda, Ethics)  
Bainbridge Island Fire Department Station 21 (PLN11791SPR/CUP)  
Bainbridge Island Fire Department Station 22 (PLN14200SPR/CUP)  
Wyatt Cottages (PLN50165SPR)  
New/Old Business  
Adjourn

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**Call to Order (Attendance, Agenda, Ethics)**

Chair Grainger called the meeting to order at 2:06 PM. Other Design Review Board members in attendance were Jim McNett, Chuck Depew and Chris Gutsche. Peter Perry, Susan Bergen and Jeff Boon were absent and excused. City Staff present were Planning Manager Josh Machen and Administrative Specialist Jane Rasely who monitored recording and prepared minutes.

The agenda was reviewed and Mr. Grainger asked that the fire departments be reviewed in the opposite order with review of BIFD Station 22 coming first. There were not any conflicts of interest disclosed.

**Bainbridge Island Fire Department Station 22 (PLN14200SPR/CUP)**

Planning Manager Josh Machen gave an overview of the changes to the project citing the unexpected change in City personnel that occurred on both BIFD projects.

Brett Hansen and Chauncey Drinon from Mackenzie Architects presented the proposed site plans for both fire stations. Mr. Drinon walked the DRB through the changes made to Station 22 stating the biggest change was the removal of the public meeting space. Landscaping elements, types of shrubbery, etc., were highlighted. He stated with the removal of the public meeting room, less impervious surface would be created and more trees would be saved as the entire project would be moved further south on the site. Mr. Grainger asked if there was a site plan available that showed the reduction in parking. Mr. Hanson replied there was not one available at that time. Mr. Gutsche asked how much closer to the street the building would be. Mr. Hanson replied it would be 20-25 feet closer. Mr. Depew asked what the reduction in the building size would be. Mr. Drinon replied the building would go from 15,000 square feet to 14,000 square feet. Color and design materials were presented and described as applicable to both Fire Station 22 and 21.

Mr. McNett asked if there would be any mechanical equipment on the roof. It was stated there would not be any mechanical equipment on either of the proposed fire station roofs. Discussion of the trash enclosure ensued with Mr. Machen reminding the designers that the enclosure must be designed and built in the same style as the building.

**The Design Checklist was reviewed with the following answers given by the DRB:**

1. Variation in facade provided visual interest - Yes
2. Modulate scale of building - Yes
3. Limit visual impact of blank walls and facades - Yes
4. Establish visually prominent ground floor facades - Yes

- 
5. Maintain pedestrian scale along facades - Yes
  6. Maintain pedestrian activities - Yes
  7. Reduce overall scale of building - Yes
  8. Encourage creation of public outdoor spaces – No; Gap in application. Mr. Hanson stated they would submit the landscape plan.
  9. Soften impact of built environment - Yes
  10. Compatible with community and neighborhood characteristics - Yes
  11. Minimize intrusiveness of signage - Yes
  12. Improve pedestrian environment - Yes
  13. Provide pedestrian access - Yes
  14. Provide weather protection for pedestrians - Yes
  15. Maintain smaller scale commercial buildings - Yes
  16. Reduce visual impact of parking areas - Yes

**Motion: I move conditional approval of application subject to submission of a final site plan that conforms to the new design as well as the landscaping plan that relates to that site plan based on their presentation on March 7, 2016.**

**Depew/Grutsche: Unanimous approval.**

#### **Bainbridge Island Fire Department Station 21 (PLN11791SPR/CUP)**

Mr. Hansen provided a review of the key element questions the DRB had during the pre-application presentation. He stated there was a reduction in the size of the building by one equipment bay. Some of the administration function was moved back to the first floor with the bulk staying on the second floor. Mr. Hansen also reiterated they would be using the same materials and design concepts as Fire Station 22 to maintain continuity between fire stations as well as provide recognition from the public. The private areas of the fire station were explored and locations of the gates were shown. Discussion ensued of the roofline over the southern part of the bays/service area. Arguments were presented for moving the “fire tower” with the identifying 21 to the other end of the bays from its current situation with numerous other locations for the “21” explored. Mr. Grainger asked Mr. Machen whether the site plan typically included the landscape plans. Mr. Machen stated it could be part of it, but was not necessarily part of the Design Guidelines. He also pointed out that the landscaping plans were part of the packet and that a presentation by the actual landscape architect had not been expected in the past. The DRB asked for them to come back with landscape information as they were going to do for Station 22.

#### **The Design Checklist was reviewed with the following answers given by the DRB:**

1. Variation in façade provided visual interest - Yes
2. Modulate scale of building - Yes
3. Limit visual impact of blank walls and facades - Left Open to see south elevation again
4. Establish visually prominent ground floor facades - Yes
5. Maintain pedestrian scale along facades - Left Open to see landscape plan
6. Maintain pedestrian activities - Yes
7. Reduce overall scale of building - Yes
8. Encourage creation of public outdoor spaces - Left Open to see landscape plan
9. Soften impact of built environment - Left Open to see landscape plan
10. Compatible with community and neighborhood characteristics - Yes
11. Minimize intrusiveness of signage - Yes
12. Improve pedestrian environment - N/A

13. Provide pedestrian access - Yes
14. Provide weather protection for pedestrians - Yes
15. Maintain smaller scale commercial buildings - Yes
16. Reduce visual impact of parking areas - Yes

**Motion: I move we approve the design as shown with the contingency based on the presentation of the landscape plan by the landscape architect. Approval also contingent upon material requirements brought forth at next meeting (March 7, 2016).**

**McNett/Depew: Passed unanimously**

#### **Wyatt Cottages PLN50165 SPR**

Josh Machen provided an overview of the changes to the design that was seen at the pre-application review.

When the applicant entered, introductions were made around the table. There were two citizens/neighbors present as well as developer James Laughlin and architect Bruce Anderson. Mr. Anderson provided a review of the previous action during the pre-application phase as well as an overview of the current project mentioning that the "tot lot" park was no longer being moved. Mr. Laughlin mentioned that he held an open house at the Oliver house and invited all members of the public who left their e-mails during the pre-application phase to view the new plans which were well received. Mention was made of the two parking spaces the park district asked for the applicant to provide for the park. (BIMPRD actively discourages parking at the "tot lot.") Mr. Grainger asked about the large scale trees in the small scale landscaping. Mr. Anderson stated those would be going closer to the park. Lighting of entry ways as well as the driveway was discussed as "moon" light as opposed to "up" lighting. Mr. McNett asked whether covers for vehicle parking would be allowed. Mr. Laughlin stated if the owners all voted for that (the development would be a condominium) they could do that but it would be written into the CCRs. There was discussion with citizens (see attached sign-in sheet) regarding the acceptability of changes made to the site plan from the previous site plan submitted.

**Motion: I move acceptance of the revised site plan for the project.**

#### **Answers to Design Guidelines were:**

1. Parking lot visually unobtrusive – Yes
2. Open space and amenities – Yes
3. Pedestrian connection – Yes
4. Shielded lighting – Yes
5. Service areas screened – Yes
6. Common open space – Yes
7. Overall form – Very positive
8. Entrances from street clear – Yes
9. Mechanical equipment concealed – Yes
10. Structured parking – N/A
11. Varied details – Yes
12. Integrated signage – N/A



- 13. Creativity – Yes
- 14. Awning signs – N/A
- 15. Landscape front setbacks – Yes
- 16. Strong reference point to key intersections – Yes
- 17. Residential roof forms – Not following normal, but great solution

**Motion: I move acceptance of the revised site plan for the project.**  
**Depew/McNett: Passed unanimously 4-0**

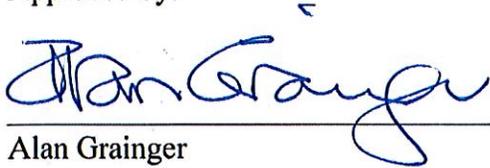
**New/Old Business**

Mr. Grainger informed the other DRB members he would be their representative at the all-day interview process for the new Planning Director on February 23, 2016.

**Adjourn**

The meeting was adjourned at 6:00 PM.

Approved by:

  
\_\_\_\_\_  
Alan Grainger

  
\_\_\_\_\_  
Jane Rasely



CITY OF  
BAINBRIDGE ISLAND

CITY OF BAINBRIDGE ISLAND  
DESIGN REVIEW BOARD - REGULAR MEETING

February 22, 2016

PLEASE PRINT

Join  
ListServ  
Yes/No

Name	Affiliation	Phone/ E-Mail	Join ListServ Yes/No
Chris Gutsche	DRB	chris@ecosmithdesign.com	Y
Jim McNett	DRB		
Alan Grainger	DRB		
Ron Pfeiffer	DRB	vip@ten@bainbrIDGE.WA.GOV	
Chuck Popow	DRB		
Brian Kalkin	Bainbridge Review	editor@bainbridge-review.com	Y
Hank Teresh	FIRE	nteran@BIFD.Org	
Janine Courtemanche	FIRE	flourtemanche@bifd.org	
BRETT HANSON	MACKENZIE	bhanson@mcknzie.com	
CHARLES DRINDON	MACKENZIE	cdrinon@mcknzie.com	
BRUCE ANDERSON	CAA	BRUCE@COTTEL-ANDERSON.COM	
CRAIG SPENCER	CITIZEN	cspencerstudio@gmail.com	
Margaret Celestina	ME16H30R	stability4@gmail.com	
James Laughlin	Applicant	jmlaughlin32@gmail.com	

Call to Order (Attendance, Agenda, Ethics)  
Approval of Minutes - January 4, 2016  
Jones House Site Plan Review (PLN50311SPR)  
Bainbridge Island Fire Department Station 21 (PLN11791SPR/CUP)  
Bainbridge Island Fire Department Station 22 (PLN14200SPR/CUP)  
New/Old Business  
Adjourn

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**Call to Order (Attendance, Agenda, Ethics)**

Chair Alan Grainger called the meeting to order at 2:14 PM. Design Review Board (DRB) members in attendance were Jim McNett, Chuck Depew, Chris Gutsche and Jeffrey Boon. Susan Bergen and Peter Perry were absent and excused. Council Liaison Ron Peltier attended. City Staff present were Planning Manager Josh Machen, Senior Planner Heather Beckmann and Administrative Specialist Jane Rasely who monitored recording and prepared minutes. The agenda was reviewed without any conflicts reported.

**Approval of Minutes - January 4, 2016**

**Motion: I move to approve the minutes.**

**Gutsche/Depew: Passed unanimously**

**Jones House Site Plan Review (PLN50311SPR)**

Jon Thornburgh presented the newest information for the Jones House, one of the two houses his family owns on Ericksen Avenue. He passed out new plans stating the site plans contained in the agenda packet were out of date. Mr. Thornburgh answered the previous questions from the DRB showing pictures of mature trees and giving explanations as to the thought process that went into the site plan. Mr. Grainger engaged in a discussion of moving the staircase in order to facilitate door and dormer placement on the second floor. ADA parking was presented as well. The need for underground parking was canvassed and it was suggested that Mr. Thornburgh check with Project Manager Heather Beckmann about possibilities regarding the height and underground parking correlation and whether underground parking was necessary to accommodate the increased height of the building. Mr. McNett and Mr. Depew agreed it would have been nice to see actual pictures of the current building.

Discussion of the DRB's role in determining land use occurred between project reviews with the Assistance Dogs of Hawaii project used as an example.

Conversation about public buildings and how the requirements are different than for private projects led out of a preview of the landscape plans for BIFD Station 22.

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Chris Gutsche excused himself from the meeting briefly at 3:10 PM.

**Bainbridge Island Fire Department Station 22 (PLN14200SPR/CUP)**

Mr. Brett Hansen introduced Jeff Bouma who spoke about the landscape site design. Mr. Bouma stated they had surveyed and inventoried every tree on both sites as well as updated the buffer diagrams. He said they were meeting the buffer requirements for both sites. He also stated they would be saving 134 out of 140 trees at Station 22. He furthered the discussion by relating the impetus for the plant palate citing an inspirational beautiful Japanese maple tree onsite. Mr. Depew asked about the difference in plants chosen for a rain garden as opposed to other areas on a site. Mr. Hansen spoke about the adjustments in parking due to the change in programming for the building. Removing some of the parking spaces allowed for increased landscaping. ADA parking and the trash closure were revisited. It was stated that full screen buffers were planned for the northern border of the property.

**Bainbridge Island Fire Department Station 21 (PLN11791SPR/CUP)**

Mr. Bouma gave the overview of the plant palate for Station 21 stating there were more evergreens in this palate to provide structure and year round color. He also stated that because of visibility from the road, as well as the big, long apparatus bay, the landscaping would be a little less natural until further out toward the edge of the property. The grouping of trees placed strategically against the large southern wall of the apparatus bays was described. Mr. McNett asked how big the trees would be when they were planted. Placement of trees, benches and perhaps a covered bench at the entrance were discussed. Mr. Gutsche asked what percentage of stormwater was being managed on the site. The answer was 100% was being handled through rain gardens. Mr. Hansen then presented the promised updates.

**Charles Schmid, Citizen** – Asked whether trees within the buffer along Highway 305 would be removed. Mr. Bouma stated none of them would be removed.

Mr. Grainger asked if there had been any thought to connecting the public transportation drop-off point on Highway 305 to the sidewalk on Madison. Mr. Hansen stated a walk lane would be provided by the fog lane.

**Motion: I move that the Fire Stations 21 and 22 have met the contingencies to their approval.**

**Gutsche/McNett: Unanimously approved.**

**New/Old Business**

There was extensive discussion about how to really save trees on properties being developed and how to prevent clear cutting. Tree and building clusters and the efficacy of clustering were questioned. Mr. Grainger wondered why new subdivision developments did not have to come to the DRB. Mr. Peltier asked the DRB if they felt they had the time to take on expanded duties.



**Design Review Board  
Regularly Scheduled Meeting Minutes  
Monday, March 7, 2016**

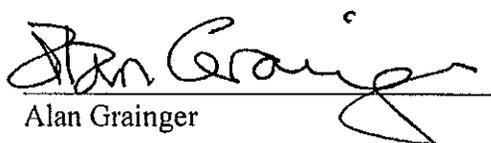
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He also asked them to look at the Land Use Element of the Comprehensive Plan and felt that might be a good place to expand the purview of the DRB. The DRB felt it would be helpful to have a work session with the new Planning Director when he arrives. Mr. Peltier thanked the DRB for their work and said he found their work interesting.

**Adjourn**

The meeting was adjourned at 4:56 PM.

Approved by:

  
Alan Grainger

  
Jane Rasely



# Bainbridge Island Fire Department

## Memo

January 30, 2016

TO: Josh Machen, Planning Department

FR: Assistant Chief Luke Carpenter, Fire Marshal

RE: BIFD Station 21

PLN11791B CUP

The submittal has been reviewed resulting in the following comments:

1. Proposed fire station must be equipped with fire sprinklers and fire alarms as per the adopted Code.
2. The project shall comply with the requirements of the adopted Fire Code and any applicable NFPA standards or other requirements as stipulated by the Fire Marshal.

**ATTACHMENT X**

APR 29  
~~JAN 15~~ 2016

Planning and  
Community Development

Bainbridge Island Fire Department  
Station 21 and 22  
April 15, 2016  
Provided by Fischer Bouma Partnership

## SPR Landscape Plan Supplemental Information

This document is intended to supplement the Landscape drawings for Site Plan Review.

### TREE & VEGETATION PROTECTION STRATEGIES

Trees to be retained as shown on Tree Retention Plan will be protected as required using methods as described in BIMC 18.15.010 (below copied out of current code). These methods are also provided in the project details on Civil drawing sheet Nos. C2.0 and C5.1 and Specification Section 31 10 00.

#### 4. Protection during Construction and Development.

a. Intent. The intent of these regulations is to provide the best protection for significant trees and tree stands, including protection for trees on adjacent properties.

#### b. Requirements.

i. No cutting of significant trees shall be allowed on a site until the tree retention and planting plans have been approved by the director.

ii. In order to preserve future ecological function, the applicant shall identify areas of prohibited disturbance, generally corresponding to the dripline or critical root zone (as identified by a consulting arborist) of the significant trees and/or tree canopy of tree stands to be retained, buffers, areas of existing vegetation to be maintained, future rain gardens, and future planting areas larger than 400 square feet (i.e. landscape islands in parking lots). The prohibited disturbance areas shall be reviewed and approved by the director as part of the land use permit review process.

iii. A temporary five-foot-high chain link fence with tubular steel poles or "T" posts shall delineate the area of prohibited disturbance defined in subsection C.4.b.ii of this section, unless the director has approved the use of a four-foot-high plastic net fence as an



alternative. The fence shall be erected before construction starts and shall remain in place until construction has been completed, and shall at all times have affixed to it a sign indicating the protected area.

iv. No impervious surfaces, fill, excavation, vehicle operations, compaction, removal of native soil or storage of construction materials shall be permitted within the area defined by the required construction fencing. If avoiding construction and compaction in future planting areas is unavoidable, the landscape plan for the project shall include methods for aerating and/or augmenting compacted soil to prepare for new planting, pursuant to subsection H.2 of this section.

v. A rock well shall be constructed if the grade level around the tree is to be raised more than one foot. The inside diameter of the well shall be equal to the diameter of the dripline or critical root zone (as identified by a consulting arborist) of the tree or tree canopy of tree stands.

vi. The grade level shall not be lowered within the larger of (A) the dripline or critical root zone (as identified by a consulting arborist) of the tree, or the tree canopy of tree stands, or (B) the area recommended by a consulting arborist.

vii. Alternative protection methods may be used if recommended by a consulting arborist and determined by the director to provide equal or greater tree protection.

viii. Wherever this subsection C.4 allows or requires the involvement of a consulting arborist, that individual shall be selected from the city's list of current arborists certified by the American Society of Consulting Arborists and his or her services shall be paid for by the applicant.

## **VEGETATION CLEARING STRATEGIES**

Clearing of vegetation is controlled by Specification Section 31 10 00 Site Clearing being provided by the Civil Engineer.

Vegetation will be cleared by typical mechanical means in areas proposed for development. The contractor will be required to protect and preserve trees and vegetation which are outside the clearing limits or



indicated to be protected as indicated on the plans. Trees, underbrush, and all vegetation indicated to be cleared will be done in a manner to protect adjacent property and items on-site to remain.

### **TOPSOIL PROTECTION & RE-USE STRATEGIES**

Topsoil protection and re-use are controlled by Specification Sections 31 10 00 Site Clearing being provided by the Civil Engineer and Specification Section 32 29 00 Landscape Planting provided by the landscape architect, respectively.

Topsoil to remain in place will be protected through vegetation protection strategies and temporary erosion control measures implemented during construction. Native topsoil in areas to be developed will be removed to its full depth and stockpiled on site. All stockpiled topsoil will be covered. Stockpiles will be constructed in accordance with FWRC and WAC requirements. Side slopes will be appropriate for the material to prevent sloughing, erosion, or instability. Stockpiled soil should be screened so that it is free of clay clumps, rubble, cobbles, rubble or any other material that may limit plant growth. One hundred percent of the planting soil should pass through a  $\frac{3}{4}$ " sieve. See the following section for re-use strategies of amended (if necessary) native topsoil that has been stockpiled.

### **NATIVE SOIL AMMENDMENT STRATEGIES**

Native soil amendment strategy is controlled by Specification Section 32 29 00 Landscape Planting.

Native soil in areas to be developed will be stripped and stockpiled per the section above. On-site soils fall in two categories: 1) those stripped and stockpiled and 2) those that were undisturbed (where final grades match existing grades) and that will have enhancement plantings added to them. Both stockpiled and undisturbed native topsoil will then be tested to determine the necessary amendments. Based upon the test results, state recommendations for soil treatments and soil amendments are to be incorporated to produce satisfactory planting soils suitable for health, viable plants. Organic content for the soil shall also be tested and the soil amended so that it is 3-8% by weight per LOI.

Prior to placing a given depth of amended topsoil, the subgrade will be disturbed/loosened to a determined depth by scarification, discing, or ripping to enable a rototiller to fully incorporate a compost amendment. The amended topsoil will then be placed on the prepared subgrade to a depth determined by analysis of the existing soils on site. Due to the typical mineral composition of the glacial till soils in this region, excavated areas to be planted with trees will typically require a 12" to 18" depth of amended topsoil placed. Areas of shrub, groundcover, and turf planting will require less of a depth of amended topsoil placement.

### **PLANTING TIMES**

Planting time is controlled by Specification Section 32 29 00 Landscape Planting.

Planting should be performed between September 1 and May 31. Planting should not occur outside of this window or when the temperature is below 32 degrees F or above 80 degrees F. Planting should not occur when the soil is completely saturated or when the wind velocity is greater than 25 mph.



## **IRRIGATION**

Irrigation is controlled by Specification Section 32 81 00 Landscape Irrigation. New landscape will be irrigated temporarily for establishment or permanently depending on location and typology.



Katy Bigelow  
206.351.1375  
arboristkaty@gmail.com

April 19, 2016

ATTN: Jeff Bouma  
Fischer Bouma Partnership  
310 Madison Avenue South, Suite A  
Bainbridge Island, WA 98110

Chief Luke Carpenter at:  
Bainbridge Island Fire Department  
8895 Madison Ave.  
Bainbridge Island, WA 98110

Dear Mr. Bouma and Mr. Carpenter:

Thank you for having me evaluate trees at the redeveloping Fire Station 21 on Bainbridge Island. To evaluate the trees addressed in this memo I combined my field experience and education with current accepted practices as defined by the American National Standards Institute (ANSI) and the International Society of Arboriculture (ISA).

The tools I use to make an assessment are limited to a rubber mallet, binoculars, compass, laser pointer, hand brush, shovel and hand trowel unless otherwise noted. A visual tree assessment and other methods are only conclusive for the day of inspection and do not guarantee that conditions will remain the same in the future.

-----

I was asked by Mr. Bouma and Mr. Carpenter to assess significant trees in and near the buffer areas of the proposed redevelopment of the Fire Station 21 property. I was specifically asked to determine the health of the trees, if they were reasonable candidates to retain during construction that would retain vigor after construction, and to determine their drip lines corresponding to where tree protection would need to be installed before the project began. I completed my assessment on April 13, 2016.

I evaluated 23 trees growing in the buffer areas on the west, north and south sides of the site (see Site Plans). These areas are populated with a variety of mainly ornamental shrubs and trees.

Five of the trees growing in the buffer were in poor condition and had poor structure OR grow too close to the project clearing boundary to reasonably be retained (58, 59, 237, 238 and 245). They should be removed. The remaining trees are in fair or good condition and can be retained with tree protection installed to the drip lines or as otherwise specified. Refer to the Supporting Data for specific information on each tree.

**ATTACHMENT Z**

Tree assessment memo -- Bouma Fischer  
Bainbridge Fire Station 21, Madison Ave. BI  
4/19/16

Several trees were growing on the boundary to the buffer zone (Trees 47, 52, 56, 60 and 63). These trees are in fair condition but likely will lose up to 50% of their critical roots during grading processes. I recommend these trees are removed.

When possible, use tree protection fencing to surround trees close to each other instead of encircling individual trees. Buffer areas can be fenced off for tree protection at clearing boundaries. I strongly recommend installing tree protection fencing prior to the start of any clearing and grading activities.

Many of the "trees" slated for removal in the west boundary are actually Photinia shrubs. They can be retained as desired to maintain a screen. This species can tolerate root activity close to their trunks.

Thank you very much for calling me for your arboricultural concerns.

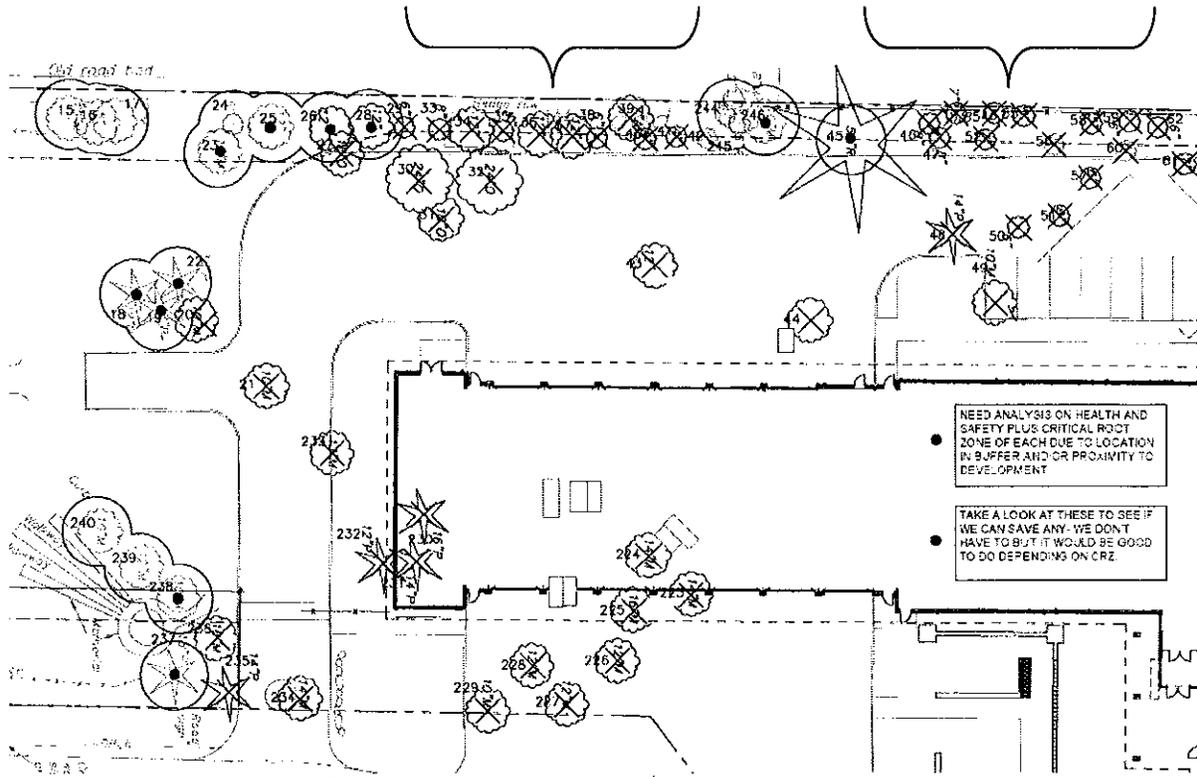


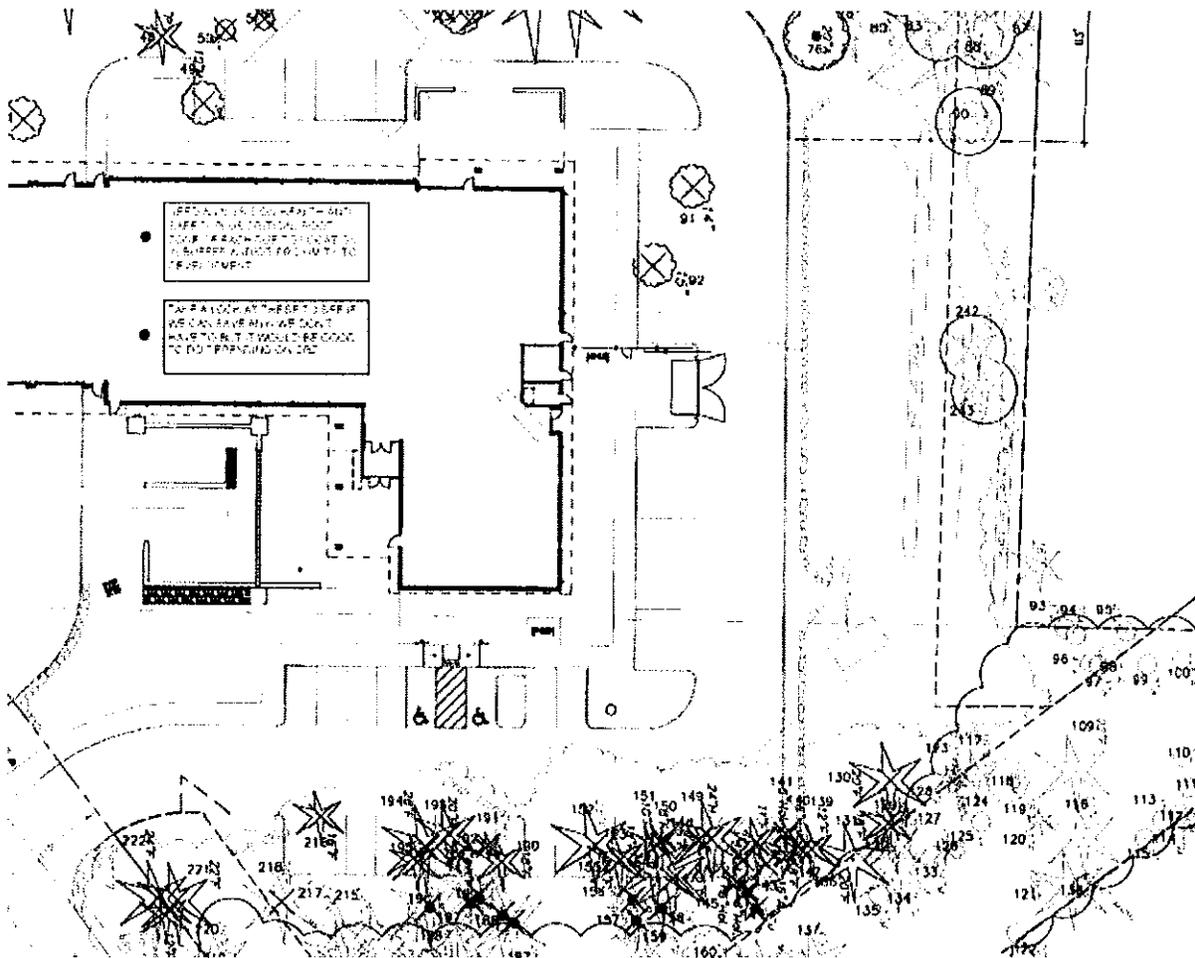
*Katy Bigelow*  
*PNW ISA member # PN-3069AT*  
*PNW Certified Tree Risk Assessor # 199*  
*Registered Consulting Arborist® #490*

### Site Plans

Most of these are Photinia shrubs.

Mainly Photinia.





Red dotted trees were determined to not be good candidates for retention.

### Supporting Data

TREE #	SPECIES	DBH	DRIP LINE	CURRENT HEALTH RATING	COMMENTS/ RECOMMENDATIONS
237	Scotch pine/ <i>Pinus sylvestris</i>	21.0"	-	Fair	Previously topped, canopy is majorly asymmetrical, poor structure overall. Remove
238	Sycamore/ <i>Platanus occidentalis</i>	8.0"	-	Poor	Canopy is majorly asymmetrical, not a good candidate for retention after exposure. Remove
18	Pine species/ <i>Pinus species</i>	16.0"	To existing curb	Fair	Retain 18, 19 and 22 as a grove. Install tree protection encircling all three trees.
19	Pine species/ <i>Pinus species</i>	17.0"	To existing curb	Fair	Retain 18, 19 and 22 as a grove. Install tree protection encircling all three trees.
22	Pine species/ <i>Pinus species</i>	22.0"	To existing curb	Fair	Retain 18, 19 and 22 as a grove. Install tree protection encircling all three trees.
23	Oak species/ <i>Quercus species</i>	14.0"	*	Fair	* = to existing asphalt to E, N/S 7' W into buffer. Trunk forked at 7'
25	Photinia/ <i>Photinia × fraseri</i>	multi	To buffer edge	Fair	Considered a shrub. Can be retained and pruned down as desired without effect to its vigor.
26	Photinia/ <i>Photinia × fraseri</i>	multi	To buffer edge	Fair	Considered a shrub. Can be retained and pruned down as desired without effect to its vigor.
28	Photinia/ <i>Photinia × fraseri</i>	multi	To buffer edge	Fair	Considered a shrub. Can be retained and pruned down as desired without effect to its vigor.
246	Scouler's willow/ <i>Salix scouleriana</i>	multi	To buffer edge	Fair	Over 50 stems in clump. Most stems < 3" DBH. Remove stems within three feet of new curb. Most stems will have to be removed.
244	Scouler's willow/ <i>Salix scouleriana</i>	multi	To buffer edge	Fair	Over 50 stems in clump. Most stems < 3" DBH. Remove stems within three feet of new curb.
245	Apple species/ <i>Malus</i>	2.5"	-	Poor	Poor form. Remove.

	<i>species</i>				
45	<i>Sequoia/Sequoia sempervirens</i>	45.0"	at least 15' all directions	Good	Trunk forked. Would need at least 15' radius of non-disturbance from its base free from any hardscape to reasonably be retained.
46	Red maple/ <i>Acer rubrum</i>	8.0"	To buffer edge	Fair	Retain
47	Red maple/ <i>Acer rubrum</i>	7.0"	-	Fair	Too many critical roots will be disturbed for reasonable retention. Remove.
52	Red maple/ <i>Acer rubrum</i>	7.0"	-	Fair	Too many critical roots will be disturbed for reasonable retention. Remove.
56	Red maple/ <i>Acer rubrum</i>	5.0"	-	Fair	Too many critical roots will be disturbed for reasonable retention. Remove.
58	Scouler's willow/ <i>Salix scouleriana</i>	7.0"	-	Poor	Open wound in the low trunk with rot. Leans to the west. Remove.
59	Scouler's willow/ <i>Salix scouleriana</i>	8.0"	-	Poor	Leans towards project area. Poor structure. Remove.
60	Red maple/ <i>Acer rubrum</i>	7.0"	-	Fair	Too many critical roots will be disturbed for reasonable retention. Remove.
62	Red maple/ <i>Acer rubrum</i>	6.0"	To buffer edge	Fair	Retain
63	Red maple/ <i>Acer rubrum</i>	6.0"	-	Fair	Too many critical roots will be disturbed for reasonable retention. Remove.
76	Red alder/ <i>Alnus rubra</i>	30.0"	To existing curb	Fair	Open wound in the trunk at area trunks fork at 6'. Mature specimen of this species.
<b>LEGEND</b>					
Green font indicates trees standing on buffer line.					
Tree #: Tree number corresponding with surveyed map number					
Species: Common and Latin tree name					

Tree assessment memo – Bouma Fischer  
 Bainbridge Fire Station 21, Madison Ave. BI  
 4/19/16

<b>DBH:</b> Trunk diameter(s) at 4.5' above average ground level.		
<b>Drip Line:</b> A horizontal area equal to the maximum extent of branches and leaves in a certain direction. Distance from trunk at which to place tree protection fencing.		
<b>Current Health Rating:</b> A description of general health ranging from dead, dying, hazard, poor, suppressed, fair, good, very good, to excellent.		
<b>Comments/Recommendations:</b> Comments relative to the recommendations concerning the tree being a good candidate for retention.		

## Assumptions, Limiting Conditions and General Waiver

I, Katy Bigelow, certify that:

I have personally inspected the tree(s) and or the property referred to in this report;

I have no current or prospective financial or other interest in the vegetation or the property which is the subject of this report and have no personal interest or bias in favor of or against any of the involved parties or their respective position(s), if any;

The analysis, opinions and conclusions stated herein are the product of my independent professional judgment and based on current scientific procedures and facts, and the foregoing report was prepared according to commercially reasonable and generally accepted arboricultural standards and practices for the Pacific Northwest and Puget Sound areas;

The information included in this report covers only those trees that were examined and reflects the condition of the trees as of the time and date of inspection;

This report and the opinions expressed herein are not intended, nor should they be construed, as any type of warranty or guarantee regarding the condition of the subject trees in the future;

Covenants, Conditions, and Restrictions (“CC&Rs”) may restrict the number, type and height of vegetation on the subject property, and I have made no investigation regarding whether the property is subject to such CC&Rs; and

To the best of my knowledge and belief, all statements and information in this report are true and correct and information provided by others is assumed to be true and correct.

I am not an attorney or engineer. This report does not cover these areas of expertise and represents advice only of arboricultural nature. Without limiting the generality of the preceding sentence, it is specifically understood that nothing contained in this report is intended as legal advice, or advice or opinions regarding soil stability or zoning laws, and this report should not be relied upon to take the place of such advice.



*Katy Bigelow*  
*PNW ISA member # PN-3069AT*  
*PNW Certified Tree Risk Assessor # 199*  
*Registered Consulting Arborist® #490*

# MACKENZIE.

DESIGN DRIVEN | CLIENT FOCUSED

October 17, 2016

City of Bainbridge Island  
Attention: Janelle Hitch  
280 Madison Avenue N.  
Bainbridge Island, WA 98110

Re: **Bainbridge Island Fire Department - Station #21**  
*Parking Analysis Letter*  
Project Number 2150124.00

Dear Ms. Hitch:

Mackenzie has prepared this letter to satisfy City of Bainbridge parking requirements for the subject fire station application.

The 28,350-square-foot fire station is proposed at 8895 Madison Avenue N in Bainbridge Island, Washington. The proposed fire station will replace an existing 14,230-square-foot fire station at the same site. The existing fire station has 75 striped parking spaces which have been observed to be underutilized.

The proposed fire station's site plan proposes 43 striped parking spaces. A majority of these spaces are proposed to serve a 1,674-square-foot community room within the fire station. The City's Development Code, Table 18.15.020-1, requires a minimum of 10 parking spaces per 1,000 square feet of assembly area. Based on this requirement, the fire station's community room will require a minimum of 17 parking spaces.

Table 18.15.020-1 of the City's Development Code does not include a specific minimum parking requirement for fire stations, but rather requires an adequate number of parking spaces to accommodate peak shift employees for governmental facilities. A trip generation letter, dated January 18, 2016, and revised August 24, 2016, reported that the number of employees would remain unchanged for the proposed new fire station. The existing fire station has six (6) dayshift employees and five (5) shift employees, assuming a shift rotation every 48 hours. As a worst-case scenario, it is assumed that all employee parking will need to be accommodated during shift changes, resulting in a peak demand of 11 parking spaces on-site. Emergency vehicles will not require passenger vehicle parking spaces since those vehicles will be stored in the apparatus bays.

In summary, the community room is required to have a minimum of 17 parking spaces and the fire station is anticipated to have an 11-space parking demand during peak times, for a total minimum parking requirement of 28 spaces. It is anticipated the proposed 43 striped parking spaces will far exceed the minimum 28-parking space requirement, allowing for additional parking for visitors, deliveries, and others, as needed.

Please let us know if you have any questions or comments regarding the information in this letter.

Sincerely,



Brent Ahrend  
Senior Associate | Traffic Engineer

Enclosure(s): Site Plan

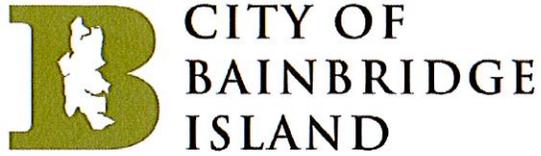
c: Michael Chen, Brett Hanson, Janet Jones - Mackenzie

**M.** P 503.224.9560 • F 503.228.1285 • W MCKNZE.COM  
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Portland, Oregon • Vancouver, Washington • Seattle, Washington

1100 Portland, OR 97214

LANDSCAPE ARCHITECTURE

**ATTACHMENT A A**



DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT

STAFF REPORT

Date: October 19, 2016  
To: City of Bainbridge Island Planning Commission  
From: Joshua Machen, AICP  
Planning Manager  
Project: Bainbridge Island Fire Department Station 22  
Site Plan and Design Review/ Conditional Use Permit  
File Number: PLN50231 SPR/CUP

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I. INTRODUCTION

**Applicant:** Mackenzie  
C/O Michael Chen  
500 Union Street, Suite 545  
Seattle, WA 98101

**Owner:** Bainbridge Island Fire Department

**Request:** The proposed project is the redevelopment of the existing fire station on the site, Station #22. The redeveloped fire station will have a gross floor area of approximately 16,808 square feet with 30 parking spaces (Attachments B & C).

**Location:** 7934 Bucklin Hill Road NE, being portions of Sect. 28, T. 25N. R. 02E. W.M. Tax parcel numbers 4178-000-012-2004

**Environmental Review:** A SEPA Threshold Determination was issued on October 11, 2016

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**Recommendation:**

Approve the application with the following conditions:

**SEPA Conditions**

1. All graded materials removed from the subject property shall be hauled to and deposited at City approved locations (Note: local regulations require that a grade/fill permit is obtained for any grading or filling of 50 cubic yards of material or more if the grading or filling occurs on sites that have not been previously approved for such activities. A SEPA Threshold Determination is required

for any fill over 100 cubic yards on sites that have not been previously received a SEPA determination).

2. Contractor is required to stop work and immediately notify the Department of Planning and Community Development and the Washington State Office of Archaeology and Historic Preservation if any historical or archaeological artifacts are uncovered during excavation or construction.
3. To mitigate the possible impact on adjacent properties from light and glare, all exterior lighting shall be hooded and shielded so that the bulb is not visible from adjacent properties. All landscape lighting shall be downcast and lighting within surface parking lots shall be no higher than 14 feet above grade. All exterior lighting shall comply with BIMC Chapter 15.34.
4. Prior to any clearing or grading on the site, chain-link construction fencing shall be installed and inspected by the City at the edge of the tree's dripline for trees being preserved as part of the development.
5. All construction activities shall comply with the construction operating hours limitations contained in BIMC Chapter 16.16. Noise produced by this development must comply with the maximum environmental noise levels established by the Washington Administrative Code 173-60 or its successor.
6. All on-site stormwater facilities shall remain privately owned and maintained. The owner shall be responsible for maintenance of the storm drainage facilities for this development following construction. Annual inspection and maintenance reports shall be provided to the City. A declaration of Covenant for stormwater system operation and maintenance will be required to be recorded before issuance of occupancy permits. The approved language for the Declaration of Covenant is found in BIMC 15.21.

Project Conditions:

7. The site shall be developed in substantial conformance with the revised site plans date stamped April 29, 2016 and building elevations date stamped received October 18, 2016, except to conform to these conditions.
8. The applicant shall obtain an approved building and/or grading permit from the Department of Planning and Community Development, prior to any construction activities on the site.
9. Sign permits shall be obtained as required by BIMC Section 15.08.
10. Parking shall be improved in substantial conformance with the approved site plan. The parking area shall be paved, all stalls shall be striped to their full dimensions and appropriate signage shall be placed at each handicap stall(s). Each parking stall shall meet the dimensional standards of BIMC Table 18.15.020-3 outside of required driving aisles or minimum sidewalk width of five feet.
11. The primary walkways throughout the development shall meet accessibility requirements including being surfaced with nonskid hard surfaces and providing a minimum of five feet of unobstructed width.

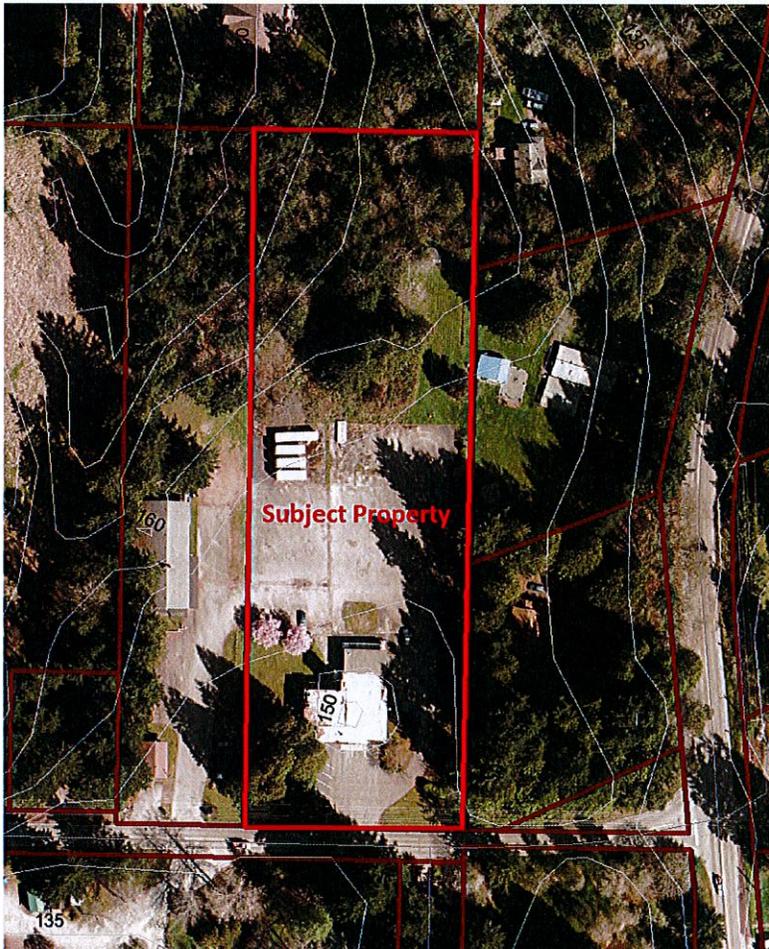
12. A minimum of 4 bicycle spaces shall be installed prior to final inspection. The rack shall allow for the wheel and frame of the bicycle to be locked. The bicycle stalls shall provide for both visitors and employees of the station.
13. The location of generator pad on the east side of the property shall be relocated outside of the minimum 15-foot perimeter buffer. Prior to building permit issuance revised drawings depicting this change shall be submitted and approved.
14. All landscaping shall be installed as provided in the approved landscaping plans or a performance assurance device shall be submitted and approved, prior to final inspection of the station. The installation of landscaping shall be verified by a Landscape Professional and a landscaping declaration shall be signed by either the certified landscape professional or owner.
15. Prior to occupancy of the station or the release of a landscaping performance assurance, a landscaping maintenance assurance device for the required landscaping shall be provided to COBI for a period of three years. All landscaping and buffers shall be maintained for the life of the project.
16. An International Society of Arboriculture (ISA) valuation for all trees, where the critical root zone of a tree required to be retained may be impacted by clearing, grading construction, development, or maintenance, shall be submitted to issuance of the building permit for the station.
17. In order to define the circulation system and pedestrian separation, raised curbs shall be used to separate landscaping and raised walkways from parking stalls and drive aisles.
18. Exterior trash receptacles/recycling facilities shall be fully screened with solid walls and gates (no chain-link fencing). The screening enclosures shall be architecturally consistent with the adjacent station. All enclosures shall be constructed and inspected prior to final inspection.
19. At the time of building permit submittal, detailed lighting plans demonstrating compliance with the lighting standards shall be submitted for review and approval by the COBI.
20. All mechanical equipment shall either be located underground, incorporated into landscaping or integrated within the building or roof form of the building. Parapet walls may be used to screen roof top mechanical equipment as long as equipment is completely obscured from view and the parapet does not substantially detract from the building architecture.
21. Proposed fire station must be equipped with fire sprinklers and fire alarms as per the adopted code.
22. The COBI Non-Motorized 6-year Capital Improvement Plan includes the C40-Bucklin Phase 2 Project that extends along the frontage of the Bainbridge Island Fire Station 22. Frontage improvements along Bucklin Hill Road NE corresponding to the CIP C40 Bucklin Phase 2 Project must be completed or a fee-in-lieu must be provided to the City prior to Certificate of Occupancy and /or Final Inspection. Frontage improvements include pedestrian sidewalk and bike lane.

## Staff Analysis

### II. FINDINGS OF FACT

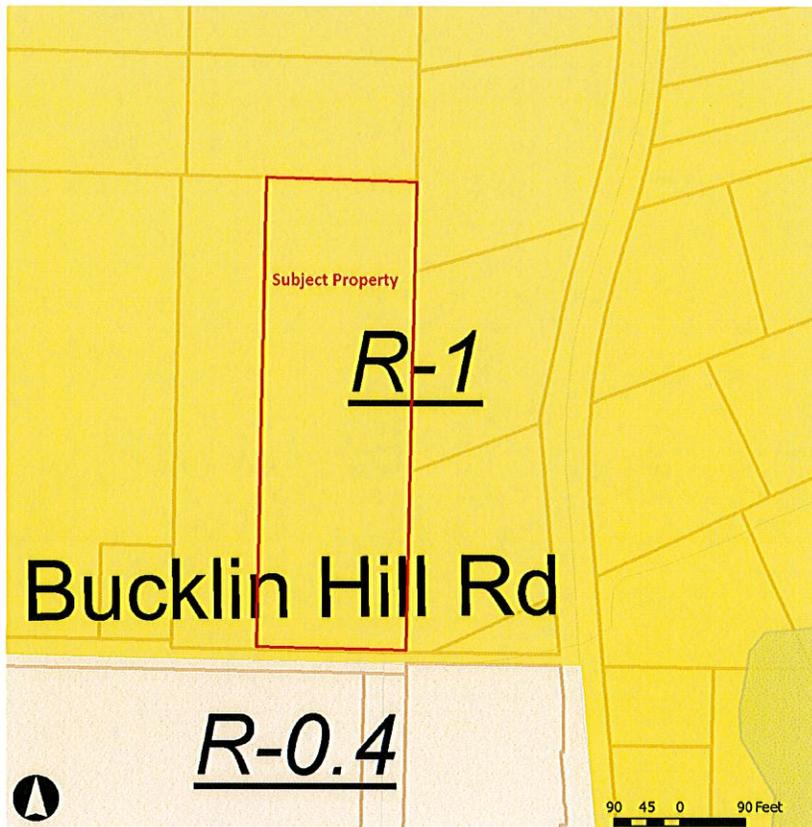
#### A. Site Characteristics

1. ASSESSOR'S RECORD INFORMATION:  
4178-000-012-2004, Bainbridge Island Fire District, 3.05 Acres
2. TERRAIN:  
The property relatively flat with a gentle slope up in the northern portion of the property. The property has just a few trees adjacent to the existing fire station and native trees and shrub forest in the north 100-150 feet of the property. (Attachments B & E)



3. SITE DEVELOPMENT/EXISTING USE:  
The site contains an existing fire station and related improvements, including parking and septic system.

4. ACCESS:  
Vehicular access to the site is from Bucklin Hill Road
5. PUBLIC SERVICES:
  - a. Police - Bainbridge Island Police Department
  - b. Fire - Bainbridge Island Fire District
6. EXISTING ZONING/ COMPREHENSIVE PLAN DESIGNATION:  
R-1, 1 unit per acre residential zone  
OSR-1, Open Space Residential, one unit per acre



7. SURROUNDING ZONING/COMPRHENSIVE PLAN DESIGNATION:
  - a. North: R-1
  - b. East: R-1
  - c. South: R-0.4
  - d. West: R-1
8. SURROUNDING USES:
  - a. North: Single-family Residential Development
  - b. East: American Legion Hall
  - c. South: Hyla, a Private School
  - d. West: Single-family Residential Development

B. History

1. A pre-application conference was held December 1, 2015
2. The Design Review Board reviewed the pre-application proposal on December 21, 2015 and made several recommendations on the design (Attachment U).
3. A public participation meeting was held at the City on January 11, 2016. Comments and responses from that meeting are included as Attachment A.
4. A formal application for the Site Plan and Design Review and Conditional Use Permit were submitted on January 15, 2016.
5. On February 22, 2016, the application was taken back to the Design Review Board, the board reviewed the revised drawings and recommended approval of the application (Attachment V).
6. Revisions to the application and submission documents were submitted on March 4, 2016.
7. A notice of technically complete application was issued on March 9, 2016, but requested additional information to be provided prior to the complete processing of the applications.
8. A Notice of Application/SEPA comment period was published and mailed out March 11, 2016.
9. Revisions to the application were received on April 29, 2016
10. On May 27<sup>th</sup> a memo from the City's Development engineer indicated the application was still deficient proper documentation regarding stormwater.
11. On June 7, 2016 a Preliminary Technical Information Report (drainage report) was submitted to the City.
12. On June 22, 2016 the Development Engineer provided a memo indicating that the revisions received from the applicant in April and the drainage report received in June were still deficient and that additional information and revisions were required.
13. On September 23, 2016 revised stormwater drainage plans were received.
14. The City Development Engineer reviewed the project and provided final comments and recommended conditions on October 13 (Attachment T).

C. Public Comment

No public comment was received during the official public comment period.

D. Comprehensive Plan Analysis

**OS 4.1 The R-1 District is intended to recognize an existing development pattern of one unit per acre.** The subject development is proposed within the residential one-acre zone. The Fire station is an essential public facility and is replacing an existing fire station. The changes to the station are being properly reviewed through the Site Plan and Design Review process as well as the Conditional Use Permit process to ensure compliance with the code requirements as well as compatibility with neighboring development

**SD 1.3** The City shall require new development to provide both on-site improvements and off-site improvements necessary to avoid adverse downstream water quality and quantity impacts. **The applicant has submitted an engineered comprehensive stormwater drainage plan that addresses downstream water quality and quantity impacts. The submitted plans were reviewed by and found to meet local and state regulations (Attachments T).**

**SD 1.4** Where appropriate and feasible, infiltration of stormwater is preferred over surface discharge to downstream systems. The return of uncontaminated precipitation to the soil at natural rates near where it falls should be encouraged through the use of detention ponds, grassy swales and infiltration facilities. **The stormwater drainage plan submitted with the application includes several of these features, including but not limited to raingardens, and infiltration/dispersal systems that will allow water to return to the soil.**

E. Land Use Code Analysis

1. **BIMC Chapter 16.16 Noise Regulations**

The proposal is subject to noise regulations related to the construction hours and maximum environmental noise levels from the proposed uses on the property. Since the construction for this proposal will occur within 100 feet of residentially zoned properties, construction activities are limited to certain hours and days of the week in accordance with BIMC 16.16.025. The maximum allowed environmental noise levels are those allowed by state law.

2. **BIMC Chapter 18.06.0020C**

The purpose of the R-1 zone is to provide residential neighborhoods in an environment with special island character consistent with other land uses such as agriculture and forestry, and the preservation of natural systems and open space. **The proposed reconstructed fire station is consistent with the purpose of this district as it is an essential public facility that directly serves the residential neighborhoods of the island in addition to the commercial zones. The subject property is well suited for a fire station as it only has residential uses on two sides and the closest neighbor, which is to the east of the station has maintained a 40 buffer of existing trees and vegetation between their house and the fire station. The fire station will be providing a 15-foot full screen buffer along their neighboring property line.**

3. **BIMC Chapter 18.09.020 Permitted Use Table**

The permitted use table lists the multiple uses that are that are permitted and conditionally permitted within the R-1 zone. Included in those allowed by a conditional use permit are governmental facilities.

4. **BIMC Chapter 18.12 Dimensional Standards**

The proposed development is within the R-1 zone, therefore the maximum lot coverage for is 15 percent. **The proposed lot coverage for this development is approximately 10.7 percent overall (approximately 14,314 square feet of building footprint).**

The setback from the Bucklin Hill Road right-of-way is a minimum of 25 feet in order to provide the required landscape buffer and front yard setback. **The proposed building is setback approximately 95 feet from Bucklin Hill Road. The required side setbacks are 5' minimum, however the perimeter buffers are larger. The north and east property line has a minimum buffer or 15 feet with an average of 25 and the buffer along the west property line the buffer is a minimum of 10 feet with an average of 20-feet.**

The building height limitations for the R-1 zone is 30 feet to the mid-point of the highest gable. **The tallest portion of the proposed building has a height of 24 feet.**

5. **BIMC Chapter 18.15 Development Standards and Guidelines**

a. **18.15.010 Landscaping and Screening**

Since the proposal is located within the R-1 zone, the project has a required 25-foot average perimeter full-screen landscape buffer along the single-family use to the north east and a 20-foot average perimeter full-screen landscape buffer to the American Legion property to the west. The development is also required to provide a 25-foot partial screen buffer to the Bucklin Hill Road right-of-way. **The proposed development is providing the full 25 foot full-screen landscape buffer to the residential property along the north property line and is providing an averaged 25-foot full landscape buffer along the east property line. The proposal also includes a 20-foot averaged buffer along the west property line to accommodate fire apparatus access driveway. The proposal will also provide a landscape buffer along Bucklin Hill Road with the exception of the entrance driveway.**

The parking lot landscaping requirement for the proposed development entails providing one tree for every two parking stalls for the parking lot located in front of the building and one tree for every 4 for the parking lots located to the side or rear of the building. **The project has a 6 parking stalls out front and 13 stalls behind; therefore 7 parking lot related trees will need to be planted. The applicant has proposed planting close to 8 new trees around the parking lot and building outside of the required buffers, which is in excess of the minimum required for the parking lot landscaping, the required buffers and the tree unit requirements. Also as required the landscaping plan properly designates landscape areas at the end of the parking stalls and aisles (Attachment P & Q).**

The total site tree requirements for non-residential development within the R-1 zone is 40 tree units per acre. Each existing tree that is being preserved is given a tree unit value based on its size, then all proposed trees to be planted are also given a tree unit value. The property is 2.18 acres outside the required buffers; therefore, a total of 87.2 tree units would typically be required outside of the significant trees being preserved within required perimeter buffers or the roadside buffers. **The proposal is retaining 193.2 tree units (note: only 87.2 units are required retention tree units) and is proposing to plant 100 trees. While some of the replanted trees are required for the buffers and the parking lot tree planting requirements, the proposed retained trees in addition to the planted trees exceed the minimum tree unit requirements. As conditioned, all landscaping and buffers need to be maintained throughout the life of the project and the proposed landscaping must be installed or an assurance device provided prior to any occupancy of the buildings (Condition 15).**

Retained trees within buffers and those needed to be retained to meet the tree unit requirements require protection during construction. **The applicant has proposed tree protection strategies in compliance with the municipal code requirements including but not limited to construction fencing, dripline protection, soil amendment, and root pruning (Attachment Y). In addition, the City required and the applicant provided a Certified Arborist analysis of any tree proposed to be retained whose dripline/critical root zone would be affected by the construction. Her review, analysis and recommendations are included in Attachment Z.**

b. **18.15.020 Parking and Loading**

The proposed building is proposed as a fire station facility; therefore, the parking regulations require enough spaces to meet the peak demand as verified by the Director. **As proposed, there are 19 parking spaces proposed, which meets the peak parking demand forecasted by the Bainbridge Island Fire Department, 8 spaces. While the Director recognizes the benefit of additional spaces, only the Planning Commission may approve parking spaces above the required spaces, which is the peak demand in this special case (Attachment AA).**

While recommended by this section of code the locations of the parking in front of the building is discouraged, the parking spaces in front of the building will be primarily for the public while the parking to the side and rear will be primarily reserved for fire fighters. As conditioned, each parking stall is to meet the dimensional standards of Table 18.15.020-3 outside of required driving aisles or minimum sidewalk widths (Conditions 10).

c. **18.15.030 Mobility and access**

In order to define the circulation system and pedestrian separation, raised curbs shall be used to separate landscaping and raised walkways from parking stalls and drive aisles (Condition 17). In accordance with the regulations regarding mobility and access, all internal walkways shall be surfaced with nonskid hard surfaces and provide at least five feet of unobstructed width (Condition 11).

One bicycle space is required for every five parking spaces, therefore, four bicycle spaces are required for the proposed station. The site plan properly designates 4 available spaces that will serve both employees and the public (Condition 12).

d. **18.15.040 Outdoor Lighting**

In order to preserve and enhance the view of the dark sky and promote health, safety and security, outdoor lighting is to be shielded and maintained so there is no light trespass. All lighting shall be downcast and shielded such that it masks the horizontal surface of the light source. At the time of building permit submittal, detailed lighting plans demonstrating compliance with the lighting standards shall be submitted for review and approval by the City (Condition 3).

6. **BIMC Chapter 18.18 Design Guidelines**

The proposed project is subject to; the commercial and mixed-use design guidelines for all zoning districts. The application was first reviewed by the Design Review Board on December 21, 2015 during the pre-application conference phase of review. The Design Review Board made several suggestions during that meeting and the applicant responded by making several modifications to the design including but not limited to addressing the proposed entrance and front façade of the building (Attachment U).

The Design Review Board (DRB) again reviewed the proposal after the applicant had submitted the formal Site Plan and Design Review Application on February 22, 2016. The Board reviewed the design line checklist and found the design compliant with the guidelines but wanted to see the landscaping plans at the meeting on March 7, 2016. (Attachment V). At the March 7, 2016 meeting the landscape architect for the project presented the

landscape plan and the DRB finalized their recommendation for approval of the station as designed (Attachment W ).

#### 7. BIMC Chapter 2.16.040 Site Plans and Design Review

The Bainbridge Island Fire Station 22 was properly submitted as a major site plan and design review application along with a major conditional use permit. Since this is a major conditional use permit and site plan and design review application, the applicant first applied for a pre-application conference and had the preliminary plans reviewed by the Design Review Board. The applicants met with the Design Review Board once during the pre-application phase of the project, on December 21, 2015. The application was also required to have a Public Participation Meeting, which was held January 11, 2016. The DRB reviewed the application again as part of their formal submittal on February 22, 2016 (Attachment V) As a Major Site Plan and Design Review, the application is properly before the Planning Commission for review in a public meeting. The Planning Commission is to provide a recommendation to the Hearing Examiner, who will issue a decision on the consolidated project. The staff recommends the Planning Commission recommend approval of the application with the conditions listed in this project report.

#### *Decision Criteria*

The Director and Planning Commission shall base their respective recommendations or decision on site plan and design review applications on the following criteria:

- a. The site plan and design is in conformance with applicable code provisions and development standards of the applicable zoning district; **As summarized in this staff report and in the materials prepared by the applicant, the application, with conditions, is in conformance with the Bainbridge Island Municipal Code.**
- b. The locations of the buildings and structures, open spaces, landscaping, pedestrian, bicycle and vehicular circulation systems are adequate, safe, efficient and in conformance with the non-motorized transportation plan; **The proposed site plan provides a building layout, parking and circulation system that are an efficient use of the available land while providing the required setbacks and screening from the adjacent properties and open spaces.**
- c. The Kitsap County Health District has determined that the site plan and design meets the following decision criteria:
- e. The proposal conforms to current standards regarding domestic water supply and sewage disposal; or if the proposal is not to be served by public sewers, then the lot has sufficient area and soil, topographic and drainage characteristics to permit an on-site sewage disposal system. **The Health District reviewed the project and had no comment.**
  - i. If the Health District recommends approval the application with respect to those items in subsection i., the health district shall so advise the director. **No comment.**
  - ii. If the health district recommends disapproval of the application, it shall provide a written explanation to the director. **The Health District has not denied the application.**
- d. The city engineer has determined that the site plan and design meets the following decision criteria:

- i. The site plan and design conforms to regulations concerning drainage in Chapters 15.20 and 15.21 BIMC; and
- ii. The site plan and design will not cause an undue burden on the drainage basin or water quality and will not unreasonably interfere with the use and enjoyment of properties downstream; and
- iii. The streets and pedestrian ways as proposed align with and are otherwise coordinated with streets serving adjacent properties; and
- iv. The streets and pedestrian ways as proposed are adequate to accommodate anticipated traffic; and
- v. If the site will rely on public water or sewer services, there is capacity in the water or sewer system (as applicable) to serve the site, and the applicable service(s) can be made available at the site; and
- vi. The site plan and design conforms to the "City of Bainbridge Island Engineering Design and Development Standards Manual," unless the city engineer has approved a variation to the road standards in that document based on his or her determination that the variation meets the purposes of BIMC Title 18.

**The Development Engineer has reviewed the submitted civil plans and has found them to be consistent with these requirements. A detailed response and analysis is contained in her memo (Attachment T).**

- e. The site plan and design is consistent with all applicable design guidelines in BIMC Title 18, unless strict adherence to a guideline has been modified as a housing design demonstration project pursuant to BIMC 2.16.020.Q; **The proposed development was reviewed by the Design Review Board during the pre-application stage of the development and during the Site Plan and Design Review process (Attachments U,V & W). The applicant has incorporated the applicable Design Review Board's recommendations into the final design of the buildings and layout of the site. As now proposed, the design of the building form, wall articulation, roof form, landscaping and overall layout is consistent with the intent of the applicable design guidelines.**
- f. No harmful or unhealthful conditions are likely to result from the proposed site plan; **The proposed development is for a use conditionally allowed by the Municipal Code within the R-1 zoning district.**
- g. The site plan and design is in conformance with the Comprehensive Plan and other applicable adopted community plans; **The proposed plan provides for the replacement of an essential public facility.**

- h. Any property subject to site plan and design review that contains a critical area or buffer, as defined in Chapter 16.20 BIMC, conforms to all requirements of that chapter; **No critical areas are identified on the subject property.**
- i. Any property subject to site plan and design review that is within shoreline jurisdiction, as defined in Chapter 16.12 BIMC, conforms to all requirements of that chapter; **The subject property is not within the shoreline jurisdiction.**
- j. If the applicant is providing privately owned open space and is requesting credit against dedications for park and recreation facilities required by BIMC 17.20.020.C, the requirements of BIMC 17.20.020.D have been met; **The requirements of BIMC 17.20.020 are not applicable to this application because the proposal contains no new dwelling units.**
- k. The site plan and design has been prepared consistent with the purpose of the site design review process and open space goals; **The proposed site plan was prepared consistent with the overall purpose and goals of the Site Plan and Design Review process. The process has provided a means for guiding the development in a logical, safe, attractive and expedient manner.**
- l. For applications in the B/I zoning district, the site plan and development proposal include means to integrate and re-use on-site storm water as site amenities. **The subject property is not located within the B/I zoning district.**

**8. BIMC Chapter 2.16.110 Major Conditional Use Permit**

The applicants have also properly applied for a major Conditional Use Permit to allow a fire station within the residential R-1 zoning district. Fire Stations are considered government facilities; therefore, they are required to be reviewed under the major Conditional Use Permit process. Since this Conditional Use Permit is part of a consolidated project review with the Site Plan and Design Review application, it was also subject to a pre-application conference, public participation meeting and Design Review Board recommendation. The role of the Planning Commission is to make a recommendation to the Hearing Examiner.

**Decision Criteria**

A conditional use may be approved or approved with conditions if:

The conditional use is harmonious and compatible in design, character and appearance with the intended character and quality of development in the vicinity of the subject property and with the physical characteristics of the subject property; provided, that in the case of a housing design demonstration project any differences in design, character or appearance that are in furtherance of the purpose and decision criteria of BIMC 2.16.020.Q. shall not result in denial of a Conditional Use Permit for the project; **The proposed development is for the renovation/redevelopment of an existing fire station, which is an essential public facility. While a couple of the surrounding uses are single-family residences, the**

fire station has been in existence and operation for many years. No public comment or concerns were raised during the public comment period. The other adjacent developments are the American Legion Hall and a private school across Bucklin Hill Road.

- a. The conditional use will be served by adequate public facilities including roads, water, fire protection, sewage disposal facilities and storm drainage facilities; **As discussed in this staff report, with conditions, the proposed conditional use is adequately served by public roads, water, sewer, fire protection and proper storm drainage facilities will be constructed.**
- b. The conditional use will not be materially detrimental to uses or property in the vicinity of the subject property; **Allowing the reconstruction/expansion of the fire station will not be materially detrimental to uses in the vicinity. The use will continue with little change from the way it has operated for many years. Proper access, parking, circulation, landscaping are being provided.**
- c. The conditional use is in accord with the Comprehensive Plan and other applicable adopted community plans, including the Non-Motorized Transportation Plan; **As discussed in Section II. C of this report, the proposed conditional use is consistent with the goals and policies of the Comprehensive Plan.**
- d. The conditional use complies with all other provisions of the BIMC, unless a provision has been modified as a housing design demonstration project pursuant to BIMC 2.16.020.Q; **As detailed in this staff report, the conditional use complies, as does the building covered by the Site Plan Review, with all provisions of the BIMC.**
- e. All necessary measures have been taken to eliminate or reduce to the greatest extent possible the impacts that the proposed use may have on the immediate vicinity of the subject property;
- f. Noise levels shall be in compliance with BIMC [16.16.020](#) and 16.16.040.A; **While the project is required by law to be in compliance with BIMC 16.16.020, under emergency situations, it is likely that the noise standards will be exceeded by ambulances or fire trucks leaving the facility.**
- g. The vehicular, pedestrian, and bicycle circulation meets all applicable city standards, unless the city engineer has modified the requirements of BIMC 18.15.020.B.4 and B.5, allows alternate driveway and parking area surfaces, and confirmed that those surfaces meet city requirements for handling surface water and pollutants in accordance with Chapters [15.20](#) and [15.21](#) BIMC; **The Site Plan Review application and the Conditional Use Permit application have been reviewed by the city's development engineer and as conditioned the vehicular, pedestrian, and bicycle circulation meets all applicable City standards and pollution controls. A detailed analysis prepared by the Development Engineer was provided to staff in memo (Attachment T). Pedestrian improvements are being required along Bucklin Hill Road, by the City requiring the sidewalk and bike lane improvements (Condition 22).**
- h. The city engineer has determined that the conditional use meets the following decision criteria:
  - i. The conditional use conforms to regulations concerning drainage in Chapters [15.20](#) and [15.21](#) BIMC; and
  - ii. The conditional use will not cause an undue burden on the drainage basin or water quality and will not unreasonably interfere with the use and enjoyment of properties downstream; and

- iii. The streets and pedestrian ways as proposed align with and are otherwise coordinated with streets serving adjacent properties; and
- iv. The streets and pedestrian ways as proposed are adequate to accommodate anticipated traffic; and
- v. If the conditional use will rely on public water or sewer services, there is capacity in the water or sewer system (as applicable) to serve the conditional use, and the applicable service(s) can be made available at the site; and
- vi. The conditional use conforms to the "City of Bainbridge Island Engineering Design and Development Standards Manual," unless the city engineer has approved a variation to the road standards in that document based on his or her determination that the variation meets the purposes of BIMC Title [17](#).

**The city's Development Engineer has reviewed the Conditional Use Permit and Site Plan and Design Review application for consistency with each of the above criteria and has found the design and specifications to be consistent and in compliance with the above criteria or has recommended proper conditions to address the concerns (Attachment T).**

- i. If a major conditional use is processed as a housing design demonstration project pursuant to BIMC 2.16.020.Q, the above criteria will be considered in conjunction with the purpose, goals, policies, and decision criteria of BIMC 2.16.020.Q. **The proposal is not a housing design demonstration project.**
- j. Additional Decision Criteria for Institutions in Residential Zones. Applications to locate any of those uses categorized as educational facilities, governmental facilities, religious facilities, health care facilities, cultural facilities, or clubs in Table 18.09.020 in residential zones shall be processed as major conditional use permits and shall be required to meet the following criteria, in addition to those in subsection D of this section:
  1. All sites must front on roads classified as residential suburban, collector, or arterial on the Bainbridge Island functional road classification map. **The subject property is fronting on Bucklin Hill Road which is a collector road.**
  2. If the traffic study shows an impact on the level of service, those impacts have been mitigated as required by the city engineer. **The submitted trip generation data did not show a change to the level of service requiring mitigation. The project is increasing the PM peak hour trips by three trips and increasing trips overall by less than 33, therefore a traffic study was not required.**
  3. If the application is located outside of Winslow study area, the project shall provide vegetated perimeter buffers in compliance with BIMC [18.15.010](#). **The proposal is within the Winslow study area, but will still be providing buffers in accordance with 18.15.010.**
  4. The proposal meets the requirements of the commercial/mixed use design guidelines in BIMC [18.18.030.C](#). **The proposal is consistent with the commercial/mixed use design guidelines as far as they were applicable. The DRB reviewed the project and has provided a recommendation of approval (Attachments U, V & W).**
  5. The scale of proposed construction including bulk and height and architectural design features is compatible with the immediately surrounding area. **As a fire station, the construction has unique architectural style, however it will be**

**compatible with the immediate surrounding properties which include the American Legion Hall and a private school (Hyla).**

6. If the facility will have attendees and employees numbering fewer than 50 or an assembly seating area of less than 50, the director may waive any or all the above requirements in this subsection E, but may not waive those required elsewhere in the BIMC. **No requirements were waived.**
7. Lot coverage does not exceed 50 percent of the allowable lot coverage in the zone in which the institution is located, except that existing public schools and governmental facilities, as defined in BIMC Title 18, shall be allowed 100 percent of the lot coverage established in the underlying zoning district in which it is located unless conditions are required to limit the lot coverage to mitigate impacts of the use. **This construction is replacing an existing governmental facility therefore it is allowed 100 percent of the underlying lot coverage. The proposal is to use only 10.7% lot coverage when 15% would be allowed.**

9. **BIMC Chapter 2.16.170 Consolidated Project Review**

When an applicant has two or more land use applications, they may request that the applications be processed under a single consolidated project review process. **The proposed project requires both Site Plan and Design Review in addition to a Conditional Use Permit. The two applications are being reviewed under the consolidated project review provisions.**

F. Environmental Review:

A SEPA Mitigated Determination of Nonsignificance was issued by the SEPA Official on October 11, 2016.

III. **CONCLUSIONS**

***Staff's Conclusion***

As conditioned, the proposed project is consistent with the Comprehensive Plan, the applicable sections of the Bainbridge Island Municipal Code including: BIMC Chapter 16.16 Noise Regulations; BIMC Chapter 18.06.0020C R-1 Zone; BIMC 18.09.020 Permitted Use Table; 18.12 Dimensional Standards; BIMC 18.15.010 Landscaping and Screening; BIMC 18.15.020 Parking and loading; BIMC 18.15.030 Mobility and access; BIMC 18.15.040 Outdoor Lighting; and BIMC 18.18 Design Guidelines; BIMC 2.16.040 Site Plans and Design Review; and BIMC Chapter 2.16.170 Consolidated Project review. Appropriate notice of application was made, and a SEPA Mitigated Determination of Non-significance was issued. The application is properly before the Planning Commission in a public meeting.

IV. **Attachments:**

- A. Public Participation Meeting Summary and comment matrix
- B. Site Plan and Design Review Application, Revised March 4, 2016
- C. Conditional Use Permit Application, Revised March 4, 2016
- D. Vicinity Map/Aerial Photo of Site
- E. Project Summary/Introduction/Description of Proposal by Architect
- F. Existing Site Conditions Map
- G. Site Plan Map

- H. Floor Plan
- I. Color Elevation Renderings
- J. Elevation Drawings
- K. Building Section
- L. Illustrative Landscaping Plan
- M. Tree Retention Analysis
- N. Tree Retention Plan
- O. Plant Schedule
- P. Planting Plan (South)
- Q. Planting Plan (North)
- R. Landscape Buffer Diagram
- S. Trip Generation Letter
- T. Development Engineer Project Review Memo
- U. Design Review Board Minutes, December 21, 2015
- V. Design Review Board Minutes, February 22, 2016
- W. Design Review Board Minutes, March 7, 2016
- X. Bainbridge Island Fire Department Memo/Recommendation
- Y. Landscape Plan Supplemental Information
- Z. Arborist Analysis of trees potential impacts by development
- AA. Letter from Mackenzie regarding parking demand

Redevelop the existing fire station 21 located at 8895 Madison Avenue North as a two-story, approximately 30,760 sf building with living quarters, meeting spaces, offices, storage areas and an apparatus bay, with associated site improvements. The existing memorial is to be preserved; the flagpoles are to be relocated. The existing helicopter landing pad and fence in the southern portion of the site are to be preserved.

Redevelop the existing fire station 22 located at 7934 Bucklin Hill Road as a two-story, approximately 16,808 sf building with living quarters, meeting spaces, offices, storage areas and an apparatus bay with associated site improvements. The existing flag poles will be relocated.

ISSUE/QUESTION	RESPONSE FROM APPLICANT OR CITY
Was the facility considered to have a joint police/fire building?	It was originally considered but as of 2014 there was a vote to not combine the two.
Does this plan prohibit a combined facility?	Yes, this is only large enough for fire. The land is too small for a combined facility.
What is the acreage of Station 21?	3.91 acres.
Have you looked into geothermal?	Yes, but the size and cost make it prohibitive.
Are both buildings being replaced or remodeled?	Replaced.
How big are the existing buildings compared to the proposed?	Building 21 is approximately 1/3 <sup>rd</sup> larger than the existing.
How many parking spaces for the public is there at Station 21? Is there more or less than existing?	The same amount is proposed.
Would you consider a safe route from bus stop on 305 to the Barn property through the Fire station?	Our concern is for safety due to the helipad. We will look into a possible pedestrian connection.
Why are you proposing 9 bays?	We are designing for 50 years and anticipate extra bays for engines and EMS services.

CITY OF BAINBRIDGE ISLAND

**SITE PLAN AND DESIGN REVIEW APPLICATION**

FORM MUST BE COMPLETED IN INK, PREFERABLY BLUE.

PENCIL WILL NOT BE ACCEPTED.



<p><b>DATE STAMP FOR CITY USE ONLY</b></p>  <p style="color: blue; font-size: 1.2em;">City of Bainbridge Island</p> <p style="color: blue; font-size: 1.2em;">MAR 4 2016</p> <p style="color: blue; font-size: 1.2em;">Planning and Community Development</p>	<p style="text-align: center;"><b><u>TO BE FILLED OUT BY APPLICANT</u></b></p> <p>PROJECT NAME: BAINBRIDGE ISLAND FIRE DEPARTMENT, STATION 22</p> <p>TAX ASSESSOR'S NUMBER: <u>4178-000-012-0004</u></p> <hr/> <hr/> <p>PROJECT STREET ADDRESS OR ACCESS STREET: <b>7934 BUCKLIN HILL ROAD NE</b></p> <p>ENVIRONMENTAL CHECKLIST SUBMITTED : <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO</p> <hr/> <p style="text-align: center;"><b><u>FOR CITY USE ONLY</u></b></p> <p>FILE NUMBER:</p> <p>PROJECT NUMBER:</p> <p>DATE RECEIVED:</p> <p>APPLICATION FEE:</p> <p>TREASURER'S RECEIPT NUMBER:</p>
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<b>SUBMITTAL REQUIREMENTS</b>	
<b>APPLICATION</b>	<i>One original (which must contain an original signature) and six copies</i> must be provided. Whenever possible, originals must be <i>signed in blue</i> . Please identify the original document.
<b>SUPPORTING DOCUMENTS</b>	<i>One original (which must contain an original signature)</i> , where applicable, and <i>six copies</i> (if an original is not applicable, <i>seven copies</i> must be provided).
<b>FULL-SIZE DRAWINGS</b>	<i>Seven copies</i> of the required drawings must be provided. Drawings <i>must be folded and 18" x 24"</i> in size. <i>No construction drawings or other sized drawings</i> will be accepted unless specifically requested.
<b>REDUCED DRAWINGS</b>	<i>Five copies</i> of the drawings reduced to 11" x 17" must be provided.
<b>SUBMITTING APPLICATIONS</b>	Applications <i>must be submitted in person</i> by either the owner or the owner's designated agent. Should an agent submit the application, a <i>notarized Owner/Agent Agreement</i> must accompany the application. Please call (206) 780-3762 to make an appointment to submit your application.
<b>FEES</b>	Please call the Department of Planning & Community Development for submittal fee information.
<b>ATTACHED SUBMITTAL CHECKLIST</b>	Please refer to attached Submittal Checklist for further information. <b>NOTE:</b> when submitting this application, please do not copy or include the Submittal Checklist sheets attached to the back of this application.
<p><b>APPLICATIONS WILL NOT BE ACCEPTED</b> unless these basic requirements are met and the submittal packet is deemed counter complete.</p>	

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT  
280 MADISON AVENUE NORTH • BAINBRIDGE ISLAND, WA • 98110-1812  
PHONE: (206) 842-2800 • FAX: (206) 842-2855 • EMAIL: [pland@bainbridgewa.gov](mailto:pland@bainbridgewa.gov)

CITY OF BAINBRIDGE ISLAND

SITE PLAN AND DESIGN REVIEW APPLICATION

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A. GENERAL INFORMATION

1. Name of property owner: Bainbridge Island Fire Department
Address: 8895 Madison Avenue N, Bainbridge Island, WA 98110
Phone: (206) 842-7686 Fax:
E-mail: Lcarpenter@bifd.org

Name of property owner:
Address:
Phone: Fax:
E-mail:

Name of property owner:
Address:
Phone: Fax:
E-mail:

If the owner(s) of record as shown by the county assessor's office is (are) not the agent, the owner's (owners') signed and notarized authorization(s) must accompany this application.

2. Applicant/agent: Michael Chen, Mackenzie
Address: Logan Building, 500 Union Street, Suite 545, Seattle, WA 98101
Phone: (206) 749-9993, Extension 513 Fax: (206) 749-5565
E-mail: MChen@mcknze.com

3. Name of land surveyor: Adam & Goldsworthy, Inc.
Address: 1015 NE Hostmark Street, Suite 103, Poulsbo, WA 98370
Phone: (360) 779-4299 Fax: (360) 779-4213
E-mail: info@agols.com

4. Planning department personnel familiar with site: Nan Gladstein, Josh Machen

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CITY OF BAINBRIDGE ISLAND

**SITE PLAN AND DESIGN REVIEW APPLICATION**

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5. Description of proposal: Redevelopment of existing fire station as single-story (with mezzanine), approximately 14,150-square foot building, with living quarters, meeting spaces, offices, storage areas, and an apparatus bay, with associated site improvements. Existing flagpoles to be relocated.

6. Driving directions to site: The site is accessible from Bucklin Hill Road NE to the south.

7. Please give the following existing parcel information:

Assessor's Parcel Number	Parcel Owner	*Lot Area
4178-000-012-0004	City of Bainbridge Island	3.05 ac.
Use additional sheet if necessary	Total of all parcels:	3.05

\* As defined in Bainbridge Island Municipal Code 18.12.050

8. Legal description (or attach): STETSON'S, H A, 5 ACRE TRACTS BLK-000 LOT-012 D-00 N 658.02FT OF LOT 12 EXC W 120FT

9. Current comprehensive plan, zoning and shoreline designations and use of subject parcel(s):

Lot Number	Comp Plan Designation	Zoning Designation	Shoreline Designation	Current Use
Lot 4178-000-012-0004	OSR-1	R-1		Existing fire station
Lot				
Lot				
Lot				

10. Current comprehensive plan, zoning and shoreline designations and use of adjacent properties:

Property	Comp Plan Designation	Zoning Designation	Shoreline Designation	Current Use

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North	OSR-1	R-1		Residential properties
South	OSR-0.4	R-0.4		Hyla Middle School
East	OSR-1	R-1		Residential properties
West	OSR-1	R-1		American Legion Post

11. Common name of adjacent water area or wetlands area: N/A

12. Does the site contain an environmentally sensitive area as defined in Critical Areas Ordinance (*Bainbridge Island Municipal Code Chapter 16.20*)?  yes  no  unknown

If yes, check as appropriate:

<input type="checkbox"/> wetland*	<input type="checkbox"/> geologically hazardous area**
<input type="checkbox"/> wetland buffer*	<input type="checkbox"/> zone of influence**
<input type="checkbox"/> stream*	<input type="checkbox"/> slope buffer**
<input type="checkbox"/> stream buffer*	<input type="checkbox"/> fish and wildlife habitat area

\* If your site includes a wetland or wetland buffer, a wetland report is required with your application.

\*\* If your site includes a geologically hazardous area or is within the zone of influence as defined in *Bainbridge Island Municipal Code 16.20*, a geotechnical report may be required with your application.

13. Are there underlying/overlying agreements on the property?  yes  no  unknown  
If yes, check as appropriate and provide a copy of the decision document:

<input checked="" type="checkbox"/> CUP Conditional Use Permit	<input type="checkbox"/> SPR Site Plan Review
<input type="checkbox"/> MPD Master Planned Development	<input type="checkbox"/> SPT Short Plat
<input type="checkbox"/> PUD Planned Unit Development	<input type="checkbox"/> SSDP Shoreline Permit
<input type="checkbox"/> REZ Contract Rezone	<input type="checkbox"/> SUB Prior Subdivision
<input type="checkbox"/> RUE Reasonable Use Exception	<input type="checkbox"/> VAR Zoning Variance
	<input type="checkbox"/> Other:

Under which jurisdiction was the approval given?

City of Bainbridge Island  Kitsap County

Approval date: Unknown

14. Is there any other information which is pertinent to this project?  yes  no

If yes, please explain:

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If yes, existing sidewalks are \_\_\_\_\_ feet wide. Sidewalk installation is proposed as part of the development project:  yes  no

Proposed sidewalks:  adjacent to the parcel and are to be \_\_\_\_\_ feet wide.  internal to the proposal and are to be 5-7 feet wide.

8. Intended use of the land, as well as the sequence and timing of the proposed development: The land is proposed for redevelopment under the same use as a fire station. Pending the required approvals, construction is expected to start in the fall of 2016 and to be complete in 2018.

9. Floor area ratio: 0.18

10. Dimensions of proposed structures: Proposed GSF: 14,150 SF; proposed lot coverage: 13,422 SF / 9.9%

11. Height of proposed buildings or structures: 26'-6" (maximum roofline)

12. Square footage of all spaces: retail: N/A office: 4,361 square feet A-3: 549 square feet storage: 728 square feet residential: 1,128 square feet other: 6,850 square feet (apparatus bay and support spaces)

13. Number of stories proposed: 1 (ground floor and mezzanine)

14. Square feet per story: (1) 13,422 square feet (2) 728 square feet (3) N/A

15. Setback requirements: north: 15 feet east: 10-25 feet south: 25 feet west: 10-25 feet

16. Number of parking stalls 19

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CITY OF BAINBRIDGE ISLAND

SITE PLAN AND DESIGN REVIEW APPLICATION

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required: \_\_\_\_\_

17. Number of parking stalls proposed: 19

18. Amount of square footage of proposed paved areas: 43,330 square feet

19. Square footage of building area: 13,422 square feet

20. Percent of site to be covered by impervious surfaces: 37.9% (If the proposal results in more than 1,000 square feet of additional impervious surface, a drainage plan shall be required.)

21. Percentage of site to be covered by landscaping: 62.0%

22. Percentage of parking area to be covered by landscaping: 21.4%

23. Percentage of site to remain undeveloped: 56.2%

24. Is the applicant proposing any terms, conditions, covenants and agreements or other documents regarding the intended development: (If yes, attach copies) [ ] yes [x] no [ ] unknown

25. List any other permits for this project from state, federal or local governmental agencies for which you have applied or will apply, including the name of the issuing agency, whether the permit has been applied for, and if so, the date the application was approved or denied, and the application or permit number:

Design Review and Conditional Use Permit Approval, SEPA Determination: City of Bainbridge Island NPDES Permit: Washington State Department of Ecology

26. Will the completed project result in 800 or more square feet of impervious surface (building footprint + driveways + parking)? [x] yes [ ] no [ ] unknown

27. Will the project result in clearing more than six significant trees or 2,500 square feet of ground? [x] yes [ ] no [ ] unknown

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28. Do storm water systems exist on the site?  yes  no  unknown

If yes, were they constructed after 1982?  yes  no  unknown

If yes, what type of storm water system exists on the site?

infiltration  open ditching  closed conveyance  detention

29. Will the completed project result in excavating of or filling in:

less than 50 cubic yards.  more than 50 cubic yards but less than 100 cubic yards.  more than 100 cubic yards.

I hereby certify that I have read this application and know the same to be true and correct.

*[Handwritten Signature]*

\_\_\_\_\_  
\*Signature of owner or authorized agent

*January 14, 2016*

\_\_\_\_\_  
Date

*Michael Chen*

\_\_\_\_\_  
Please Print

*\*If signatory is not the owner of record, the attached "Owner/Agent Agreement" must be signed and notarized.*





**CITY OF BAINBRIDGE ISLAND**

Department of Planning & Community Development  
 280 Madison Avenue North, Bainbridge Island, WA 98110

Phone: 206-842-2552 Email: [pcd@bainbridgewa.gov](mailto:pcd@bainbridgewa.gov)

Website: [www.bainbridgewa.gov](http://www.bainbridgewa.gov)

Portal: <https://ci-bainbridgeisland-wa.smartgovcommunity.com/portal>

**APPLICATION - PAGE 1**

DATE STAMP

City of Bainbridge Island  
 MAR - 4 2016  
 Planning and  
 Community Development

DATE SUBMITTED 01/15/2016	PROJECT NUMBER PLN14200 CUP	
PROJECT NAME <b>BAINBRIDGE FIRE STATION 22</b>		
PROJECT TYPE <b>Conditional Use</b>		
PROJECT ADDRESS OR ACCESS STREET 7934 Buckling Hill Road		
TAX PARCEL NUMBER(S) 4178-000-012-0004		
<b>REVISIONS RECEIVED: MARCH 4, 2016</b>		
FEE HISTORY	AMOUNT	PAID
Condition Use Permit Fee	\$10,494.00	\$10,494.00

PROJECT DESCRIPTION

**PRIMARY FILE. 3/4/2016 REVISION:** Redevelopment of existing fire station as a single story (with mezzanine), approximately 14150 sf building, with living quarters, offices, storage areas and an apparatus bay, with associated site improvements. Existing flag poles will be relocated.

PEOPLE ASSOCIATED WITH CASE

COBI PROJECT MANAGER <b>JOSH MACHEN</b> -- PHONE: 206-780-3765 E-MAIL: <a href="mailto:jmachen@bainbridgewa.gov">jmachen@bainbridgewa.gov</a>
OWNER <b>BAINBRIDGE ISLAND FIRE DISTRICT</b> , Luke Carpenter, 8895 Madison Avenue North, Bainbridge Island, WA 98110 Phone: 206-842-7686 E-MAIL: <a href="mailto:lcarpenter@bifd.org">lcarpenter@bifd.org</a>
CONTACT <b>MICHAEL CHEN</b> , Mackenzie Design, 500 Union Street, Suite 545, Seattle, WA 98101 Phone: 206-749-9993 ext 513 E-MAIL: <a href="mailto:mchen@mcknze.com">mchen@mcknze.com</a>
CONTACT <b>LUKE CARPENTER</b> , Assistant Chief, Bainbridge Island Fire Department, 8895 Madison Avenue North, Bainbridge Island, WA 98110 Phone: 206-451-2033 E-MAIL: <a href="mailto:lcarpenter@bifd.org">lcarpenter@bifd.org</a>

CITY OF BAINBRIDGE ISLAND

**CONDITIONAL USE PERMIT APPLICATION**

FORM MUST BE COMPLETED IN INK, PREFERABLY BLUE.  
PENCIL WILL NOT BE ACCEPTED.



<p style="text-align: center;"><b>DATE STAMP FOR CITY USE ONLY</b></p> <p style="text-align: center; color: blue;">City of Bainbridge Island</p> <p style="text-align: center; color: blue;">MAR 4 2016 <i>Revision</i> Planning and Community Development</p>	<p style="text-align: center;"><b>TO BE FILLED OUT BY APPLICANT</b></p> <p>PROJECT NAME: BAINBRIDGE ISLAND FIRE DEPARTMENT, STATION 22</p> <p>TAX ASSESSOR'S NUMBER: <u>4178-000-012-0004</u></p> <hr/> <p>PROJECT STREET ADDRESS OR ACCESS STREET: <u>7934 BUCKLIN HILL ROAD NE</u></p> <p>ENVIRONMENTAL CHECKLIST SUBMITTED: <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO</p> <hr/> <p style="text-align: center;"><b>FOR CITY USE ONLY</b></p> <p>FILE NUMBER: <u>PLN14200 CUP</u></p> <p>PROJECT NUMBER: <u>14200</u></p> <p>DATE RECEIVED: <u>Revision of 3/4/2016</u></p> <p>APPLICATION FEE: <u>\$10,494.00 rec'd 1/15/2016</u></p> <p>TREASURER'S RECEIPT NUMBER: <u>16-00053 of 1/15/2016</u></p>
<b>SUBMITTAL REQUIREMENTS</b>	
<b>APPLICATION</b>	<i>One original (which must contain an original signature) and six copies</i> must be provided. Whenever possible, originals must be <i>signed in blue</i> . Please identify the original document.
<b>SUPPORTING DOCUMENTS</b>	<i>One original (which must contain an original signature)</i> , where applicable, and <i>six copies</i> (if an original is not applicable, <i>seven copies</i> must be provided).
<b>FULL-SIZE DRAWINGS</b>	<i>Seven copies</i> of the required drawings must be provided. Drawings <i>must be folded and 18" x 24"</i> in size. <i>No construction drawings or other sized drawings</i> will be accepted unless specifically requested.
<b>REDUCED DRAWINGS</b>	<i>Two copies (five if commercial)</i> of the drawings reduced to 11" x 17" must be provided.
<b>SUBMITTING APPLICATIONS</b>	Applications <i>must be submitted in person</i> by either the owner or the owner's designated agent. Should an agent submit the application, a <i>notarized Owner/Agent Agreement</i> must accompany the application. Please call (206) 780-3762 to make an appointment to submit your application.
<b>FEES</b>	Please call the Department of Planning & Community Development for submittal fee information.
<b>ATTACHED SUBMITTAL CHECKLIST</b>	Please refer to attached Submittal Checklist for further information. <b>NOTE:</b> when submitting this application, please do not copy or include the Submittal Checklist sheets attached to the back of this application.
<b>APPLICATIONS WILL NOT BE ACCEPTED</b> unless these basic requirements are met and the submittal packet is deemed counter complete.	

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT  
280 MADISON AVENUE NORTH • BAINBRIDGE ISLAND, WA • 98110-1812  
PHONE: (206) 842-2552 • FAX: (206) 780-0955 • EMAIL: pcd@bainbridgewa.gov  
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CITY OF BAINBRIDGE ISLAND

CONDITIONAL USE PERMIT APPLICATION

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A. GENERAL INFORMATION

Please indicate which of the following is the reason for which you are applying:  
\_\_\_\_\_ Height       Use

1. Name of property owner: Bainbridge Island Fire Department  
Address: 8895 Madison Avenue N, Bainbridge Island, WA 98110  
Phone: (206) 842-7686 Fax: \_\_\_\_\_  
E-mail: Lcarpenter@bifd.org

Name of property owner: \_\_\_\_\_  
Address: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
E-mail: \_\_\_\_\_

Name of property owner: \_\_\_\_\_  
Address: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
E-mail: \_\_\_\_\_

*If the owner(s) of record as shown by the county assessor's office is (are) not the agent, the owner's (owners') signed and notarized authorization(s) must accompany this application.*

2. Authorized agent: Michael Chen, Mackenzie  
Address: Logan Building, 500 Union Street, Suite 545, Seattle, WA 98101  
Phone: (206) 749-9993, Extension 513 Fax: (206) 749-5565  
E-mail: MChen@mcknze.com

3. Person responsible for payment: Luke Carpenter, Assistant Chief, Bainbridge Island Fire Department  
Address: 8895 Madison Avenue N, Bainbridge Island, WA 98110  
Phone: (206) 451-2033 Fax: \_\_\_\_\_  
E-mail: Lcarpenter@bifd.org

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CITY OF BAINBRIDGE ISLAND

**CONDITIONAL USE PERMIT APPLICATION**

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4. Project contact: Michael Chen, Mackenzie  
 Address: Logan Building, 500 Union Street, Suite 545, Seattle, WA 98101  
 Phone: (206) 749-9993, Extension 513 Fax: (206) 749-5565  
 E-mail: MChen@mcknze.com

5. Name of land surveyor: Adam & Goldsworthy, Inc.  
 Address: 1015 NE Hostmark Street, Poulsbo, WA 98370  
 Phone: (360) 779-4299 Fax: (360) 779-4213  
 E-mail: info@agols.com

6. Planning department personnel familiar with site: Nan Gladstein, Josh Machen

7. Description of proposal: Redevelopment of existing fire station as single-story  
(with mezzanine), approximately 14,150-square foot building, with living quarters,  
offices, storage areas, and an apparatus bay, with associated site  
improvements. Existing flagpoles to be relocated.

8. Driving directions to site: The site is accessible from Bucklin Hill Road NE to the  
south.

9. Please give the following existing parcel information:

Assessor's Parcel Number	Parcel Owner	*Lot Area
4178-000-012-0004	City of Bainbridge Island	3.05 ac.
Use additional sheet if necessary	Total of all parcels:	3.05

\* As defined in Bainbridge Island Municipal Code 18.12.050

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CITY OF BAINBRIDGE ISLAND

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10. Legal description (or attach): STETSON'S, H A, 5 ACRE TRACTS BLK-000  
LOT-012 D-00 N 658.02FT OF LOT 12 EXC W 120FT

11. Current comprehensive plan, zoning and shoreline designations and use of subject parcel(s):

Lot Number	Comp Plan Designation	Zoning Designation	Shoreline Designation	Current Use
Lot 4178-000-012-0004	OSR-1	R-1		Existing fire station
Lot				
Lot				
Lot				

12. Current comprehensive plan, zoning and shoreline designations and use of adjacent properties:

Property	Comp Plan Designation	Zoning Designation	Shoreline Designation	Current Use
North	OSR-1	R-1		Residential properties
South	OSR-0.4	R-0.4		Hyla Middle School
East	OSR-1	R-1		Residential properties
West	OSR-1	R-1		Undeveloped <del>Legion Hall</del>

13. Common name of adjacent water area or wetlands area: N/A

14. Does the site contain an environmentally sensitive area as defined in Critical Areas Ordinance (*Bainbridge Island Municipal Code Chapter 16.20*)?  yes  no  unknown

If yes, check as appropriate:

<input type="checkbox"/> wetland*	<input type="checkbox"/> geologically hazardous area**
<input type="checkbox"/> wetland buffer*	<input type="checkbox"/> zone of influence**
<input type="checkbox"/> stream*	<input type="checkbox"/> slope buffer**
<input type="checkbox"/> stream buffer*	<input type="checkbox"/> fish and wildlife habitat area

\* If your site includes a wetland or wetland buffer, a wetland report is required with your application.

\*\* If your site includes a geologically hazardous area or is within the zone of influence as defined in *Bainbridge Island Municipal Code 16.20*, a geotechnical report is required with your application.

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15. Are there underlying/overlying agreements on the property?  yes  no  unknown  
If yes, check as appropriate and provide a copy of the decision document: Please see list of land use decisions included with Conditional Use Permit Narrative.

<input checked="" type="checkbox"/> CUP Conditional Use Permit	<input type="checkbox"/> SPR Site Plan Review
<input type="checkbox"/> MPD Master Planned Development	<input type="checkbox"/> SPT Short Plat
<input type="checkbox"/> PUD Planned Unit Development	<input type="checkbox"/> SSDP Shoreline Permit
<input type="checkbox"/> REZ Contract Rezone	<input type="checkbox"/> SUB Prior Subdivision
<input type="checkbox"/> RUE Reasonable Use Exception	<input type="checkbox"/> VAR Zoning Variance
	<input type="checkbox"/> Other:

Under which jurisdiction was the approval given?

City of Bainbridge Island  Kitsap County

Approval date: Unknown

16. Is there any other information which is pertinent to this project?  yes  no

If yes, please explain:

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DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT  
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CITY OF BAINBRIDGE ISLAND

**CONDITIONAL USE PERMIT APPLICATION**

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**B. TECHNICAL INFORMATION**

1. Name of water purveyor: City of Bainbridge Island  
If a private well, what class? N/A

2. Type of sewage disposal:  on-site septic  off-site septic  sewer  
Sewer district:  City of Bainbridge Island  Sewer District 7

3. General description of the existing terrain: The site is relatively flat, fully developed, with no known geological concerns.

4. Soil survey classification: Geotechnical information on the site is available from Aspect Consulting; soil types found on the site include sand and silt.

5. Flood plain designation:  X  AE

6. Access (street functional road classifications):

Street Type	Required ROW Width	Street Name	Existing ROW Width
primary arterial	150 feet		
secondary arterial	60 feet	Bucklin Hill Road NE	60 feet
collector	50 feet		
residential urban	40 feet		
residential suburban	30 feet		
private	20 - 30 feet		

7. Sidewalks are adjacent to the parcel:  yes  no  
If yes, existing sidewalks are \_\_\_\_\_ feet wide.  
Sidewalk installation is proposed as part of the development project:  yes  no  
Proposed sidewalks:  adjacent to the parcel and are to be \_\_\_\_\_ feet wide.

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internal to the proposal and are to be 5-7 feet wide.

8. Intended use of the land, as well as the sequence and timing of the proposed development:
The land is proposed for redevelopment under the same use as a fire station.
Pending the required approvals, construction is expected to start in the fall of 2016 and to be complete in 2018.

9. Dimensions of proposed structures: Proposed GSF: 14,150 SF; proposed lot coverage: 13,422 SF / 9.9%

10. Height of proposed buildings or structures: 26'-6" (maximum roofline)

11. Square footage of all spaces: retail: N/A, office: 4,361 square feet, A-3: 549 square feet, storage: 728 square feet, residential: 1,128 square feet, other: 6,850 square feet (apparatus bay and support spaces)

12. Number of stories proposed: 1 (ground floor and mezzanine)

13. Square feet per story: (1) 13,422 SF (2) 728 SF (3) N/A

14. Setback requirements: north: 15 feet, east: 10-25 feet, south: 25 feet, west: 10-25 feet

15. Number of parking stalls required: 19

16. Number of parking stalls proposed: 19

17. Amount of square footage of proposed paved areas: 43,330 square feet

18. Square footage of building area: 13,422 square feet

19. Percent of site to be covered by impervious surfaces: 37.9%

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CITY OF BAINBRIDGE ISLAND

CONDITIONAL USE PERMIT APPLICATION

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(If the proposal results in more than 1,000 square feet of additional impervious surface, a drainage plan shall be required.)

20. Percentage of site to be covered by landscaping: 62%

21. Percentage of parking area to be covered by landscaping: 21.4%

22. Percentage of site to remain undeveloped: 56.2%

23. Is the applicant proposing any terms, conditions, covenants and agreements or other documents regarding the intended development: (If yes, attach copies)  
 yes  no  unknown

24. Is the proposal part of a phased development plan? (If so, an outline of the future plans must be submitted.)  
N/A

25. List any other permits for this project from state, federal or local governmental agencies for which you have applied or will apply, including the name of the issuing agency, whether the permit has been applied for, and if so, the date the application was approved or denied, and the application or permit number:  
Site Plan and Design Review Approval, SEPA Determination, Building Permit:  
City of Bainbridge Island  
NPDES Permit: Washington State Department of Ecology

26. Will the completed project result in 800 or more square feet of impervious surface (building footprint + driveways + parking)?  yes  no  unknown

27. Will the project result in clearing more than six significant trees or 2,500 square feet of ground?  yes  no  unknown

28. Do storm water systems exist on the site?  yes  no  unknown  
If yes, were they constructed after 1982?  yes  no  unknown

If yes, what type of storm water system exists on the site?

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CITY OF BAINBRIDGE ISLAND

CONDITIONAL USE PERMIT APPLICATION

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- infiltration
- open ditching
- closed conveyance
- detention

29. Will the completed project result in excavating of or filling in:

- less than 50 cubic yards.
- more than 50 cubic yards but less than 100 cubic yards.
- more than 100 cubic yards.

**C. Conditional Use Permit Criteria**

In accordance with Chapter 2.16.050 and 2.16.110 of the City of Bainbridge Island Municipal Code, the applicant must answer the following questions:

1. In what manner is the requested conditional use harmonious and compatible in design, character and appearance with the existing or intended character and quality of development in the vicinity of the subject property and with the physical characteristics of the subject property?

Please refer to the Conditional Use Permit Narrative and Exhibits included  
included with this application.

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2. How has the proposed conditional use made adequate provision for the following facilities:

- a) roads; Please refer to the Conditional Use Permit
  - b) water availability; Narrative and Exhibits included
  - c) fire protection; included with this application.
  - d) sewage disposal facilities, and
  - e) storm drainage facilities.
- 
- 

3. Describe how the proposed conditional use will not be materially detrimental to uses or property in the vicinity of the subject property:

Please refer to the Conditional Use Permit Narrative and Exhibits included  
included with this application.

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DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT  
 280 MADISON AVENUE NORTH • BAINBRIDGE ISLAND, WA • 98110-1812  
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4. Relate how the proposed conditional use will meet all the criteria otherwise applicable to the zone in which it is to be developed:

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

5. How is the conditional use in conformance with the Comprehensive Plan?

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

6. How does the conditional use comply with all of the other provisions of the city code?

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

7. Describe how the conditional use will not adversely affect the area or alter the area's predominantly residential nature?

Please refer to the Conditional Use Permit Narrative and Exhibits included included with this application.

8. How is the subject property suitable for the proposed conditional use and will it be

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CONDITIONAL USE PERMIT APPLICATION

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detrimental to surrounding land uses or sensitive areas? Consideration shall include items such as topography, streets and adjacent land uses:

Please refer to the Conditional Use Permit Narrative and Exhibits included  
included with this application.

9. Describe how all necessary measures have been taken to eliminate the impacts that issuance of the conditional use permit may have on the area in which it is to be located:

Please refer to the Conditional Use Permit Narrative and Exhibits included  
included with this application.

I hereby certify that I have read this application and know the same to be true and correct.

*Luke Carpenter*  
\*Signature of owner or authorized agent

1/13/16  
Date

Luke Carpenter  
Please Print

*\*If signatory is not the owner of record, the attached "Owner/Agent Agreement" must be signed and notarized.*

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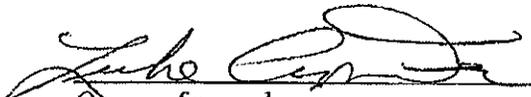
# Owner/Agent Agreement

The undersigned is (are) the owner(s) of record of the property identified by the Kitsap County Assessor's account number 4178-000-012-0004, located at 7934 Bucklin Hill Road NE, Bainbridge Island, Washington. The undersigned hereby gives (give) consent and approval to Michael Chen, Mackenzie

to act on his/her (their) behalf as his/her (their) agent to proceed with an application for (please check all items that apply):

- preapplication conference
- planning permits
- construction permits (i.e. building, water/sewer availability, right-of-way, etc)

on the property referenced herein. This agreement authorizes the agent to act on the owner's behalf for the above checked applications through (date or specific phase) construction permits.


1/13/16  
 Owner of record Date
\_\_\_\_\_
Date

STATE OF WASHINGTON )  
 ) ss.  
 COUNTY OF KITSAP )

On this 13<sup>th</sup> day of January, 2016, before me, the undersigned, a Notary Public in and for the State of Washington, duly commissioned and sworn, personally appeared:

Luke Carpenter

to me known as the individual(s) described in and who executed the foregoing instrument, and acknowledged to me that he/she/they signed and sealed the said instrument, as his/her/their free and voluntary act and deed for the uses and purposes therein mentioned, and on oath stated that he/she/they was (were) authorized to execute said instrument.

WITNESS MY HAND AND OFFICIAL SEAL, hereto affixed the day and year in this certificate above written.

HILARY D. HALL  
 Notary Public  
 State of Washington  
 My Commission Expires  
 March 01, 2016



Notary Public in and for the State of Washington

Residing at 275 NW Scandia Rd, Poulsbo WA

My appointment expires: 3/1/2016

Aerial Image – Project Site



ATTACHMENT D



## I. PROJECT SUMMARY

**Applicant:** Bainbridge Island Fire Department  
8895 Madison Avenue N  
Bainbridge Island, WA 98110

**Owner:** Bainbridge Island Fire Department  
8895 Madison Avenue N  
Bainbridge Island, WA 98110

**Site Address:** 7934 Bucklin Hill Road NE  
Bainbridge Island, WA 98110

**Assessor Site Acreage:** 3.05 acres

**Zoning:** 1 Unit per Acre Zone (40,000 square feet; R-1)  
Critical Overlay District

**Comprehensive Plan:** 1 Unit per Acre Zone (40,000 square feet; OSR-1)

**Adjacent Zoning:** R-1 to the north, east, and west, and 1 Unit per Acre Zone (100,000 square feet; R-0.4) to the south

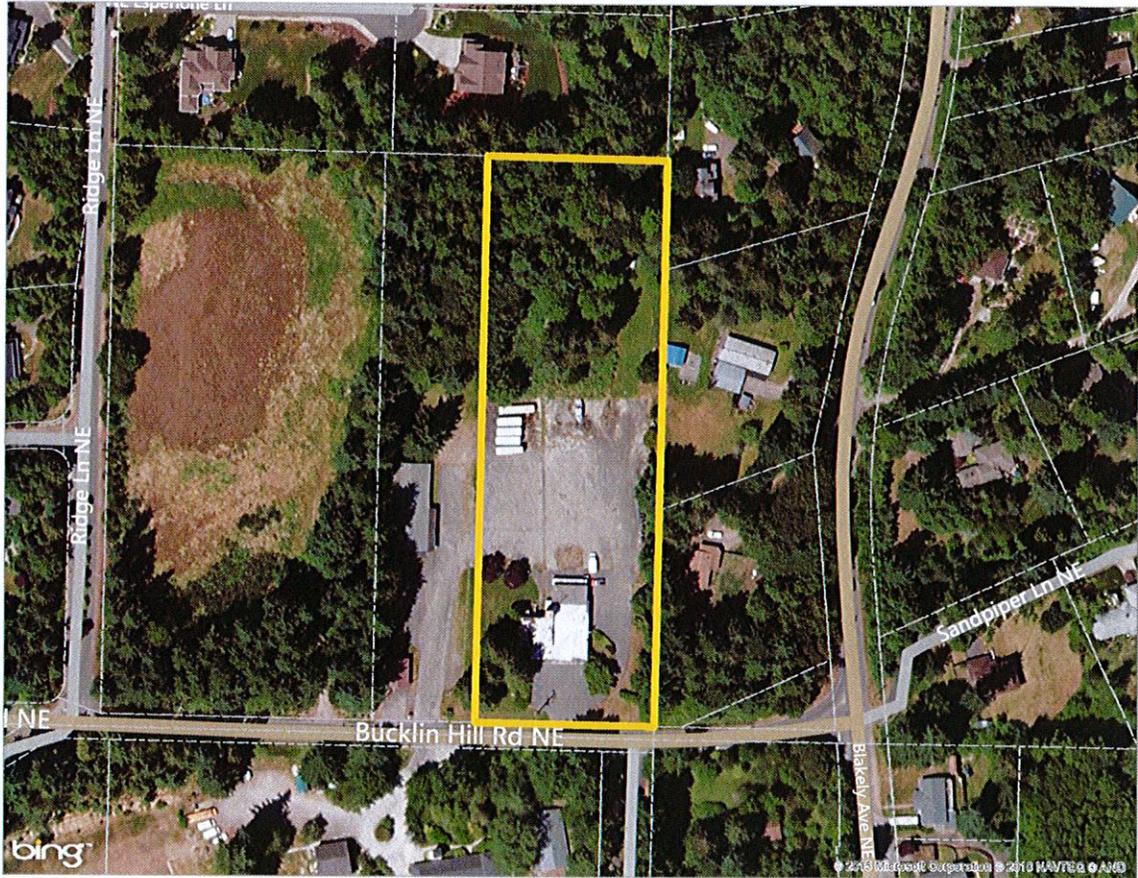
**Existing Structures:** Fire station, shop addition, shipping container, concrete and gravel pads, and other site improvements

**Request:** Conditional Use Permit

**Project Contact:** Mackenzie, Applicant's Representative  
Michael Chen, Senior Planner  
Logan Building  
500 Union Street, Suite 545  
Seattle, WA 98101  
(206) 749-9993, Extension 513  
MChen@mcknze.com

## ATTACHMENT E

Aerial Image – Project Site



### III. NARRATIVE & COMPLIANCE

1. *In what manner is the requested conditional use harmonious and compatible in design, character and appearance with the existing or intended character and quality of development in the vicinity of the subject property and with the physical characteristics of the subject property?*

**Response:** The proposed building is designed for compatibility with contemporary architecture characteristic of the Pacific Northwest in general and the Bainbridge Island community specifically. In a precedent study early in the development process, the applicant considered aesthetic features of the Bainbridge Island City Hall building, some elements of which may be incorporated into the final design of the fire station. Community members will be involved in the redevelopment of the fire station through participation in the community meeting to be held in the initial stages of the review process. As part of the community meeting, the applicant will present their ideas for the proposed fire station to both participants and the City of Bainbridge Island Design Review Board, all of whom will have the opportunity to comment on the proposal. The applicant supports this initiative to develop a meaningful process for citizen participation and looks forward to engaging the community's vision for Station 22.

2. *How has the proposed conditional use made adequate provision for the following facilities: a) roads, b) water availability, c) fire protection, d) sewage disposal facilities, and e) storm drainage facilities?*

**Response:**

- a) **Roads:** The site is currently served by Bucklin Hill Road NE to the south, which is functionally classified as a secondary arterial street according to the draft update of the City's Island Wide Transportation Plan (IWTP). The existing size of Bucklin Hill Road NE meets the required right-of-way width of 60 feet. The existing driveway will be relocated slightly to the west as an approximately 24-foot-wide driveway for both passenger vehicles and emergency access, and a seven-foot sidewalk will be constructed to connect the driveway to the parking area to the north and to the main building entrance to the north of the parking area.
- b) **Water availability:** The site is currently served by public water utility infrastructure provided by the City of Bainbridge Island that is adequate to meet the needs of the proposed development; therefore, no new water utilities or utility connections are proposed.
- c) **Fire protection:** The proposed building is designed for compliance with the Washington State Building Code, which includes fire protection measures. The building will be fully sprinklered, and some walls will be fire-rated.
- d) **Sewage disposal facilities:** The site is currently served by a private on-site septic system which is proposed to be redesigned in order to accommodate the larger redeveloped fire station.
- e) **Storm drainage facilities:** Stormwater facilities are proposed in the landscape area, including stormwater planters in the landscaping on the northern side and in the southeast corner of the site. As required according to jurisdictional erosion control standards, Best Management Practices (BMPs) and the relevant standard details will be utilized throughout the site and in construction activity.
3. *Describe how the proposed conditional use will not be materially detrimental to uses or property in the vicinity of the subject property.*

**Response:** The subject property is surrounded by residential properties to the north, Hyla Middle School to the south, residential properties to the east, and undeveloped property to the west. The proposed fire station use will be limited to 11.2 percent building coverage and 43.8 percent impervious area coverage on the site. Any noise related to the fire station use that can be defined as "[s]ounds created by emergency equipment and work necessary [...] for healthy safety or welfare of the community" are

exempt from all provisions of the Washington Administrative Code adopted by the City of Bainbridge Island governing maximum environmental noise levels (see Bainbridge Island Municipal Code Section 16.16.020. and WAC Section 173-60-050 [4] [f]).

4. *Relate how the proposed conditional use will meet all the criteria otherwise applicable to the zone in which it is to be developed.*

**Response:** The proposed use is designed in compliance with the dimensional standards for the base zone, R-1, as presented in the abridged version of Bainbridge Island Municipal Code (BIMC) Table 18.12.020-2 below.

TABLE 18.12.020-2: STANDARD LOT DIMENSIONAL STANDARDS FOR RESIDENTIAL ZONE DISTRICTS (ABRIDGED)		
Standard	Requirement	Proposal
Maximum Lot Coverage	20%	9.9%
<b>Minimum Setbacks</b>		
Front/Street	25 feet	~114 feet
Side	5–15 feet	~35' (E), ~65' (W)
Rear	15 feet	~284 feet
Maximum Building Height	35 feet	26.5 feet

5. *How is the conditional use in conformance with the Comprehensive Plan?*

**Response:** As articulated in the Comprehensive Plan, one of the City’s primary goals is to “[p]rovide adequate public facilities [...] which maximize public safety and minimize adverse environmental impacts.” In the Strategic Plan Update, BIFD identified critical needs at Station 22 and recommended a formal needs assessment, which the applicant’s representative, Mackenzie, was involved in producing. The proposed redevelopment of Station 22 will ensure not only that the buildings meet BIFD’s operational needs, but also that the redeveloped fire station will serve the needs of the Bainbridge Island community for decades to come.

6. *How does the conditional use comply with all of the other provisions of the city code?*

**Response:** The proposed use is designed in compliance with the development standards and guidelines as presented in the responses to excerpts from the BIMC below.

- Landscaping, screening, and tree retention, protection, and replacement: Entry gardens are proposed to the west of the main entry on the south side of the building, and a staff patio is proposed on the west side of the building. The existing forest buffer on the northern side of the site will be preserved, and a meadow will be maintained to the south of the forest buffer, north of the internal circulation area on the north side of the proposed building. A full screen buffer will be provided on the eastern side of the site, with partial screen buffers provided on the southern and western sides of the site. Interior parking areas will also be landscaped.
- Parking and loading: 19 parking spaces have been determined necessary and will be provided for the proposed development. No loading will be necessary for the development; therefore, no loading area is proposed.
- Mobility and access: Five- to seven-foot-wide internal sidewalks will be provided for the proposed development, and the development will be accessible via the existing driveway, to be redesigned as described above. One accessible parking space will be provided in the southern parking area. Bicycle parking will be provided in close proximity to the building entrance.

- Outdoor lighting: A photometric lighting plan is included with this application. Lighting design minimizes spillover to neighboring properties.
  - Signs: Exterior building signage is proposed.
7. *Describe how the conditional use will not adversely affect the area or alter the area's predominantly residential nature?*

**Response:** The fire station use is surrounded by residential properties to the north, Hyla Middle School to the south, residential properties to the east, and undeveloped property to the west. The station is strategically located so as to be able to respond to emergencies in both residential and non-residential areas of the City as rapidly as possible. The residential uses on two sides of the property and the school use on one side of the property have existed adjacent to the property in its current use as a fire station without adverse effects to the area or alterations to the area's predominately residential nature. The proposed building will be located in the center of the property, and landscape buffers will be provided on the perimeter of the property in order to reduce any potential impacts on adjacent uses.

8. *How is the subject property suitable for the proposed conditional use and will it be detrimental to surrounding land uses or sensitive areas? Consideration shall include items such as topography, streets and adjacent land uses.*

**Response:** The subject property is relatively flat, fully developed, with no known geological concerns. There are no known critical areas on the site. The site is served by Bucklin Hill Road NE to the east; the existing size of Bucklin Hill Road NE meets the required right-of-way width of 60 feet. The existing access to the site from Bucklin Hill Road NE will be redeveloped but preserved. The proposed use of the subject property as a fire station will be fully contained on the site and will not conflict with any of the uses in the vicinity.

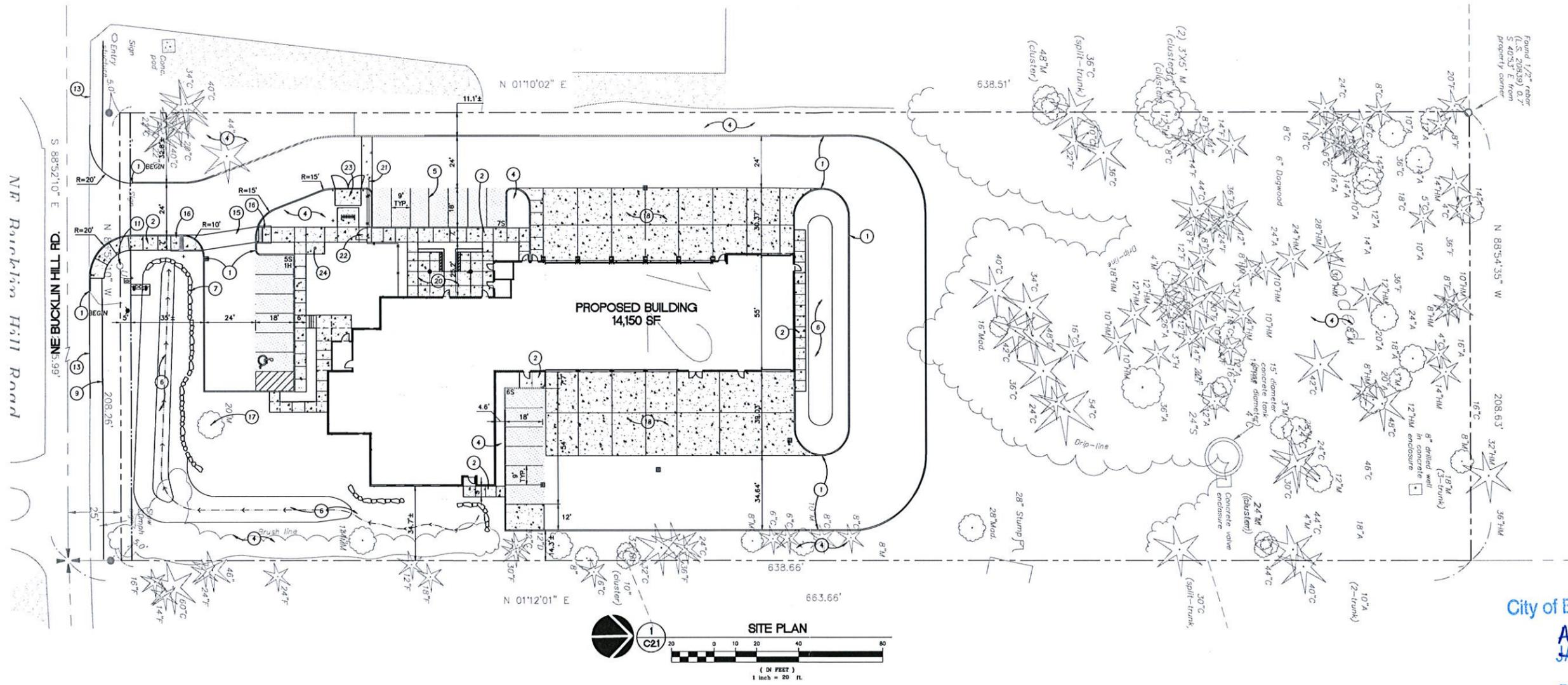
9. *Describe how all necessary measures have been taken to eliminate the impacts that issuance of the conditional use permit may have on the area in which it is to be located.*

**Response:** Community members will be involved in the redevelopment of the fire station through participation in the community meeting to be held in the initial stages of the review process. As part of the community meeting, the applicant will present their ideas for the proposed fire station to both participants and the City of Bainbridge Island Design Review Board, all of whom will have the opportunity to comment on the proposal. The applicant supports this initiative to develop a meaningful process for citizen participation and looks forward to engaging the community's vision for Station 22.

**IV. CONCLUSION**

The applicant submits this narrative and the attached Exhibits in seeking approval of a Conditional Use Permit for the proposed redevelopment of the existing fire station located at 7934 Bucklin Hill Road NE. The preliminary land use application to follow will continue to demonstrate that the proposal is in compliance with the relevant standards provided by the City of Bainbridge Island.





City of Bainbridge Island  
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**GENERAL NOTES**

- ALL WORK SHALL CONFORM TO THE STANDARD SPECIFICATIONS AND THE REQUIREMENTS OF THE CITY OF BAINBRIDGE ISLAND AND THE CURRENT AMERICAN PUBLIC WORKS ASSOCIATION STANDARDS FOR PUBLIC WORKS CONSTRUCTION.
- THE WORKING DRAWINGS ARE GENERALLY DIAGRAMMATIC. THEY DO NOT SHOW EVERY OFFSET, BEND OR ELBOW REQUIRED FOR INSTALLATION IN THE SPACE PROVIDED. THEY DO NOT SHOW EVERY DIMENSION, COMPONENT PIECE, SECTION, JOINT OR FITTING REQUIRED TO COMPLETE THE PROJECT. ALL LOCATIONS FOR WORK SHALL BE CHECKED AND COORDINATED WITH EXISTING CONDITIONS IN THE FIELD BEFORE BEGINNING CONSTRUCTION. EXISTING UNDERGROUND UTILITIES LAYING WITHIN THE LIMITS OF EXCAVATION SHALL BE VERIFIED AS TO CONDITION, SIZE AND LOCATION BY UNCOVERING, PROVIDING SUCH IS PERMITTED BY LOCAL PUBLIC AUTHORITIES WITH JURISDICTION, BEFORE BEGINNING CONSTRUCTION. CONTRACTOR TO NOTIFY ENGINEER IF THERE ARE ANY DISCREPANCIES.
- EFFECTIVE EROSION PREVENTION AND SEDIMENT CONTROL IS REQUIRED. EROSION CONTROL DEVICES MUST BE INSTALLED AND MAINTAINED TO MEET CITY OF BAINBRIDGE ISLAND REQUIREMENTS. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE EROSION CONTROL.
- EFFECTIVE DRAINAGE CONTROL IS REQUIRED. DRAINAGE SHALL BE CONTROLLED WITHIN THE WORK SITE AND SHALL BE ROUTED SO THAT ADJACENT PRIVATE PROPERTY, PUBLIC PROPERTY, AND THE RECEIVING SYSTEM ARE NOT ADVERSELY IMPACTED. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE DRAINAGE CONTROL.
- CONTRACTOR SHALL ADJUST ALL STRUCTURES IMPACTED BY CONSTRUCTION IMPROVEMENTS TO NEW FINISH GRADES.
- EXCAVATION: EXCAVATE FOR SLABS, PAVING, AND OTHER IMPROVEMENTS TO SIZES AND LEVELS SHOWN OR REQUIRED. ALLOW FOR FORM CLEARANCE AND FOR PROPER COMPACTION OF REQUIRED BACKFILLING MATERIAL. EXCAVATOR(S) SHALL NOTIFY ALL UTILITY COMPANIES FOR LINE LOCATIONS SEVENTY-TWO (72) HOURS (MINIMUM) PRIOR TO START OF WORK. DAMAGE TO UTILITIES SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE. (ONE CALL LOCATE UTILITY NOTIFICATION CENTER - 1-800-332-2344).
- WHERE CONNECTING TO AN EXISTING PIPE, AND PRIOR TO ORDERING MATERIALS, THE CONTRACTOR SHALL EXPOSE THE END OF THE EXISTING PIPE VERIFY THE LOCATION, SIZE, AND ELEVATION. NOTIFY ENGINEER OF ANY DISCREPANCIES.
- REQUEST BY THE CONTRACTOR FOR CHANGES TO THE PLANS MUST BE APPROVED BY THE ENGINEER.

**CURB NOTE**

ALL CURB RADII TO BE 2.5' UNLESS OTHERWISE NOTED

**KEYNOTES**

- CONCRETE VERTICAL CURB PER DETAIL 1/CB.0
- CONCRETE SIDEWALK PER DETAIL 2/CB.0
- ADA ACCESSIBLE PARKING STALL PER DETAIL 3/CB.0
- LANDSCAPE AREA, SEE LANDSCAPE PLANS
- 4" WIDE WHITE PARKING STRIPE
- STORMWATER FACILITY, SEE GRADING PLAN
- ROCK WALL, SEE GRADING PLAN
- TEMPORARY FACILITIES
- FUTURE 6' PAVED SHOULDER (CONSTRUCTION BY CITY OF BAINBRIDGE ISLAND)
- CHAIN LINK FENCE
- EXISTING POWER POLE TO REMAIN, PROTECT DURING CONSTRUCTION
- CONCRETE RISER, SEE GRADING PLAN
- EDGE OF ASPHALT
- TRANSFORMER
- 12" WHITE CROSSWALK STRIPE
- SIDEWALK RAMP PER DETAIL 8/CB.0 AND 9/CB.0
- EXISTING TREE TO REMAIN, PROTECT DURING CONSTRUCTION
- CONCRETE APRON, SEE PAVING LEGEND
- GENERATOR PAD
- PATIO AREA, SEE GRADING PLAN FOR DETAILS
- MOTORIZED ROLLING GATE
- PERSONNEL GATE
- TRASH ENCLOSURE
- FLAG POLE

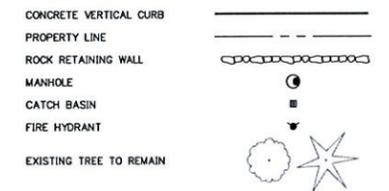
**SITE DATA**

SITE AREA	133,109 SF (3.06 AC)
BUILDING FOOTPRINT	13,422 SF
BUILDING COVERAGE	10.1%
LANDSCAPE AREA	81,993 SF (61.6%)

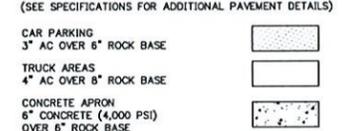
**PARKING DATA**

PARKING PROVIDED	18 SPACES
STANDARD	1 SPACE
ACCESSIBLE	0 SPACES
COMPACT	0 SPACES
TOTAL	19 SPACES (1.3/1,000)

**LEGEND**

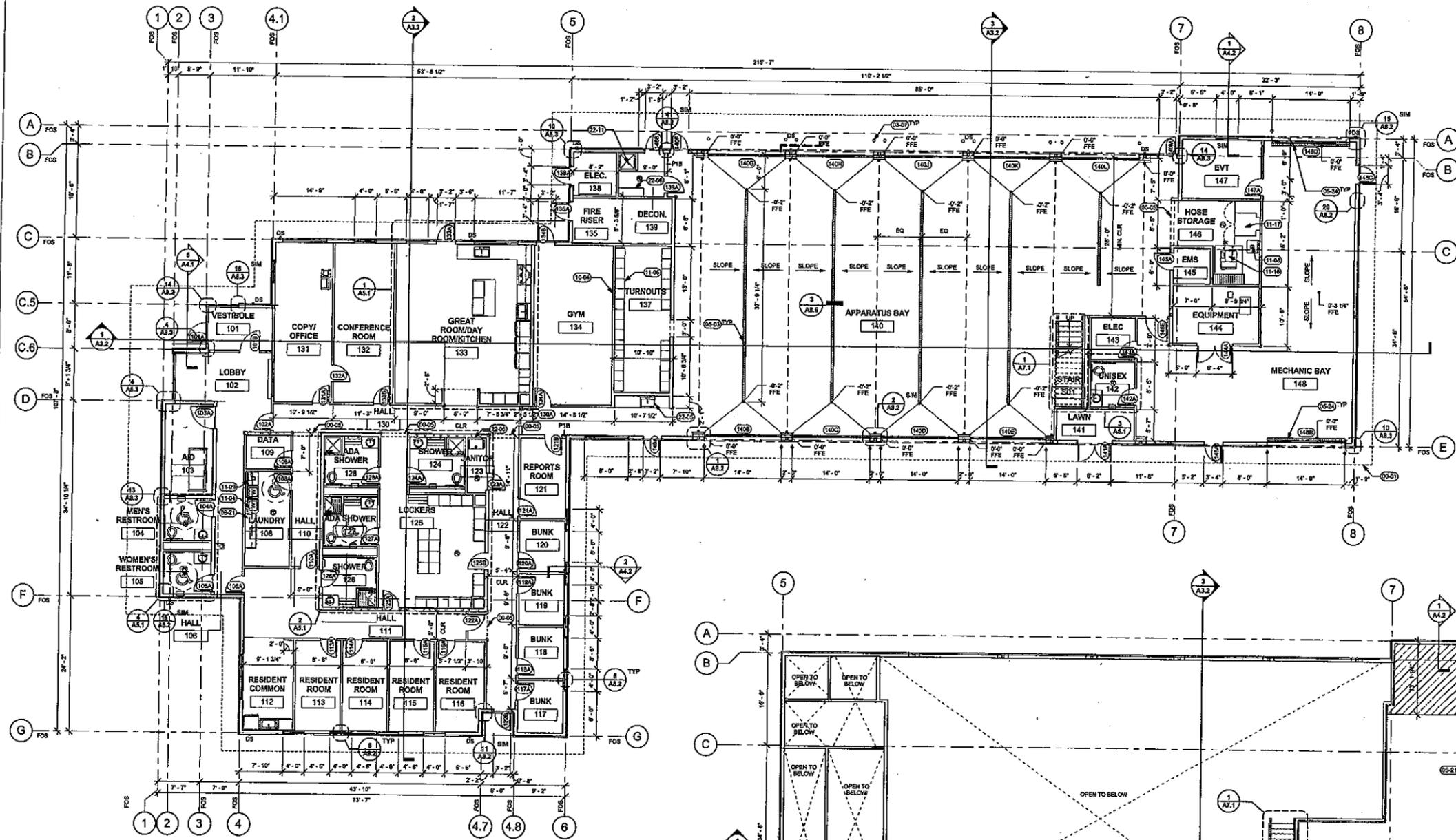


**PAVEMENT LEGEND**



ATTACHMENT G

<p>MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204</p>	<p>LANDSCAPE FISHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110</p>	<p><b>MACKENZIE</b> DESIGN DRIVEN   CLIENT FOCUSED</p>	<p>Architecture • Interiors • Planning • Engineering</p> <p>Portland, OR 503.224.9560    Vancouver, WA 360.695.7879    Seattle, WA 206.749.9993</p> <p>Client <b>BAINBRIDGE ISLAND FIRE DEPARTMENT</b> 8895 MADISON AVE. NE BAINBRIDGE, WA 98110</p>	<p>Project <b>BAINBRIDGE ISLAND FIRE DEPT STATION 22</b> 7934 BUCKLIN HILL RD. NE</p>	<p>REVISIONS:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th>NO.</th> <th>REVISIONS</th> <th>DATE</th> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </table> <p>DRAWN BY: BTS CHECKED BY: TWJ</p>	NO.	REVISIONS	DATE				<p>© MACKENZIE 2016. ALL RIGHTS RESERVED. THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER WITHOUT PRIOR WRITTEN PERMISSION.</p> <p>SHEET TITLE: <b>SITE PLAN</b></p> <p>SHEET <b>C2.1</b></p> <p>JOB NO. 2150124.00</p>
NO.	REVISIONS	DATE										



City of Bainbridge Island  
 OCT 18 2016  
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1 FIRST FLOOR PLAN  
 1/8" = 1'-0"

FLOOR PLAN LEGEND

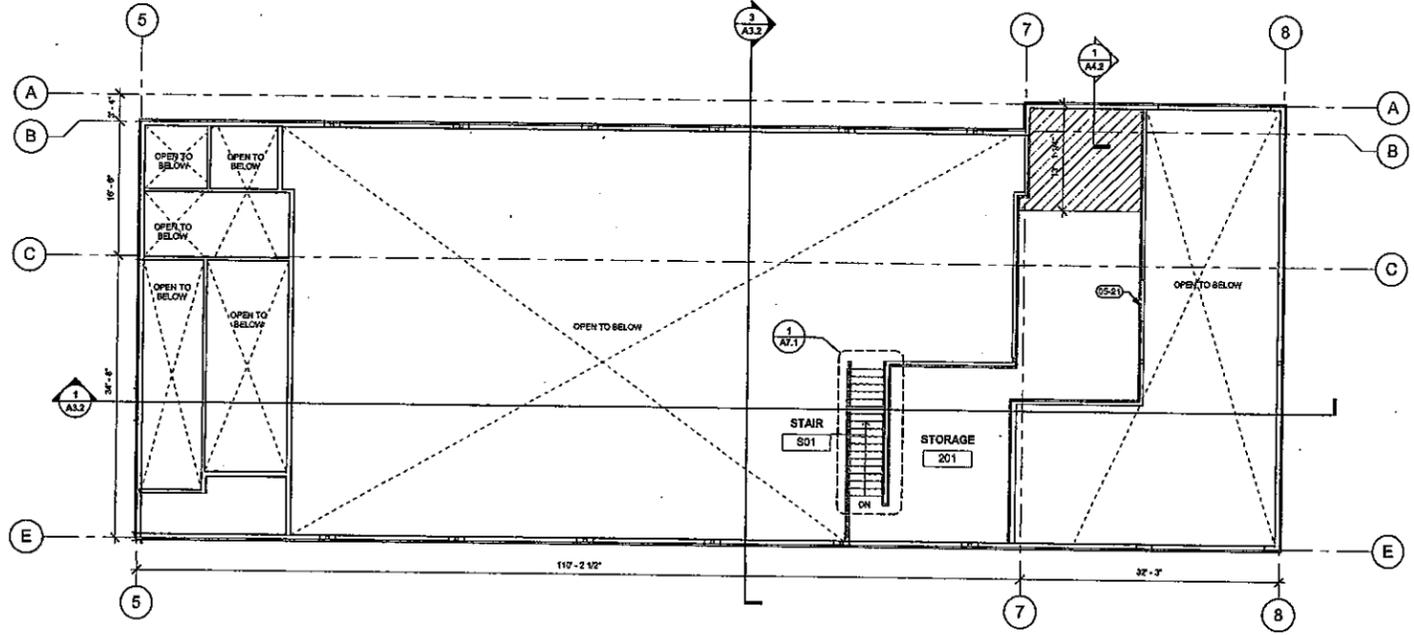
- EXTERIOR WALL - METAL PANEL ASSEMBLY
- EXTERIOR WALL - CONCRETE
- EXTERIOR WALL - NICHOSSA ASSEMBLY
- INTERIOR 4" WALL
- INTERIOR 8" WALL
- INTERIOR GLAZING
- GRIDLINES
- KEYNOTES

GENERAL NOTES

- A. SEE PLANS AND DETAILS FOR PARTITION TYPE AND CONSTRUCTION
- B. SEE INDIVIDUAL FLOOR PLANS FOR ADDITIONAL INFORMATION
- C. SEE ELECTRICAL DRAWINGS FOR EOT LIGHTING
- D. EMERGENCY POWER PROVIDED BY GENERATOR ON SITE, SEE ELECTRICAL DRAWINGS

KEYNOTES

- 00-01 LINE OF CANOPY ABOVE
- 00-05 LINE OF SOFFIT ABOVE
- 03-07 CONCRETE BOLLARD, PAINTED, SEE DETAIL 414.8
- 05-03 TRENCH DRAIN, CENTER OF APPARATUS BAY
- 05-21 OPERABLE OR REMOVABLE GATE
- 05-24 TRENCH DRAIN
- 06-21 SOLID SURFACE COUNTERTOP (SS-2)
- 10-04 TEMPERED MIRROR
- 11-04 WASHER (OFC)
- 11-06 TURNDOWN STORAGE SYSTEM (OFC)
- 11-08 STACKED DRYER (OFC)
- 11-09 READER BOARD (OFC)
- 11-18 EXTRACTOR - SEE STRUCTURAL FOR THICKENED SLAB
- 11-17 HOSE DRYER
- 22-03 MOP SINK
- 22-06 UTILITY SINK
- 22-11 DECON SHOWER



2 MEZZANINE FLOOR PLAN  
 1/8" = 1'-0"

ATTACHMENT H

MECHANICAL/ELECTRICAL  
 INTERFACE ENGINEERING  
 708 SW 3RD, SUITE 400  
 PORTLAND, OR 97204

LANDSCAPE  
 FISCHER BOUMA PARTNERSHIP  
 310 MADISON AVENUE  
 SOUTH SUITE A  
 BAINBRIDGE ISLAND, WA  
 98110



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 8895 MADISON AVE. NE  
 BAINBRIDGE WA, 98110

Project  
**BAINBRIDGE ISLAND FIRE DEPT STATION 22**  
 7934 BUCKLIN HILL ROAD NE

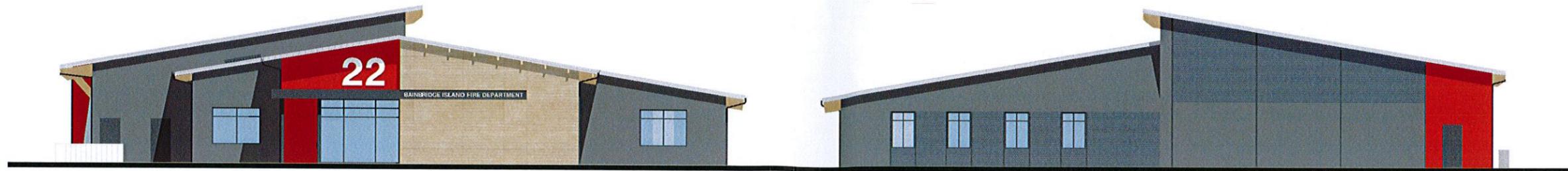


Revision Schedule	
Revision Date	Issue Date

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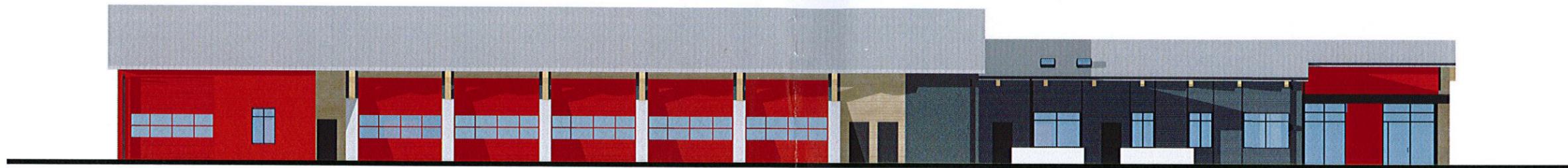
SHEET TITLE  
**FIRST & MEZZANINE FLOOR PLAN**

SHEET  
**A2.1**  
 JOB NO. 2150124.00

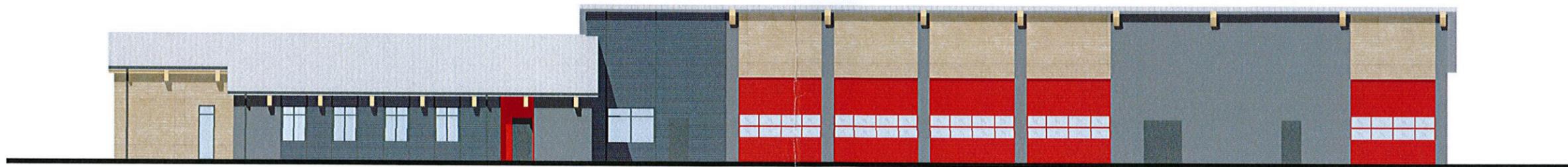


SOUTH ELEVATION

NORTH ELEVATION



WEST ELEVATION



EAST ELEVATION

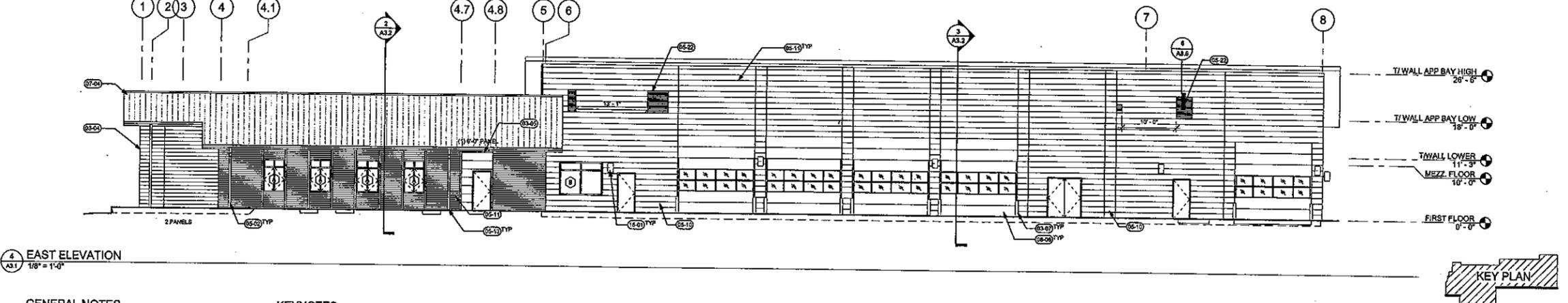
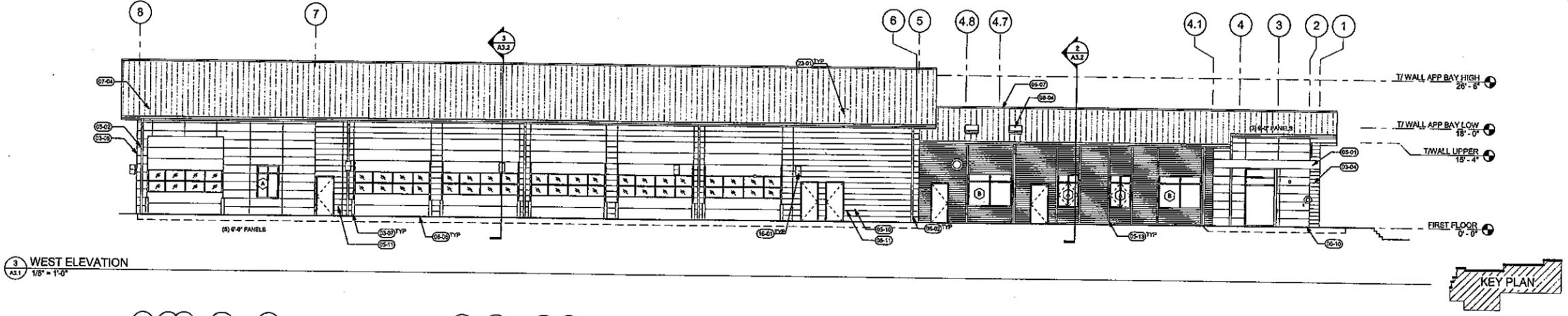
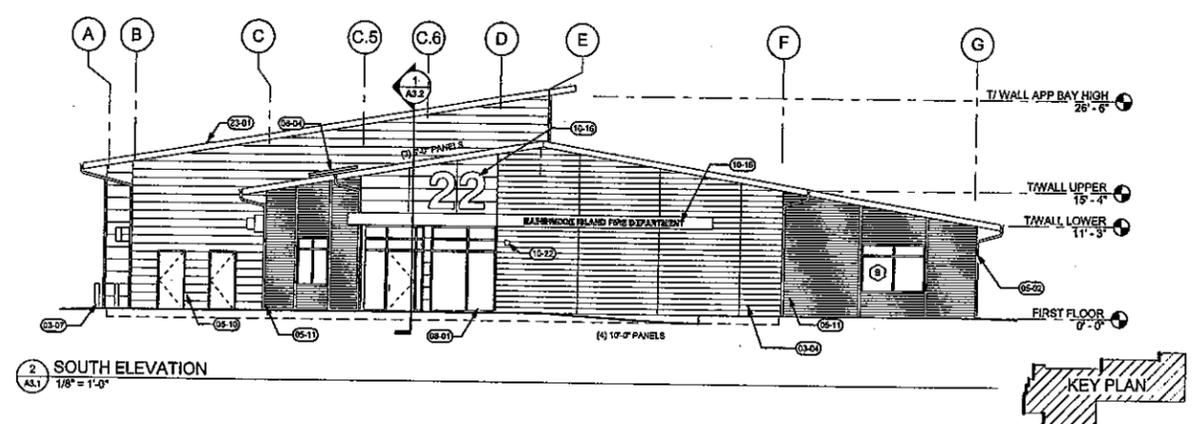
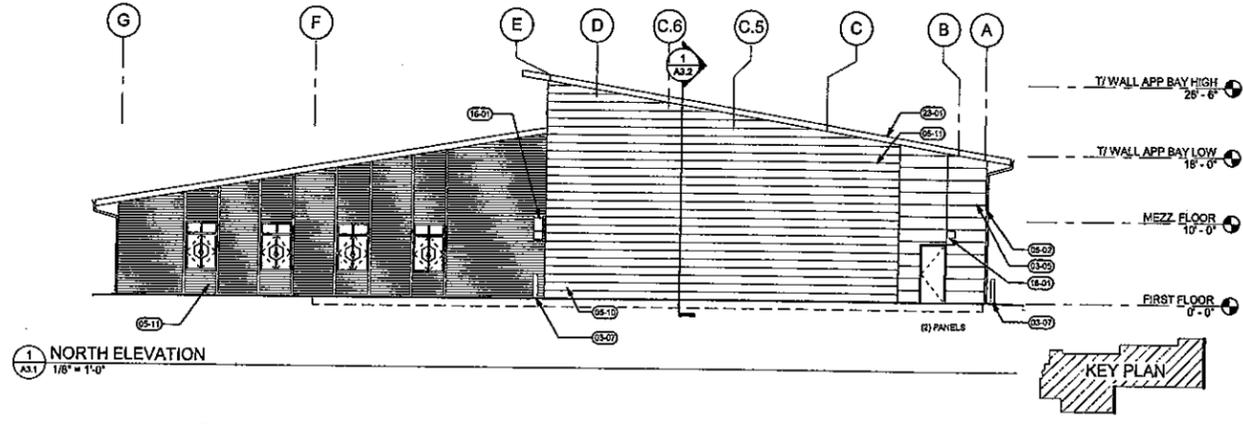
- Red Fiber Cement Board
- Wood Fiber Cement Board
- Flush Metal Panel / Corrugated Metal Panel

**EXTERIOR ELEVATIONS**

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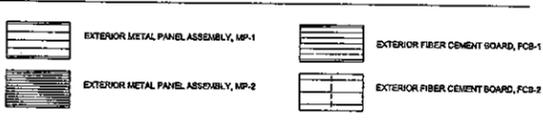
GENERAL NOTES

- A. SEE ELEVATIONS FOR EXTERIOR WINDOW TYPE DESIGNATION
- B. ELEVATION OF FINISH FLOOR ELEVATION AS INDICATED IN THE CIVIL DRAWINGS. FINISH GRADE VARIES AT BUILDING PERIMETER
- C. CONDITIONS, REFERENCE CIVIL DRAWINGS FOR ADDITIONAL INFORMATION. CONTRACTOR SHALL VERIFY AND CORRECT ALL DIMENSIONS AND LAYOUT INFORMATION. NOTIFY ARCHITECT OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO CONSTRUCTION. DO NOT SCALE DRAWINGS
- D. REFER TO UNABBREV ELEVATIONS WHERE INDICATED
- E. SEE A3.2 FOR GLAZING SCHEDULE

KEYNOTES

- 03-04 EXTERIOR FIBER CEMENT BOARD ASSEMBLY, FCB-1
- 03-05 EXTERIOR FIBER CEMENT BOARD ASSEMBLY, FCB-2
- 03-07 CONCRETE SOLLARD, PAINTED. SEE DETAIL 4.1A.8
- 05-01 EXTERIOR STEEL CANOPY
- 05-02 METAL DOWNSPOUT PER DETAIL 7.1A.2
- 05-07 6" PREFINISHED METAL GUTTER W/ MIN. 1/8" PER FOOT SLOPE COORDINATE CONNECTION WITH PLUMBING AND CIVIL
- 05-10 EXTERIOR METAL PANEL ASSEMBLY - MP-1
- 05-11 EXTERIOR METAL PANEL ASSEMBLY - MP-2
- 05-13 BRASS METAL
- 05-22 METAL LOUVER
- 07-04 STANDING SEAM METAL ROOF ASSEMBLY, SEE DETAIL 1.1A.1
- 08-01 ALUMINUM-FRAMED STOREFRONT SYSTEM - SEE GLAZING SCHEDULE
- 08-04 SKYLIGHT, REFER TO DETAIL 2.1A.1
- 08-08 OVERHEAD SECTIONAL DOOR, SEE DOOR SCHEDULE
- 10-16 EXTERIOR SIGNAGE, SEE DETAIL 7.1A.6
- 10-22 EMERGENCY ACCESS KNOX BOX - VERIFY MOUNTING HEIGHT W/ OWNER PRIOR TO INSTALLATION. RECESS AND GUTE BRICK AS REQUIRED FOR INSTALLATION
- 18-01 LIGHT FIXTURE (REFER TO ELECTRICAL)
- 23-01 ROOF TOP VENTILATOR - SEE MECHANICAL

LEGEND



City of Bainbridge Island

OCT 18 2016

Planning and  
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MECHANICAL/ELECTRICAL  
INTERFACE ENGINEERING  
708 SW 3RD, SUITE 400  
PORTLAND, OR 97204

LANDSCAPE  
FISCHER BOUMA PARTNERSHIP  
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SOUTH, SUITE A  
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Project  
**BAINBRIDGE ISLAND FIRE DEPT STATION 22**  
7934 BUCKLIN HILL ROAD NE



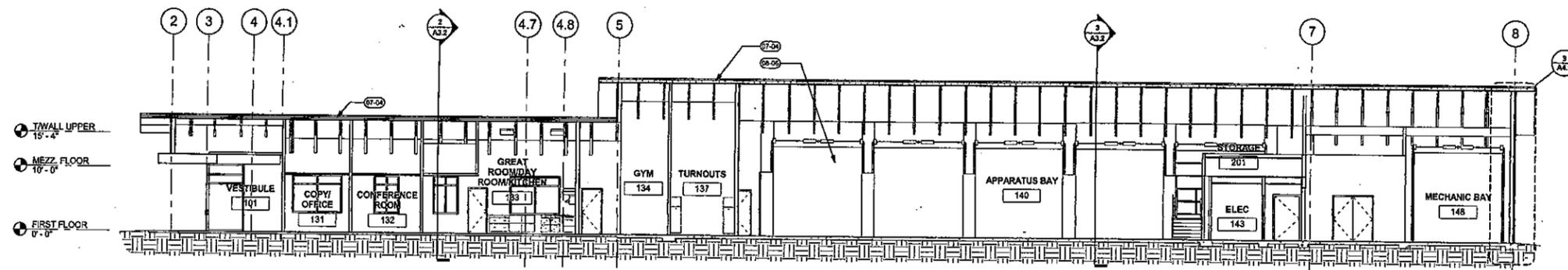
Revision Schedule	
Revision Date	Issue Date

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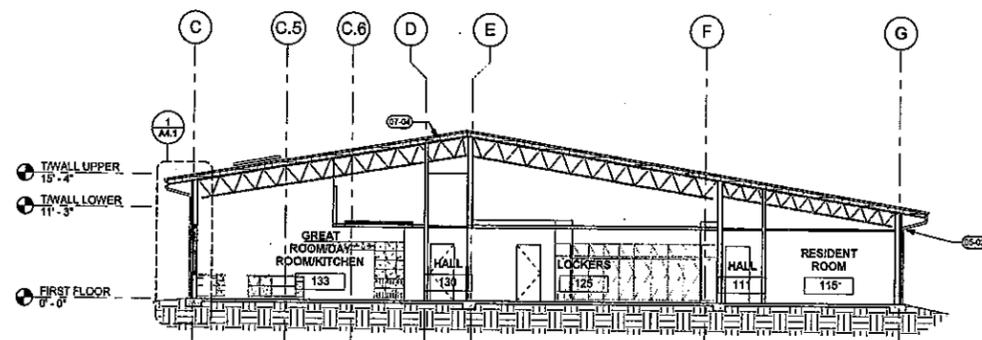
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**BUILDING ELEVATIONS**

**A3.1**

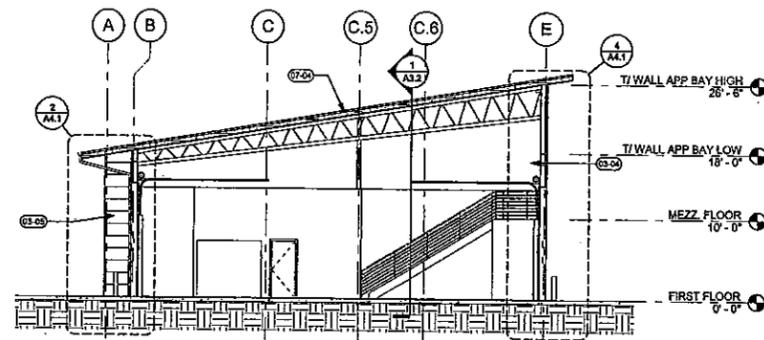
JOB NO. 2150124.00



1 BUILDING SECTION - NORTH/SOUTH  
A3.2 1/8" = 1'-0"



2 BUILDING SECTION LOOKING NORTH  
A3.2 1/8" = 1'-0"



3 BUILDING SECTION LOOKING NORTH  
A3.2 1/8" = 1'-0"



**GENERAL NOTES**

- A. FINISH FLOOR ELEVATION (FF) MAIN FLOOR 0'-0" = 148.57
- B. FF ELEVATION AS INDICATED ON CIVIL PLANS
- C. ALL GLAZING SYSTEMS ARE ALUMINUM CASSEMENT UNLESS NOTED OTHERWISE
- D. SEE A3.2 FOR GLAZING SCHEDULE
- E. SEE SALL.1 FOR TYPICAL SEALANT JOINT

**KEYNOTES**

- 03-04 EXTERIOR FIBER CEMENT BOARD ASSEMBLY, FCB-1
- 03-05 EXTERIOR FIBER CEMENT BOARD ASSEMBLY, FCB-2
- 05-02 METAL DOWNSPOUT PER DETAIL 7/AS.2
- 05-10 EXTERIOR METAL PANEL ASSEMBLY, MP-1
- 07-04 STANDING SEAM METAL ROOF ASSEMBLY, SEE DETAIL 1/AS.1
- 08-06 OVERHEAD SECTIONAL DOOR, SEE DOOR SCHEDULE

**LEGEND**

- EXTERIOR METAL PANEL ASSEMBLY, MP-1
- EXTERIOR METAL PANEL ASSEMBLY, MP-2
- EXTERIOR FIBER CEMENT BOARD, FCB-1
- EXTERIOR FIBER CEMENT BOARD, FCB-2
- WINDOW TYPE, SEE A3.2
- INTERIOR WINDOW TYPE, SEE A3.2

City of Bainbridge Island

OCT 18 2016

Planning and  
Community Development

MECHANICAL/ELECTRICAL  
INTERFACE ENGINEERING  
706 SW 3RD, SUITE 400  
PORTLAND, OR 97204

LANDSCAPE PARTNERSHIP  
FISCHER BOUMA PARTNERSHIP  
310 MADISON AVENUE  
SOUTH, SUITE A  
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Project  
**BAINBRIDGE ISLAND FIRE DEPT STATION 22**  
7934 BUCKLIN HILL ROAD NE

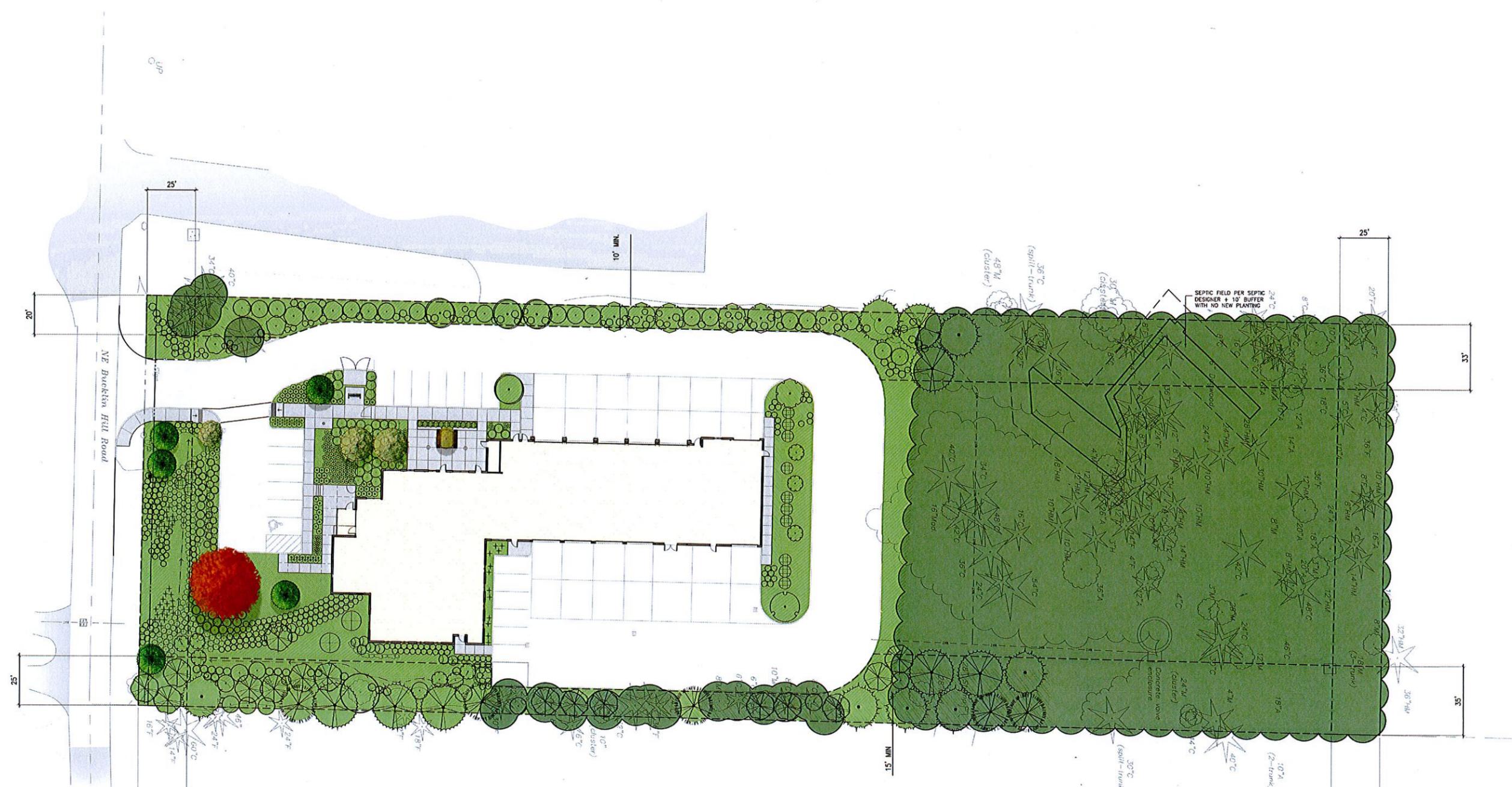


Revision Schedule	
Revision Delta	Issue Date

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CHECKED BY: BLH

SHEET TITLE:  
**BUILDING SECTIONS**

SHEET  
**A3.2**  
JOB NO. 2150124.00



BUFFER REQUIREMENTS:

DIRECTION	ADJACENT ZONING	PERIMETER LANDSCAPE (18.015.010 D)	STREET FRONTAGE LANDSCAPE (18.015.010 E)
NORTH	R-1 (SFR)	25' AVG / 15' MIN (FS)	NA (no street)
WEST	R-1 (Am Legion)	20' AVG / 10' MIN (FS)	NA (no street)
SOUTH	R-0.4 (school)	NA b/c Bucklin Rd	25' AVG/ 15' MIN (PS) Bucklin
EAST	R-1 (SFR)	25' AVG / 15' MIN (FS)	NA (no street)

BUFFERS PROVIDED:

DIRECTION	LINEAR FEET	AVG WIDTH REQ	SQUARE FEET REQ'D FOR AVERAGING	SQUARE FEET PROVIDED
NORTH	208.6	25'	5,205 SF	5,205 SF
WEST	638.5	20'	12,770 SF	13,822 SF
SOUTH	208.6	25'	5,205 SF	4,417 SF + ROAD ACCESS
EAST	638.5	25'	15,963 SF	16,574 SF

LANDSCAPE PLAN  
1"=20'-0"



<p>MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204</p>	<p>LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110</p>	<p>Architecture • Interiors • Planning • Engineering Client</p> <p>Portland, OR 503.224.9560 Vancouver, WA 360.695.7879 Seattle, WA 206.749.9993</p> <p><b>MACKENZIE.</b> DESIGN DRIVEN   CLIENT FOCUSED</p>	<p>BAINBRIDGE ISLAND FIRE DEPARTMENT 8895 MADISON AVE. NE BAINBRIDGE ISLAND, WA 98110</p>	<p>Project</p> <p>BAINBRIDGE ISLAND FIRE DEPT STATION 22</p>		<p>REVISIONS:</p> <table border="1"> <thead> <tr> <th>NO.</th> <th>REVISION</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	NO.	REVISION	DATE				<p>© MACKENZIE &amp; ASSOCIATES, INC. ALL RIGHTS RESERVED. THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER WITHOUT PRIOR WRITTEN PERMISSION.</p> <p>DRAWN BY: _____ CHECKED BY: _____</p>	<p>SHEET TITLE:</p> <p>ILLUSTRATIVE LANDSCAPE PLAN</p>	<p>SHEET</p> <p><b>L0.0</b></p> <p>JOB NO. 2150124.00</p>
NO.	REVISION	DATE													

**Tree Retention Analysis**

Project: BIFD -Station 22  
 Performed by: Fischer Bouma Partnership  
 Date: Revised 4/19/2016

**REQUIRED TREE UNITS**

PROJECT SITE: 2.18 Acres (Excludes Buffer Area of 0.88 acres) 3.06 total acres minus 0.88 buffer acres  
 CURRENT TOTAL TREE UNITS: 239.2 TUs (Excludes Buffers, unless tree is removed in buffer)  
 CURRENT TREE UNITS PER ACRE: 109.7 TUs/AC (Excludes Buffers)  
 TOTAL TREE UNITS REQUIRED: 87.2 Total per 18.15.010.G.4.iii (40 per acre)

**SUMMARY OF TREE UNITS RETAINED & NEW REQUIRED**

TREE UNITS RETAINED (SEE DATA BELOW): 95.4  
 NEW TREE UNITS REQUIRED (MIN.): 0.0

**TREE UNIT DESIGNATIONS PER CODE**

Table 18.15.010-7: Tree Unit Conversion Table for Preserved Trees

DBH	Tree Units	KEY:
0-2	0.0	
3-5	1.0	Retained
6-10	1.2	
11-12	1.4	Removed
13-15	2.0	
16-18	3.2	
19-20	3.8	Not required to be retained outside of screens/buffers
21-23	4.6	
24-26	6.2	
27-28	7.0	
29-30	7.8	
30+	8.2	

**DATA**

Notes:

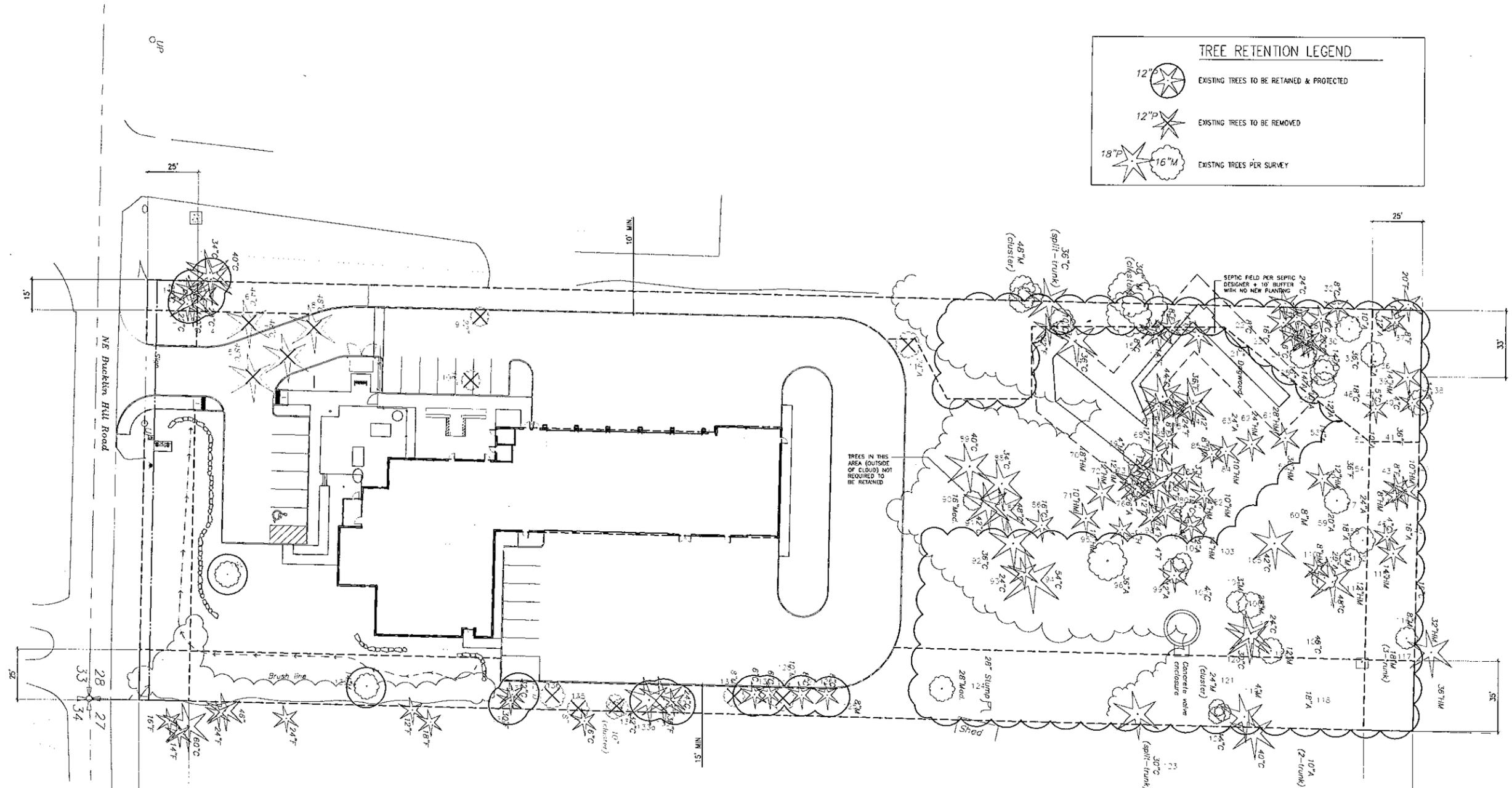
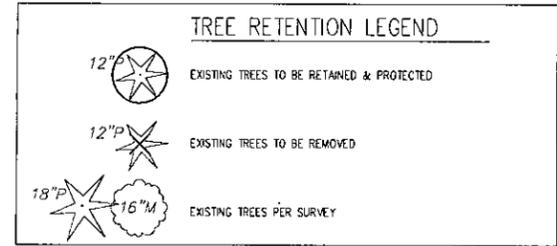
Trees within buffers are listed below but the tree units associated with each are not calculated as TU's retained (unless they need to be removed, see arborist report)  
 Trees potentially impacted by development were assessed by Katy Bigelow, Arborist, on April 13, 2016 and recommendations incorporated into this spreadsheet

Tree No.	Species	Size (DBH)	TU Retained	TU Removed	be Retained	Notes
001	Cedar	22	0.0	0.0		Buffer
002	Cedar	22	0.0	0.0		Buffer
003	Cedar	40	0.0	0.0		Buffer
004	Cedar	20	0.0	0.0		Buffer
005	Cedar	48	0.0	8.2		
006	Cedar	44	0.0	8.2		
007	Cedar	48	0.0	8.2		
008	Cedar	48	0.0	8.2		
009	Plum	22	0.0	4.6		
010	Plum	16	0.0	0.0		Blown over- observed by arborist 4/13/2016, not included in calc.
011	Alder	24	0.0	6.2		
012	Fir	22	0.0	0.0	4.6	
013		10	0.0	0.0		Buffer
014	Cedar	36	0.0	0.0	8.2	
015	Cedar	8	0.0	0.0	1.2	
016	Fir	14	0.0	0.0	2.0	
017	Cedar	44	0	0.0	8.2	
018	Fir	36	0	0.0	8.2	
019	Dogwood	8	0.0	0.0		Buffer
020	Cedar	14	0.0	0.0	2.0	
021	Cedar	6	0.0	0.0		Buffer
022	Cedar	8	0.0	0.0		Buffer
023	Alder	16	0.0	0.0		Buffer
024	Cedar	6	0.0	0.0		Buffer
025	Cedar	16	0.0	0.0		Buffer
026	Cedar	6	0.0	0.0		Buffer
027	Cedar	10	0.0	0.0		Buffer
028	Cedar	16	0.0	0.0		Buffer
029	Alder	14	0.0	0.0		Buffer
030	Cedar	20	0.0	0.0		Buffer
031	Cedar	8	0.0	0.0		Buffer
032	Cedar	8	0.0	0.0		Buffer
033	Cedar	36	0.0	0.0		Buffer
034	Alder	10	0.0	0.0		Buffer
035	Alder	12	0.0	0.0		Buffer
036	Alder	14	0.0	0.0		Buffer
037	Fir	8	0.0	0.0		Buffer
038	Alder	14	0.0	0.0		Buffer
039	Hemlock	14	0.0	0.0		Buffer
040	Cedar	4	0.0	0.0		Buffer
041	Fir	36	0.0	0.0		Buffer
042	Hemlock	10	0.0	0.0		Buffer
043	Fir	8	0.0	0.0		Buffer
044	Hemlock	8	0.0	0.0		Buffer
045	Alder	16	0.0	0.0		Buffer

046	Cedar	4	0.0	0.0		Buffer
047	Cedar	5	0.0	0.0		Buffer
048	Cedar	18	0.0	0.0		Buffer
049	Alder	14	0.0	0.0		Buffer
050	Alder	10	0.0	0.0		Buffer
051	Alder	12	0.0	0.0		Buffer
052	Alder	10	0.0	0.0		Buffer
053	Alder	14	0.0	0.0		Buffer
054	Fir	36	8.2	0.0		
055	Hemlock	12	1.4	0.0		
056	Hemlock	30	7.8	0.0		
057	Alder	24	6.2	0.0		
058	Alder	18	3.2	0.0		
059	Alder	20	3.8	0.0		
060	Maple	8	1.2	0.0		
061	Hemlock	28	0.0	0.0	7.0	
062	Hemlock	24	0.0	0.0	6.2	
063	Alder	24	0.0	0.0	6.2	
064		12	0.0	0.0	1.4	
065	Fir	24	0.0	0.0	6.2	
066	Fir	8	0.0	0.0	1.2	
067	Fir	8	0.0	0.0	1.2	
068	Fir	12	0.0	0.0	1.4	
069	Maple	4	0.0	0.0	1.0	
070	Hemlock	18	0.0	0.0	3.2	
071	Hemlock	10	0.0	0.0	1.2	
072	Hemlock	12	0.0	0.0	1.4	
073	Hemlock	12	0.0	0.0	1.4	
074	Maple	12	0.0	0.0	1.4	
075	Fir	30	0.0	0.0	7.8	
076	Alder	26	0.0	0.0	6.2	
077	Cedar	12	0.0	0.0	1.4	
078	Fir	30	0.0	0.0	7.8	
079	Fir	10	0.0	0.0	1.2	
080	Cedar	16	0.0	0.0	3.2	
081	Hemlock	4	0.0	0.0	1.0	
082	Hemlock	10	0.0	0.0	1.2	
083	Holly	3	0.0	0.0	0.0	Invasive- Remove, not included in calcs
084	Hemlock	10	0.0	0.0	1.2	
085	Hemlock	8	0.0	0.0	1.2	
086	Cedar	16	0.0	0.0	1.2	
087	Cedar	48	0.0	0.0	3.2	
088	Cedar	34	0.0	0.0	8.2	
089	Cedar	40	0.0	0.0	8.2	
090	Madrona	16	0.0	0.0	8.2	
091	Cedar	42	0.0	0.0	3.2	
092	Cedar	36	8.2	0.0		
093	Cedar	24	8.2	0.0		
094	Cedar	54	6.2	0.0		
095	Hemlock	10	0.0	0.0	8.2	
096	Hemlock	36	1.2	0.0		
097	Holly	3	0.0	0.0	8.2	
098	Fir	14	0.0	0.0	1.0	
099	Alder	12	1.4	0.0		
100	Fir	4	0.0	0.0	1.0	
101	Alder	12	1.4	0.0		
102	Cedar	4	1.0	0.0		
103	Hemlock	14	2.0	0.0		
104	Maple	3	1.0	0.0		
105	Cedar	42	8.2	0.0		
106	Maple	28	7.0	0.0		
107	Cedar	24	6.2	0.0		
108	Maple	12	1.4	0.0		
109	Cedar	46	8.2	0.0		
110	Hemlock	8	1.2	0.0		
111	Alder	20	3.8	0.0		
112	Maple	3	1.0	0.0		
113	Cedar	48	8.2	0.0		
114	Hemlock	12	1.4	0.0		
115	Hemlock	14	0.0	0.0		Buffer
116	Maple	8	0.0	0.0		Buffer
117	Maple	18	0.0	0.0		Buffer
118	Alder	18	0.0	0.0		Buffer
119	Maple	4	0.0	0.0		Buffer
120	Cedar	30	7.8	0.0		
121	Maple	24	0.0	0.0		Buffer
122	Cedar	44	0.0	0.0		Buffer
123	Cedar	30	0.0	0.0		Buffer
124	Madrona	28	0.0	0.0		Buffer
125	Lilac Shrub	8	0.0	0.0		Buffer- Shrub per arborist, not included in calcs
126	Yellow Arborvitae	8	0.0	0.0		Buffer- Shrub per arborist, not included in calcs
127	Yellow Arborvitae	8	0.0	0.0		Buffer- Shrub per arborist, not included in calcs
128	Maple	10	0.0	1.2		Buffer
129	Yellow Arborvitae	6	0.0	0.0		Buffer- Shrub per arborist, not included in calcs
130	Yellow Arborvitae	6	0.0	0.0		Buffer- Shrub per arborist, not included in calcs
131	Maple	8	0.0	1.2		Buffer
132	Cedar	24	0.0	0.0		Buffer

133	Fir	36	0.0	0.0	Buffer
133a	Cedar	32	0.0	0.0	Buffer
134	Willow	10	0.0	0.0	Buffer- remove per arborist visit 4/13/2016
135	Willow	8	0.0	0.0	Buffer- remove per arborist visit 4/13/2016
136	Willow	12	0.0	0.0	Buffer- remove per arborist visit 4/13/2016
137	Cedar	32	0.0	0.0	Buffer
138	Fir	30	0.0	0.0	Buffer
139	Elm	14	0.0	0.0	Buffer
139a	Maple	14	0.0	0.0	Buffer- not on survey. Remove per arborist visit 4/13/2016
140	Japanese Maple	20	3.8	0.0	

	RETAINED	REMOVED	Not Req'd	TOTAL
TOTAL TREE UNITS (not in buffers):	95.4	45.0	97.8	239.2



**BUFFER REQUIREMENTS:**

DIRECTION	ADJACENT ZONING	PERIMETER LANDSCAPE (18.015.010 D)	STREET FRONTAGE LANDSCAPE (18.015.010 E)
NORTH	R-1 (SFR)	25' AVG / 15' MIN (FS)	NA (no street)
WEST	R-1 (Am Legion)	20' AVG / 10' MIN (FS)	NA (no street)
SOUTH	R-0.4 (school)	NA b/c Bucklin Rd	25' AVG / 15' MIN (PS) Bucklin
EAST	R-1 (SFR)	25' AVG / 15' MIN (FS)	NA (no street)

**BUFFERS PROVIDED:**

DIRECTION	LINEAR FEET	AVG WIDTH REQ	SQUARE FEET REQ'D FOR AVERAGING	SQUARE FEET PROVIDED
NORTH	208.6	25'	5,205 SF	5,205 SF
WEST	638.5	20'	12,770 SF	12,968 SF
SOUTH	208.6	25'	5,205 SF	4,417 SF + ROAD ACCESS
EAST	638.5	25'	15,963 SF	16,574 SF

ATTACHMENT N

**TREE RETENTION PLAN**  
1" = 20'-0"



<p>MECHANICAL/ELECTRICAL INTERFADE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204</p>	<p>LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110</p>	<p><b>M.</b></p> <p>Architecture • Interiors • Planning • Engineering</p> <p>Portland, OR 503.224.9550    Vancouver, WA 360.695.7879    Seattle, WA 206.749.9993</p> <p><b>MACKENZIE</b></p> <p>DESIGN DRIVEN   CLIENT FOCUSED</p>	<p>Client <b>BAINBRIDGE ISLAND FIRE DEPARTMENT</b> 8895 MADISON AVE. NE BAINBRIDGE ISLAND, WA 98110</p>	<p>Project <b>BAINBRIDGE ISLAND FIRE DEPT STATION 22</b></p>	<p>STATE OF WASHINGTON PROFESSIONAL LANDSCAPER JERRY E. BOUMA LICENSE # 940</p>	<p>REVISIONS:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>REVISIONS PER SPEC COMMENT</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>04/20/16</td> <td>REVISIONS PER SPEC COMMENT</td> </tr> </tbody> </table> <p>DESIGNED BY: UP DRAWN BY: UP CHECKED BY: JB</p>	NO.	DATE	REVISIONS PER SPEC COMMENT	1	04/20/16	REVISIONS PER SPEC COMMENT	<p>© 2016 INVENTOR. ALL RIGHTS RESERVED. THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER WITHOUT PRIOR WRITTEN PERMISSION.</p> <p>SHEET TITLE: <b>TREE RETENTION PLAN</b></p>	<p>SHEET <b>L1.0</b></p> <p>JOB NO. 2150124.00</p>
NO.	DATE	REVISIONS PER SPEC COMMENT												
1	04/20/16	REVISIONS PER SPEC COMMENT												

PLANT SCHEDULE

TREES				GROUND COVERS								
CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SIZE	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SPACING	
ABI GRA	5	ABIES GRANDIS	GRAND FIR	1/4	6' H.T.		ARC UVA	2,156	ARCTOSTAPHYLOS UVA-URSI 'MASSACHUSETTS'	MASSACHUSETTS KINNICKINICK	4" POT	24" o.c.
ACE CR	16	ACER CIRCINATUM	WINE MAPLE	B & B	2" CAL.		CAR GB4	397	CAREX OBLUPTA	SLOUGH SEDGE	PLUG	18" c.c.
ACE MON	7	ACER GINNALIA 'MOND'	RED Rhapsody AMUR MAPLE	B & B	2" CAL.		DRY BR1	16	DRYOPTERIS ERYTHROSCA 'BRILLIANCE'	AUTUMN FERN	4" POT	24" c.c.
ACE RJB	1	ACER RUBRUM 'ARMS' TONG'	ARMS' TONG RED MAPLE	B & B	2" CAL.		JUN PAT	382	JUNCEUS PATEUS	CALIFORNIA GRAY RUSH	4" POT	24" c.c.
BET LIT	9	BETULA NIGRA 'LITTLE KING' TM	FOX VALLEY BIRCH	B & B	2" CAL.		LON PIL	46	LONGERA PLEATA	PRYET HONEYBUCKLE	1 GAL	48" c.c.
CER OCC	4	CERCIS OCCIDENTALIS	WESTERN REDBUD	B & B	2" CAL.		MAH REP	262	MAHONIA REPENS	CREEPING MAHONIA	4" POT	36" c.c.
CHI FR	2	CHIONANTHUS VIRGINICUS	WHITE FRINGETREE	B & B	2" CAL.		OPH LAP	1,262	OPHOPOGON JAPONICUS	MONDO GRASS	4" POT	12" c.c.
COR KN3	4	CORNUS X 'KX 38-8'	VENUS DOGWOOD	1/4	2" CAL.		POL ML2	585	POLYSTICHUM MUNITUM	WESTERN SWORD FERN	4" POT	42" c.c.
PIN THU	5	PINUS T-JUNBERGII 'T-UNDER-EAD'	THUNDER-HEAD SCOTCH PINE	1/4	6' H.T.		RUB EME	762	RUBUS CALYCINODES 'EMERALD CARPET'	EMERALD CARPET CREEPING RASPBERRY	4" POT	36" c.c.
PRU AMA	1	PRUNUS SERRULATA 'AMAVOGAWA'	JAPANESE FLOWERING CHERRY	B & B	2" CAL.							
PRU TA	2	PRUNUS SERRULATA 'TAZO'	ANGELS BLUSH FLOWERING CHERRY	B & B	2" CAL.							
PSE DOU	7	PSEUDOTSUGA MENZESII	DOUGLAS FIR	1/4	6' H.T.							
THJ GRE	38	THUJA OCCIDENTALIS 'GREEN GANT'	GREEN GANT ARBORVITAE	1/4	6' H.T.							
THJ PU	7	THUJA PLICATA	WESTERN RED CEDAR	1/4	6' H.T.							

SHRUBS				
CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT
COR CAR	14	CORNUS STOLONIFERA 'CARDINAL'	CARDINAL RED TWIG DOGWOOD	3 CAL.
COR FAR	13	CORNUS STOLONIFERA 'TARROW'	ARCTIC FIRE RED TWIG DOGWOOD	1 GAL
DAP CAR	6	DAPHNE X BURKWOODII 'CAROL MACKIE'	CAROL MACKIE DAPHNE	2 GAL.
GAJ SA	171	GAULTHERIA SALICINA	SALAL	1 GAL
HAM ARN	2	HAMAMELIS X INTERMEDIA 'ARNOLD PROMISE'	ARNOLD PROMISE HYBRID WITCH HAZEL	3 CAL.
HEL YEL	26	HELLEBORUS X 'YELLOW'	YELLOW HELLEBORE	1 GAL
HOS RED	8	HOSTA X 'RED OCTOBER'	RED OCTOBER HOSTA	1 GAL
MAH ADJ	68	MAHONIA AQUIFOLIUM	OREGON GRAPE	1 GAL
MAH COM	184	MAHONIA AQUIFOLIUM 'COMPACTA'	COMPACT OREGON GRAPE	1 GAL
NAV GUL	86	NANDINA DOMESTICA 'GULF STREAM' TM	HEAVENLY BAMBOO	3 CAL.
PIE BRO	11	PERIS JAPONICA 'BROUWER'S BEAUTY'	LILY OF THE VALLEY BUSH	3 CAL.
PIE FOR	13	PERIS JAPONICA 'FOREST FLAME'	FOREST FLAME LILY OF THE VALLEY BUSH	3 CAL.
POD RED	7	PODOCARPUS LAWRENCEI 'RED TIP'	MOUNTAIN PLUM PINE	3 CAL.
POL MUN	46	POLYSTICHUM MUNITUM	WESTERN SWORD FERN	1 GAL
RHA RUF	17	RHAPHICLEPS JIMBELATA 'RUF-RAPHI'	SOUTHERN MOON YEWDA HAWTHORN	3 CAL.
RHO CR	69	RHODODENDRON AZALEA 'HINO CRIMSON'	HINO CRIMSON AZALEA	2 CAL.
RHO PAC	12	RHODODENDRON MACROPHYLLUM	PACIFIC RHODODENDRON	3 CAL.
RHO CHI	34	RHODODENDRON X CHIONODES	CHIONODES RHODODENDRON	3 CAL.
THU NAV	36	THUJOPSIS DOLABRATA 'NAVA'	DEER-OAK CEDAR	3 CAL.
VAC OVA	31	VACCINIUM OVATUM	EVERGREEN HUCKLEBERRY	1 GAL

**PLANTING NOTES**

- SEE PLANT SCHEDULES FOR SPECIES, QUANTITIES, AND MINIMUM SPACING. SEE PLANTING DETAILS FOR INSTALLATION AND LAYOUT.
- CONTRACTOR SHALL ALLOW FOR THE ADDITION OF SOIL AMENDMENTS AND CONDITIONERS IN SOIL PREPARATION AND FINISH GRADING.
- ALL CONSTRUCTION MATERIAL SHALL BE EXCLUDED FROM TOPSOIL.
- LAYOUT FOR ALL PLANTING AREAS TO BE VERIFIED ON SITE BY LANDSCAPE ARCHITECT DUE TO VARYING FIELD CONDITIONS AND DENSITIES OF EXISTING VEGETATION.
- ALL PLANT MATERIAL SHALL CONFORM TO ANSI STANDARDS FOR NURSERY STOCK, LATEST EDITION.
- LANDSCAPE ARCHITECT WILL SUPERVISE THE PLANTING PROCESS. MODIFICATIONS TO THE PLANTING PLAN MAY OCCUR BASED ON FIELD CONDITIONS AND THE AVAILABILITY OF PLANT MATERIAL.
- PLANTINGS WILL BE B&B AND CONTAINERIZED NATIVE PLANT MATERIAL, AS APPROPRIATE AND SPECIFIED, FROM REGIONAL GENETIC STOCK. PLASTIC NURSERY IDENTIFICATION TAGS WILL BE ATTACHED TO THE STEM OF EACH PLANTING.
- LOCAL NURSERY STOCK WILL BE USED TO ENSURE THAT THE MATERIAL HAS ACCLIMATED TO LOCAL CONDITIONS (REDUCING PLANTING STRESS) AND IS GENETICALLY COMPARABLE WITH PLANTS IN THE LOCAL AREA.
- FINAL PLANT LISTS WILL BE CONTINGENT UPON PLANT AVAILABILITY. IF SELECTED SPECIES ARE UNAVAILABLE FROM LOCAL NURSERIES THEN UPON PRIOR APPROVAL OTHER GENUS OR SPECIES WITH SIMILAR HYDROLOGICAL REQUIREMENTS MAY BE SUBSTITUTED.
- TREES WILL BE STAKED ONLY IF A PLANT CANNOT STAND ALONE IN A MODERATE WIND PER PLANTING DETAILS.
- ALL PLANTINGS WILL BE MULCHED WITH A MINIMUM OF 3 INCHES OF ORGANIC MULCH AS SPECIFIED TO DISCOURAGE WEED GROWTH, MINIMIZE SOIL EROSION, AND RETAIN MOISTURE. THE MULCH IS NOT TO MAKE CONTACT WITH THE PLANT STEM.
- DO NOT DISTURB TREES AND VEGETATION OUTSIDE LIMIT OF WORK.
- IF TREES HAVE NOT BEEN DESIGNATED AS SAVE OR REMOVE ON DEMO OR OTHER PLANS IT SHOULD BE ASSUMED THAT THEY ARE TO BE SAVED.
- THE ENTIRE WIDTH OF THE PLANTING ISLANDS SHALL CONTAIN ONLY SOIL, AND BY WAY OF ILLUSTRATION ONLY AND NOT LAYOUT, BE FREE OF GRAVEL, CONCRETE, CONSTRUCTION DEBRIS, OR OTHER FOREIGN MATERIALS.
- ALL PLANTING AREAS TO BE IRRIGATED - IRRIGATION PLANS ARE NOT INCLUDED. IRRIGATION WILL BE DESIGN/BUILD BY CONTRACTOR BASED ON PERFORMANCE SPECIFICATIONS PROVIDED.
- DO NOT DIG BEFORE LOCATING UTILITIES.
- ALL LANDSCAPE AREAS DESIGNED FOR INSTALLATION OF TREES SHALL HAVE A MINIMUM SOIL DEPTH OF 18" PLUS 8" SCARIFIED SUBGRADE. MINIMUM SOIL DEPTH SHALL INCLUDE SOILS THAT MEET SOIL SPECIFICATIONS.
- FOR ALL TOPSOIL, THE CONTRACTOR SHALL OBTAIN A LANDSCAPE SOIL ANALYSIS CERTIFYING THAT THE SOIL MEETS THE SOIL SPECIFICATIONS. THE CERTIFICATION SHALL BE REVIEWED BY THE LANDSCAPE ARCHITECT AND APPROVED PRIOR TO SOIL DELIVERY.
- APPLY 3" DEPTH OF ORGANIC BARK MULCH IN ALL PLANTING AREAS. MULCH TO BE FREE OF WEED SEED, SANDUST, RESIN OR TANNIN, AND SHALL NOT CONTAIN ANY COMPOUNDS DETRIMENTAL TO PLANT GROWTH. PULL MULCH 3" AWAY FROM BASE OF PLANT.
- ALL PLANT MATERIAL SHOULD BE DISEASE FREE AND ARRIVE IN A VIGOROUS GROWING CONDITION.
- LANDSCAPE CONTRACTOR SHALL MAINTAIN THE SITE UNTIL FINAL INSPECTION AND ACCEPTANCE BY THE OWNER.
- ALL LANDSCAPE INSTALLATION TO BE REVIEWED AND APPROVED BY THE LANDSCAPE ARCHITECT.
- A PRE-CONSTRUCTION CONFERENCE PRIOR TO THE START OF ANY LANDSCAPE CONSTRUCTION IS REQUIRED.
- PLANTS SHALL BE INSPECTED PRIOR TO PLANTING TO VERIFY CONFORMANCE WITH PLANTING SIZE AND OTHER REQUIREMENTS.
- ALL LANDSCAPING SHALL BE WARRANTED FOR A PERIOD OF 1 YEAR AFTER FINAL ACCEPTANCE BY THE OWNER. PRIOR TO THE END OF THE WARRANTY PERIOD, THE LANDSCAPE WILL BE INSPECTED AND DEAD OR UNHEALTHY PLANTS WILL BE REQUIRED TO BE REPLACED WITH SAME SPECIES AND SIZES.

**GENERAL NOTES**

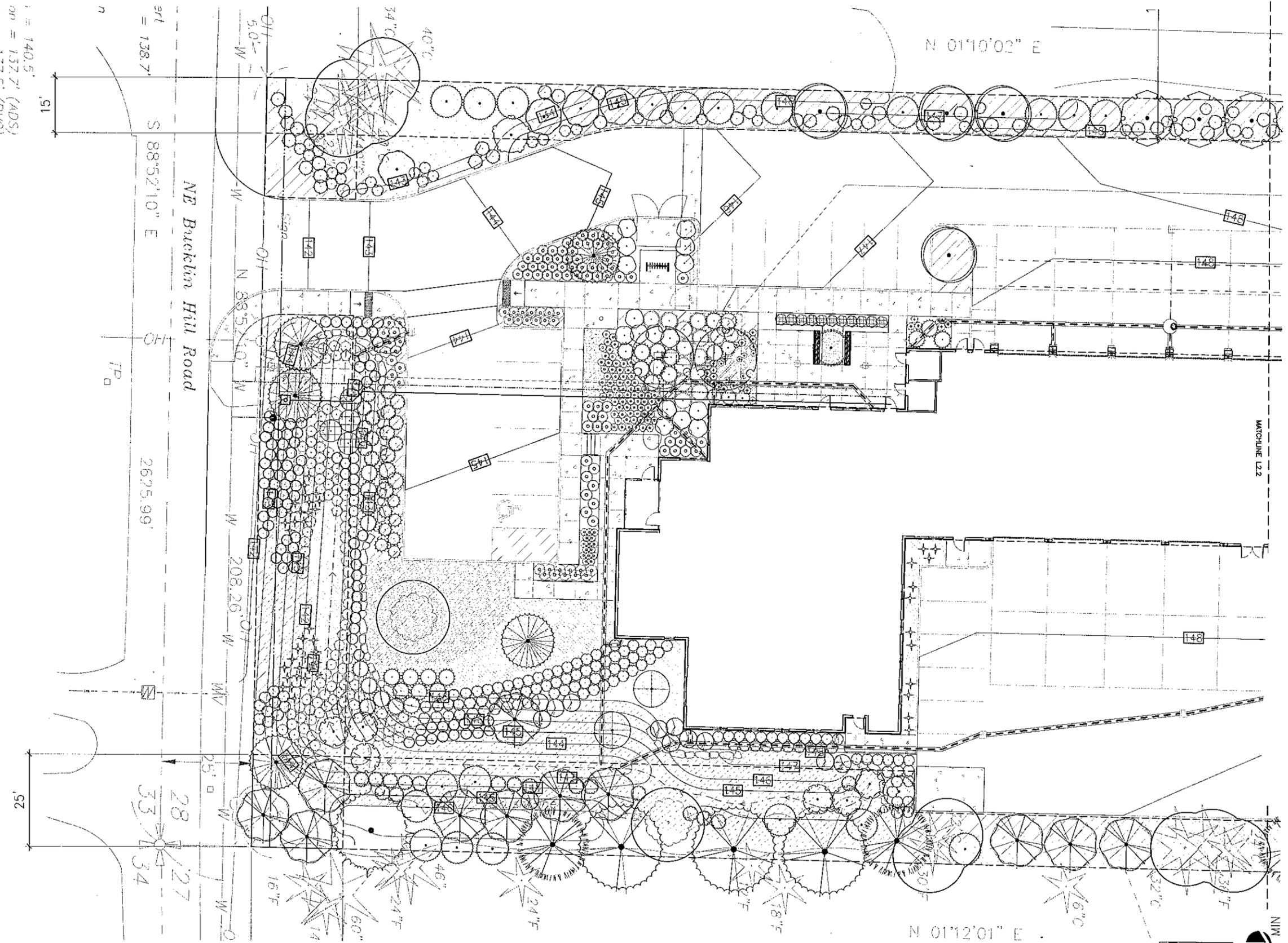
- REFER TO ARCHITECTURAL & CIVIL DRAWINGS FOR LIMITS OF WORK AND DEMOLITION OF EXISTING SITE ITEMS.
- REFER TO CIVIL DRAWINGS FOR LANDSCAPE GRADING AND DRAINAGE.
- REFER TO ARCHITECTURAL DRAWINGS FOR SITE HARDSCAPE DETAILS.

EXISTING TREES TO BE PROTECTED - REFER TO TREE RETENTION PLAN & ARBORIST'S REPORT.

<p>MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204</p>	<p>LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110</p>	<p>Architecture • Interiors • Planning • Engineering</p> <p><b>M.</b></p> <p>Bainbridge, OR 503.224.9560 Yacowitz, WA 360.695.7679 Seattle, WA 206.742.9993</p> <p><b>MACKENZIE.</b> DESIGN DRIVEN   CLIENT FOCUSED</p>	<p>Client <b>BAINBRIDGE ISLAND FIRE DEPT STATION 22</b></p>	<p>Project <b>BAINBRIDGE ISLAND FIRE DEPT STATION 22</b></p>	<p>REVISIONS:</p> <p>1. REVISED PER MEASUREMENT</p>	<p>STATE OF WASHINGTON REGISTERED LANDSCAPE ARCHITECT JERRY S. SPINA CERTIFICATE #2340</p>	<p>REVISION DATA DATE: 03/04/2016 BY: JF</p> <p>DRAWN BY: JF</p> <p>CHECKED BY: JF</p>	<p>MACKENZIE 2015 ALL RIGHTS RESERVED THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER WITHOUT PRIOR WRITTEN PERMISSION.</p>	<p>SHEET TITLE: <b>PLANT SCHEDULE + NOTES</b></p>	<p>SHEET <b>L2.0</b></p> <p>JOB NO: 2150124.00</p>
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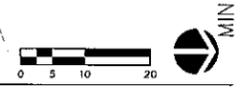
ATTACHMENT O

1" = 140.5'  
 3/4" = 137.7' (ADS)  
 3/8" = 137.5' (PVC)



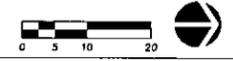
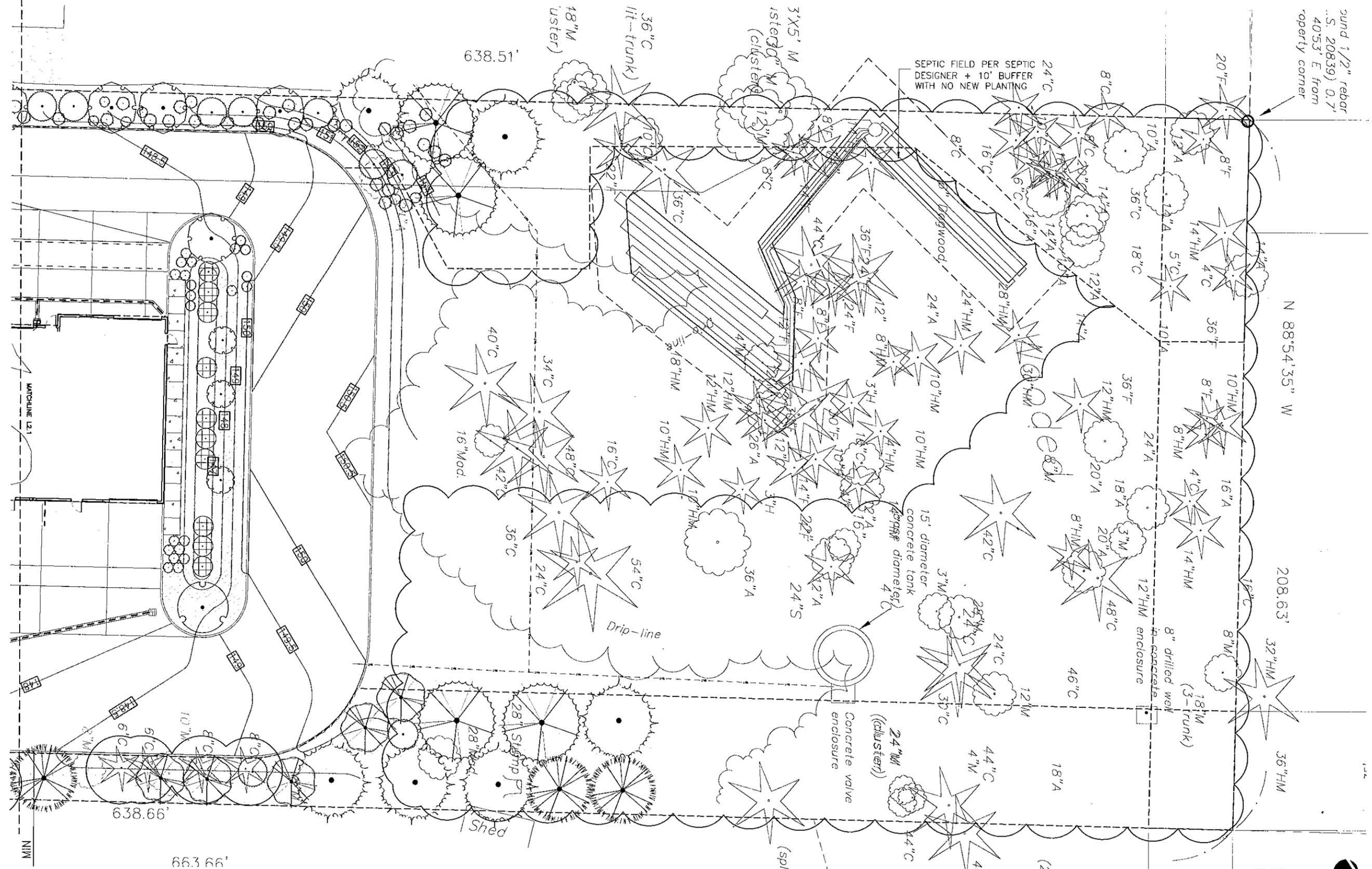
ATTACHMENT P

PLANTING PLAN - SOUTH  
1"=10'-0"



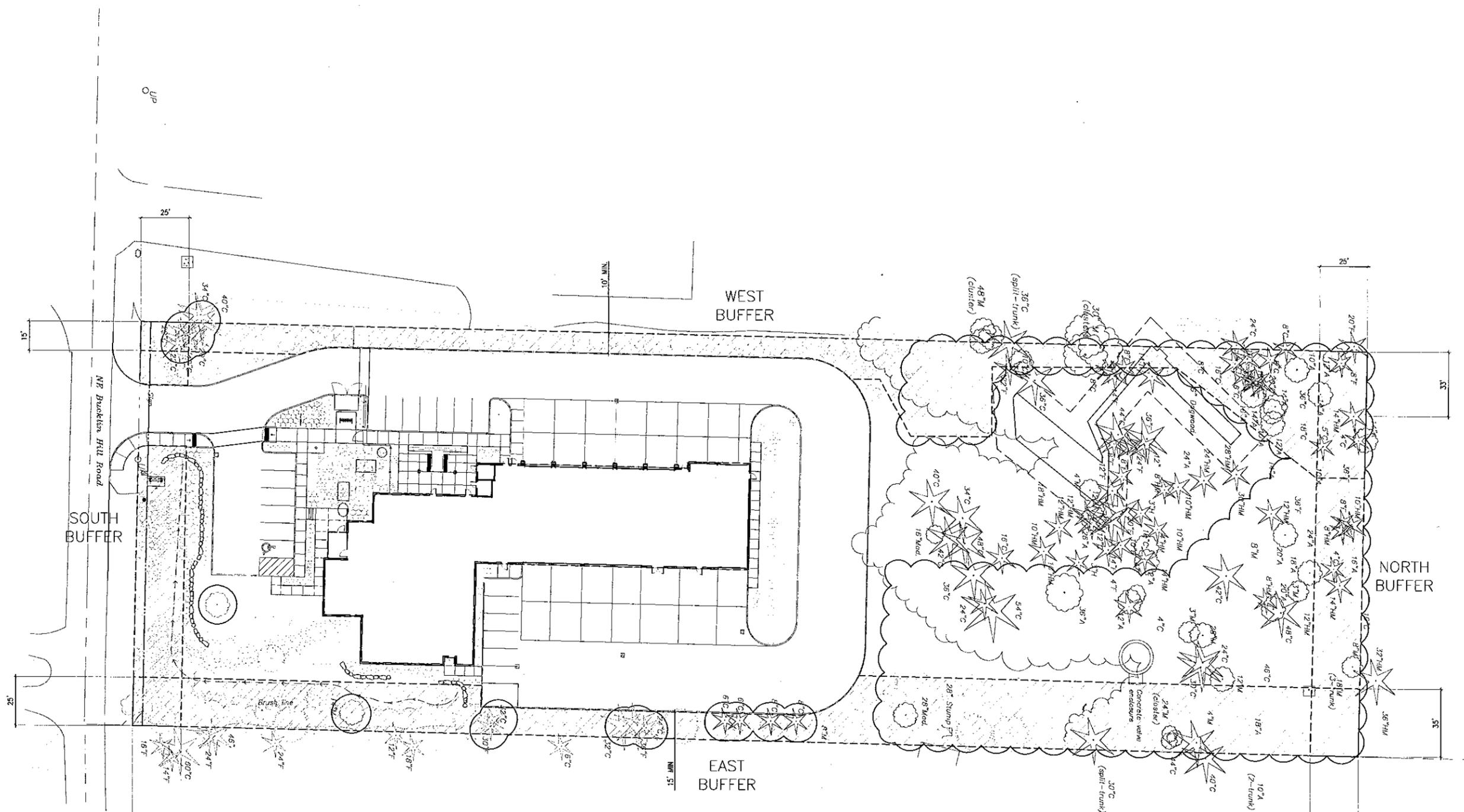
MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204	LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110		Architecture • Interiors • Planning • Engineering Client BAINBRIDGE ISLAND FIRE DEPARTMENT 8895 MADISON AVE. NE BAINBRIDGE ISLAND, WA 98110	Project BAINBRIDGE ISLAND FIRE DEPT STATION 22		REVISIONS: 1. 04/20/16 REVISED PER SPEC COMMENTS	© MACKENZIE 2016 ALL RIGHTS RESERVED THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER WITHOUT PRIOR WRITTEN PERMISSION DRAWN BY: JP CHECKED BY: JB	SHEET TITLE: PLANTING PLAN - SOUTH	SHEET L2.1 JOB NO. 2150124.00
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Found 1/2" rebar  
S. 20839) 0.7'  
40.53' E from  
property corner



PLANTING PLAN - NORTH  
1"=10'-0"

<p>MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204</p>	<p>LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110</p>	<p><b>M.</b> www.mcknz.com</p>	<p>Architecture • Interiors • Planning • Engineering Portland, OR 503.224.9560 Vancouver, WA 360.695.7879 Seattle, WA 206.748.9993</p> <p><b>MACKENZIE</b> DESIGN DRIVEN   CLIENT FOCUSED</p>	<p>Client: <b>BAINBRIDGE ISLAND FIRE DEPARTMENT</b> 8895 MADISON AVE. NE BAINBRIDGE ISLAND, WA 98110</p>	<p>Project: <b>BAINBRIDGE ISLAND FIRE DEPT STATION 22</b></p>		<p>REVISIONS: REVISION DATE BY</p>	<p>© MACKENZIE ALL RIGHTS RESERVED THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER WITHOUT PRIOR WRITTEN PERMISSION</p> <p>DRAWN BY: JP CHECKED BY: JB</p>	<p>SHEET TITLE: <b>PLANTING PLAN - NORTH</b></p>	<p>SHEET <b>L2.2</b> JOB NO. 2150124.00</p>
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**BUFFER REQUIREMENTS:**

DIRECTION	ADJACENT ZONING	PERIMETER LANDSCAPE (18.015.010 D)	STREET FRONTAGE LANDSCAPE (18.015.010 E)
NORTH	R-1 (SFR)	25' AVG / 15' MIN (FS)	NA (no street)
WEST	R-1 (Am Legion)	20' AVG / 10' MIN (FS)	NA (no street)
SOUTH	R-0.4 (school)	NA b/c Bucklin Rd	25' AVG/ 15' MIN (PS) Bucklin
EAST	R-1 (SFR)	25' AVG / 15' MIN (FS)	NA (no street)

**BUFFERS PROVIDED:**

DIRECTION	LINEAR FEET	AVG WIDTH REQ	SQUARE FEET REQ'D FOR AVERAGING	SQUARE FEET PROVIDED
NORTH	208.6	25'	5,205 SF	5,205 SF
WEST	638.5	20'	12,770 SF	12,968 SF
SOUTH	208.6	25'	5,205 SF	4,417 SF + ROAD ACCESS
EAST	638.5	25'	15,963 SF	16,574 SF

**LANDSCAPE BUFFER DIAGRAM**  
1"=20'-0"



<p>MECHANICAL/ELECTRICAL INTERFACE ENGINEERING 708 SW 3RD, SUITE 400 PORTLAND, OR 97204</p>	<p>LANDSCAPE FISCHER BOUMA PARTNERSHIP 310 MADISON AVENUE SOUTH, SUITE A BAINBRIDGE ISLAND, WA 98110</p>	<p>Architecture • Interiors • Planning • Engineering</p> <p>Portland, OR 503.224.9560 Vancouver, WA 360.695.7870 Seattle, WA 206.748.9993</p> <p><b>M. MACKENZIE</b> DESIGN DRIVEN   CLIENT FOCUSED</p>	<p>Client <b>BAINBRIDGE ISLAND FIRE DEPARTMENT</b> 8895 MADISON AVE. NE BAINBRIDGE ISLAND, WA 98110</p>	<p>Project <b>BAINBRIDGE ISLAND FIRE DEPT STATION 22</b></p>		<p>REVISIONS:</p> <table border="1"> <tr> <th>NO.</th> <th>DESCRIPTION</th> <th>DATE</th> </tr> <tr> <td>1</td> <td>ISSUED FOR PERMIT</td> <td></td> </tr> </table>	NO.	DESCRIPTION	DATE	1	ISSUED FOR PERMIT		<p>MACKENZIE 2016 ALL RIGHTS RESERVED THESE DRAWINGS ARE THE PROPERTY OF MACKENZIE AND ARE NOT TO BE USED OR REPRODUCED IN ANY MANNER WITHOUT PRIOR WRITTEN PERMISSION</p> <p>DRAWN BY: JP CHECKED BY: JB</p>	<p>SHEET TITLE: <b>LANDSCAPE BUFFER DIAGRAM</b></p>	<p>SHEET <b>L3.0</b> JOB NO. 2150124.00</p>
NO.	DESCRIPTION	DATE													
1	ISSUED FOR PERMIT														

City of Bainbridge Island

MAR 4 2016

Planning and  
Community Development

March 3, 2016

City of Bainbridge Island  
Attention: Janelle Hitch  
280 Madison Avenue N.  
Bainbridge Island, WA 98110

Re: **Bainbridge Island Fire Department - Station #22**  
*Trip Generation Letter*  
Project Number 2150124.00

Dear Ms. Hitch:

Mackenzie has prepared this letter to satisfy City of Bainbridge Island concurrency requirements for a proposed fire station by addressing the estimated trip generation and need for a traffic study. The 14,333-square-foot fire station is proposed at 7934 Bucklin Hill Road NE in Bainbridge Island, Washington. The proposed fire station will replace an existing 4,850-square-foot fire station at the same site. A site plan supporting the proposed fire station is attached to this letter.

## EXISTING TRIP GENERATION

Three buildings currently exist on the subject site, totaling approximately 4,850 square feet. The existing use on the site is a fire station including a shop addition, shipping container, and concrete and gravel pads.

The Institute of Transportation Engineers' (ITE) *Trip Generation Manual, 9th Edition* does not provide trip generation rates specific to fire stations. Mackenzie has developed a trip rate for fire station based on anticipated activity. This includes employee trips, deliveries, meetings, and emergency calls. We have found a rate of 3.49 daily trips per 1,000 square feet, based on a similar facility in Clackamas County, Oregon, is an appropriate trip rate estimate. A copy of that estimate can be provided upon request. It was assumed that 10% of the daily trips would occur during the PM peak hour. Based on these trip rates, the existing fire station's estimated trips are presented in Table 1 below.

TABLE 1 – EXISTING SITE TRIP GENERATION				
ITE Code	Land Use	Size	PM Peak Hour	Daily
N/A	Fire Station	4.85 KSF	2	17

## PROPOSED TRIP GENERATION

The existing buildings are to be demolished to accommodate the proposed 14,333-square-foot fire station. The same trip rate of 3.49 daily trips per 1,000 square feet was applied to the proposed 14,333-square-foot fire station. The proposed fire station's trip estimates are presented in Table 2.

TABLE 2 – PROPOSED SITE TRIP GENERATION				
ITE Code	Land Use	Size	PM Peak Hour	Daily
N/A	Fire Station	14.33 KSF	5	50

### NET NEW TRIP GENERATION

The fire station replace at the same site is assumed to result in an increased potential in trip generation, as noted above. The difference in existing and proposed trips is presented in Table 3 below and represents the potential for new trips on the external road network.

TABLE 3 – NET NEW SITE TRIP GENERATION		
Trip Type	PM Peak Hour	Daily
Proposed	5	50
Existing	2	17
Net New	+3	+33

The net new trip generation is estimated be an additional 3 PM peak hour trips and 33 daily trips. The City requires a traffic study for new land uses adding more than 5 PM peak hour trips or more than 50 daily trips. The net new trips are less than the City's standards for a traffic study. Therefore, a traffic study is not needed.

If you have any questions regarding the information presented in this letter, please do not hesitate to contact me.

Sincerely,



Brent Ahrend, PE  
Senior Associate | Traffic Engineer

Enclosure(s): Existing Site Plan  
Proposed Site Plan

c: Michael Chen, Brett Hanson, Janet Jones – Mackenzie









Department of Planning and Community  
Development

**Development Engineer  
Project Review**

To: Josh Machen, Planning Manger  
From: Janelle Hitch, Development Engineer *JCH*  
Date: October 13, 2016  
Re: Bainbridge Island Fire Station 22 - Site Plan Review & Conditional Use Permit

**Related Application Number:**

**PLN14200-SPR/CUP**

**Project Description:**

The proposal includes the replacement of the existing fires station with a new fire station building. Once completed, the fire station will operate in the same capacity as it is currently. No significant change in traffic is anticipated with the redevelopment. The site access will remain on Bucklin Hill Road. The location of the access point for the new station will be shifted to the west. The access point will serve for both emergency vehicles and passenger vehicle access. The site will be served by a new septic system and water service will be provided by Washington Water System. There will no longer be a shared parking lot with the neighboring American Legion Post to the west.

The site address is 7934 Bucklin Hill Road NE. The site tax parcel number is: 4178-000-012-0004.

**Comments:**

I have completed a review of the above referenced project materials received by the City of Bainbridge Island (COBI). Conditional approval of the Site Plan Review and Conditional Use Permit is recommended. Please see the comments and proposed conditions below:

1. The conditional use conforms to regulations concerning drainage in BIMC 15.20 and 15.21.
2. The conditional use will not cause an undue burden on the drainage basin or water quality and will not unreasonably interfere with the use and enjoyment of properties downstream.
3. The streets and pedestrian ways as proposed align with and are otherwise coordinated with streets serving adjacent properties, see additional condition.
4. The streets and pedestrian ways as proposed are adequate to accommodate anticipated traffic.

5. There is capacity in the water system to serve the conditional use. The applicable service can be made available at the site.
6. The conditional use conforms to the "City of Bainbridge Island Design and Construction Standards and Specifications Manual".

**Conditions:**

1. The COBI Non-Motorized 6-Year Capital Improvement Plan includes the C40-Bucklin Phase 2 Project that extends along the frontage of the Bainbridge Island Fire Station 22. Frontage improvements along Bucklin Hill Road NE corresponding to the CIP C40 Bucklin Phase 2 Project must be completed or a fee-in-lieu must be provided to the City prior to Certificate of Occupancy and/or Final Inspection. Frontage improvements include pedestrian sidewalk and bike lane.
2. All on-site stormwater facilities shall remain privately owned and maintained. The owner shall be responsible for maintenance of the storm drainage facilities for this development following construction. Annual inspection and maintenance reports shall be provided to the City. A Declaration of Covenant for stormwater system operation and maintenance will be required to be recorded before issuance of occupancy permits. The approved language for the Declaration of Covenant is found in BIMC 15.21 Exhibit A.

**Permits Required:**

1. A NPDES permit from the Department of Ecology will be required prior to ground disturbing activities.
2. A Right-of-Way permit will be required prior to any work within the right of way. The ROW permit will be subject to separate conditions and bonding requirements.



**CITY OF BAINBRIDGE ISLAND**  
**DESIGN REVIEW BOARD**  
**Regularly Scheduled Meeting Minutes**  
**Monday, December 21, 2015 at 2:00 p.m.**  
**City Council Conference Room**  
**280 Madison Ave N**  
**Bainbridge Island, Washington 98110**

Call to Order (Attendance, Agenda, Ethics)

The Ravine Apartments PLN50039 SPR Follow-up

BIFD Station 21 – Pre-application

BIFD Station 22 – Pre-application

Montessori Country School PLN17677 PRE Pre-application

Old and New Business

Adjournment

---

**Call to Order (Attendance, Agenda, Ethics)**

Chair Alan Grainger called the meeting to order at 2:01 PM. Other Design Review Board (DRB) members present were Peter Perry, Susan Bergen, Jeff Boon and Chris Gutsche. Jim McNett and Chuck Depew were absent and excused. City Staff in attendance were Planning Manager Joshua Machen, Planner Kelly Tayara and Administrative Specialist Jane Rasely who monitored recording and prepared minutes. Mr. Grainger welcomed new DRB members Mr. Gutsche and Mr. Boon.

**The Ravine Apartments PLN50039 SPR Follow-up**

Mr. Grainger gave an overview of the project to the new members. Roof slope and retaining a large tree near the building were discussed. Clients were invited in at 2:20 PM. Introductions were made all around. Devin Johnson for Johnson Squared presented the changes in roof line and the plan to save a large tree. Mr. Grainger thanked them for the changes in the roofline, saying the DRB appreciated that the bulk of the building was reduced by it. Ms. Bergen asked how they planned to save the tree, i.e., what changes were they going to make. Mr. Johnson replied they were going to move the garbage area back away from the tree and use grassy pavers instead of paving the area. Mr. Machen stated that if they were committed to saving the tree, they needed to come up with a plan for that. Mr. Johnson replied they would work with an arborist to come up with a plan for saving the tree as well as working with a Geotech engineer to plan changes to the foundation.

**Motion: I move we recommend the plan going forward.**

**Perry/Bergen: Passed unanimously 5-0**

While waiting for the BIFD architects (held up by ferry), Planner Tayara briefed the DRB about the Montessori Country School application. Mr. Gutsche asked if he needed to recuse himself for having worked with the Montessori Country School about 10 years ago. Mr. Grainger did not feel he needed to since it was so long ago, but agreed he should do so if he felt it necessary. Planner Tayara mentioned one item up for discussion was traffic and the need to keep traffic from backing up on the road during certain times of day.

**BIFD Station 21 – Pre-application**

Brett Hansen and Chauncey Drinon from Mackenzie Architects were introduced. Mr. Hansen gave an overview of their part in the design process from pre-bond stage to pre-application. He stated all the programming (design) is based on future need. Key particulars driving design were:

- Public parking moved to edge
- Pull through bays
- Medic helicopter pad
- Trees and existing storm water retention to be reused.
- Existing memorial retained
- Vegetation around helipad
- Public plaza.

There was discussion of the roof line and whether the lines would remain clean or would mechanical equipment be placed upon it. Mr. Hansen stated yes they would remain clear with the exception of ham radio antennas that would be mounted on one of the sides of the building. The number and sizes of bays were discussed as well as the size of the roof line (300 feet) and how to break up the look of it. The approach from the south when driving up Madison Avenue was also reviewed. Mr. Hansen summarized the concerns the DRB had and gave initial thoughts as to where, how and whether it was functional to respond to them. Mr. Grainger then brought up providing a connection to the existing trail behind the property. Assistant Fire Chief Luke Carpenter stated security was an issue for putting a trail through the property.

### **BIFD Station 22 – Pre-application**

Mr. Hansen gave a brief overview of the plans for Station 22 stating the same materials were being used to maintain a consistent look across the organization. Mr. Gutsche thought the design was good for the more rural setting (as opposed to Station 21) but asked if the pitch of the roof could be changed to allow for a more southern exposure to accommodate future solar panels. The DRB also expressed the desire to raise the red wall with the identifying “22” above the roof line. Mr. Grainger stated they looked forward to seeing this design at the Site Plan Review meeting.

### **Montessori Country School PLN17677 PRE Pre-application**

Introductions were made around the table with the project design team of Russ Hamlet, Brandon Hogue and Kia Micaud. Planner Tayara mentioned the public meeting for this project was being held on January 4, 2016 at the school. Mr. Hamlet expressed the vision statement of this project was to bring the two campuses together on one campus to facilitate student interaction and teacher collaboration. He continued providing an overview of the site plan showing buffers, administrative buildings and classrooms including a proposed future classroom. Ms. Micaud described landscaping, parking spaces utilizing crushed rock instead of paving, fencing, maintenance access gates and play areas. ADA accessibility was remarked upon when Mr. Grainger asked the type of materials used on the pathways that would allow wheelchair access. Mr. Hamlet discussed that deck overhangs were designed with accessibility for solar panels in mind. Storm water drainage was discussed and whether building a rain garden in the buffer would be possible. Mr. Hamlet mentioned they would come back in April or May before they applied for the building permit. Mr. Grainger spoke for the DRB saying they felt very good about this project.

### **Old and New Business**

None.

### **Adjournment**

Meeting was adjourned at 5:05 PM.

Approved by:

  
 Alan Grainger, Chair

  
 Jane Rasely, Administrative Specialist



**CITY OF BAINBRIDGE ISLAND  
DESIGN REVIEW BOARD - REGULAR MEETING**

December 21, 2015

**PLEASE PRINT**

**Join  
ListServ  
Yes/No**

Name	Affiliation	Phone/ E-Mail	Join ListServ Yes/No
Chris Gutsch		chris@ecosmithdesign.com	yes
JEFF BOON		206-499-3854	yes
PETER PERRY	DRB		
Susan Bergen	DRB		
Alan Grainger	DRB		
EMILT SCALI	Johnson Squared	emily@johnsonsguard.com	no
BILL RANSON	WLG (owner)	branson18@gmail.com	no
CHAUNCEY DRAGON	MACKENZIE	edragon@mckenze.com	
Luke Carpenter	BIFO	lcarpenter@bifd.org	
BRETT HANSON	<del>B</del> Mackenzie	bhanson@mckenze.com	
Kelsey Laughlin	Bronne Wheeler Engineers	kelseyebrownwheeler.com	yes
Meaghan Skotheim	meaghseni Country School	meaghan@meaghsenicountryschool.org	no
KID MICAUD	Catherine Micaud Landscape	KidMicaud@gmail.com	no
RUSS HAMLET	SH-A		OK
BRANON HOGG	SH-A		

**Call to Order (Attendance, Agenda, Ethics)**

Bainbridge Island Fire Department Station 21 (PLN11791SPR/CUP)

Bainbridge Island Fire Department Station 22 (PLN14200SPR/CUP)

Wyatt Cottages (PLN50165SPR)

New/Old Business

Adjourn

---

**Call to Order (Attendance, Agenda, Ethics)**

Chair Grainger called the meeting to order at 2:06 PM. Other Design Review Board members in attendance were Jim McNett, Chuck Depew and Chris Gutsche. Peter Perry, Susan Bergen and Jeff Boon were absent and excused. City Staff present were Planning Manager Josh Machen and Administrative Specialist Jane Rasely who monitored recording and prepared minutes.

The agenda was reviewed and Mr. Grainger asked that the fire departments be reviewed in the opposite order with review of BIFD Station 22 coming first. There were not any conflicts of interest disclosed.

**Bainbridge Island Fire Department Station 22 (PLN14200SPR/CUP)**

Planning Manager Josh Machen gave an overview of the changes to the project citing the unexpected change in City personnel that occurred on both BIFD projects.

Brett Hansen and Chauncey Drinon from Mackenzie Architects presented the proposed site plans for both fire stations. Mr. Drinon walked the DRB through the changes made to Station 22 stating the biggest change was the removal of the public meeting space. Landscaping elements, types of shrubbery, etc., were highlighted. He stated with the removal of the public meeting room, less impervious surface would be created and more trees would be saved as the entire project would be moved further south on the site. Mr. Grainger asked if there was a site plan available that showed the reduction in parking. Mr. Hanson replied there was not one available at that time. Mr. Gutsche asked how much closer to the street the building would be. Mr. Hanson replied it would be 20-25 feet closer. Mr. Depew asked what the reduction in the building size would be. Mr. Drinon replied the building would go from 15,000 square feet to 14,000 square feet. Color and design materials were presented and described as applicable to both Fire Station 22 and 21.

Mr. McNett asked if there would be any mechanical equipment on the roof. It was stated there would not be any mechanical equipment on either of the proposed fire station roofs. Discussion of the trash enclosure ensued with Mr. Machen reminding the designers that the enclosure must be designed and built in the same style as the building.

**The Design Checklist was reviewed with the following answers given by the DRB:**

1. Variation in facade provided visual interest - Yes
2. Modulate scale of building - Yes
3. Limit visual impact of blank walls and facades - Yes
4. Establish visually prominent ground floor facades - Yes

- 
5. Maintain pedestrian scale along facades - Yes
  6. Maintain pedestrian activities - Yes
  7. Reduce overall scale of building - Yes
  8. Encourage creation of public outdoor spaces – No; Gap in application. Mr. Hanson stated they would submit the landscape plan.
  9. Soften impact of built environment - Yes
  10. Compatible with community and neighborhood characteristics - Yes
  11. Minimize intrusiveness of signage - Yes
  12. Improve pedestrian environment - Yes
  13. Provide pedestrian access - Yes
  14. Provide weather protection for pedestrians - Yes
  15. Maintain smaller scale commercial buildings - Yes
  16. Reduce visual impact of parking areas - Yes

**Motion: I move conditional approval of application subject to submission of a final site plan that conforms to the new design as well as the landscaping plan that relates to that site plan based on their presentation on March 7, 2016.**

**Depew/Grutsche: Unanimous approval.**

#### **Bainbridge Island Fire Department Station 21 (PLN11791SPR/CUP)**

Mr. Hansen provided a review of the key element questions the DRB had during the pre-application presentation. He stated there was a reduction in the size of the building by one equipment bay. Some of the administration function was moved back to the first floor with the bulk staying on the second floor. Mr. Hansen also reiterated they would be using the same materials and design concepts as Fire Station 22 to maintain continuity between fire stations as well as provide recognition from the public. The private areas of the fire station were explored and locations of the gates were shown. Discussion ensued of the roofline over the southern part of the bays/service area. Arguments were presented for moving the “fire tower” with the identifying 21 to the other end of the bays from its current situation with numerous other locations for the “21” explored. Mr. Grainger asked Mr. Machen whether the site plan typically included the landscape plans. Mr. Machen stated it could be part of it, but was not necessarily part of the Design Guidelines. He also pointed out that the landscaping plans were part of the packet and that a presentation by the actual landscape architect had not been expected in the past. The DRB asked for them to come back with landscape information as they were going to do for Station 22.

#### **The Design Checklist was reviewed with the following answers given by the DRB:**

1. Variation in façade provided visual interest - Yes
2. Modulate scale of building - Yes
3. Limit visual impact of blank walls and facades - Left Open to see south elevation again
4. Establish visually prominent ground floor facades - Yes
5. Maintain pedestrian scale along facades - Left Open to see landscape plan
6. Maintain pedestrian activities - Yes
7. Reduce overall scale of building - Yes
8. Encourage creation of public outdoor spaces - Left Open to see landscape plan
9. Soften impact of built environment - Left Open to see landscape plan
10. Compatible with community and neighborhood characteristics - Yes
11. Minimize intrusiveness of signage - Yes
12. Improve pedestrian environment - N/A

- 
- 
13. Provide pedestrian access - Yes
  14. Provide weather protection for pedestrians - Yes
  15. Maintain smaller scale commercial buildings - Yes
  16. Reduce visual impact of parking areas - Yes

**Motion: I move we approve the design as shown with the contingency based on the presentation of the landscape plan by the landscape architect. Approval also contingent upon material requirements brought forth at next meeting (March 7, 2016).**

**McNett/Depew: Passed unanimously**

#### **Wyatt Cottages PLN50165 SPR**

Josh Machen provided an overview of the changes to the design that was seen at the pre-application review.

When the applicant entered, introductions were made around the table. There were two citizens/neighbors present as well as developer James Laughlin and architect Bruce Anderson. Mr. Anderson provided a review of the previous action during the pre-application phase as well as an overview of the current project mentioning that the "tot lot" park was no longer being moved. Mr. Laughlin mentioned that he held an open house at the Oliver house and invited all members of the public who left their e-mails during the pre-application phase to view the new plans which were well received. Mention was made of the two parking spaces the park district asked for the applicant to provide for the park. (BIMPRD actively discourages parking at the "tot lot.") Mr. Grainger asked about the large scale trees in the small scale landscaping. Mr. Anderson stated those would be going closer to the park. Lighting of entry ways as well as the driveway was discussed as "moon" light as opposed to "up" lighting. Mr. McNett asked whether covers for vehicle parking would be allowed. Mr. Laughlin stated if the owners all voted for that (the development would be a condominium) they could do that but it would be written into the CCRs. There was discussion with citizens (see attached sign-in sheet) regarding the acceptability of changes made to the site plan from the previous site plan submitted.

**Motion: I move acceptance of the revised site plan for the project.**

#### **Answers to Design Guidelines were:**

1. Parking lot visually unobtrusive – Yes
2. Open space and amenities – Yes
3. Pedestrian connection – Yes
4. Shielded lighting – Yes
5. Service areas screened – Yes
6. Common open space – Yes
7. Overall form – Very positive
8. Entrances from street clear – Yes
9. Mechanical equipment concealed – Yes
10. Structured parking – N/A
11. Varied details – Yes
12. Integrated signage – N/A



- 13. Creativity – Yes
- 14. Awning signs – N/A
- 15. Landscape front setbacks – Yes
- 16. Strong reference point to key intersections – Yes
- 17. Residential roof forms – Not following normal, but great solution

**Motion: I move acceptance of the revised site plan for the project.**

**Depew/McNett: Passed unanimously 4-0**

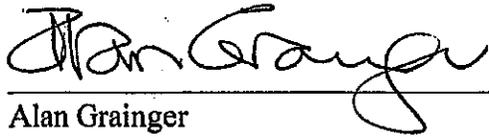
**New/Old Business**

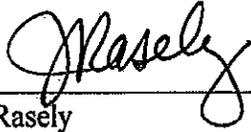
Mr. Grainger informed the other DRB members he would be their representative at the all-day interview process for the new Planning Director on February 23, 2016.

**Adjourn**

The meeting was adjourned at 6:00 PM.

Approved by:

  
\_\_\_\_\_  
Alan Grainger

  
\_\_\_\_\_  
Jane Rasely



CITY OF  
BAINBRIDGE ISLAND

**CITY OF BAINBRIDGE ISLAND  
DESIGN REVIEW BOARD - REGULAR MEETING  
February 22, 2016**

PLEASE PRINT

Join  
ListServ  
Yes/No

Name	Affiliation	Phone/ E-Mail	Join ListServ Yes/No
Chris Gutsche	DRB	chris@ecosmartsdesign.com	Y
JIM MCNETT	DRB		
Alan Grainger	DRB		
Ken Felton	DRB	v.felton@bainbridgeid.org	
Chuck Popow	DRB		
Brian Kalkb	Bainbridge Review	ed@bainbridgeid.org	Y
HANK TERED	FIRE	nteran@BIFD.ORG	
Janine Courtemanche	FIRE	flourtemanche@bifd.org	
BRETT HANSON	MACKENZIE	bhanson@mckenzie.com	
CHARLES DRINON	MACKENZIE	cdrinon@mckenzie.com	
BRUCE ANDERSON	CAA	BRUCE@CTEAL-ANDBSAI.COM	
CRAIG SPENCER	CITIZEN	cspencerstudio@gmail.com	
Margaret Celestina	WELLSBOR	stuartly4@gmail.com	
James Laughlin	Applicant	jmlaughlin32@gmail.com	

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Call to Order (Attendance, Agenda, Ethics)  
Approval of Minutes - January 4, 2016  
Jones House Site Plan Review (PLN50311SPR)  
Bainbridge Island Fire Department Station 21 (PLN11791SPR/CUP)  
Bainbridge Island Fire Department Station 22 (PLN14200SPR/CUP)  
New/Old Business  
Adjourn

---

**Call to Order (Attendance, Agenda, Ethics)**

Chair Alan Grainger called the meeting to order at 2:14 PM. Design Review Board (DRB) members in attendance were Jim McNett, Chuck Depew, Chris Gutsche and Jeffrey Boon. Susan Bergen and Peter Perry were absent and excused. Council Liaison Ron Peltier attended. City Staff present were Planning Manager Josh Machen, Senior Planner Heather Beckmann and Administrative Specialist Jane Rasely who monitored recording and prepared minutes. The agenda was reviewed without any conflicts reported.

**Approval of Minutes - January 4, 2016**

**Motion: I move to approve the minutes.**

**Gutsche/Depew: Passed unanimously**

**Jones House Site Plan Review (PLN50311SPR)**

Jon Thornburgh presented the newest information for the Jones House, one of the two houses his family owns on Ericksen Avenue. He passed out new plans stating the site plans contained in the agenda packet were out of date. Mr. Thornburgh answered the previous questions from the DRB showing pictures of mature trees and giving explanations as to the thought process that went into the site plan. Mr. Grainger engaged in a discussion of moving the staircase in order to facilitate door and dormer placement on the second floor. ADA parking was presented as well. The need for underground parking was canvassed and it was suggested that Mr. Thornburgh check with Project Manager Heather Beckmann about possibilities regarding the height and underground parking correlation and whether underground parking was necessary to accommodate the increased height of the building. Mr. McNett and Mr. Depew agreed it would have been nice to see actual pictures of the current building.

Discussion of the DRB's role in determining land use occurred between project reviews with the Assistance Dogs of Hawaii project used as an example.

Conversation about public buildings and how the requirements are different than for private projects led out of a preview of the landscape plans for BIFD Station 22.

---

Chris Gutsche excused himself from the meeting briefly at 3:10 PM.

**Bainbridge Island Fire Department Station 22 (PLN14200SPR/CUP)**

Mr. Brett Hansen introduced Jeff Bouma who spoke about the landscape site design. Mr. Bouma stated they had surveyed and inventoried every tree on both sites as well as updated the buffer diagrams. He said they were meeting the buffer requirements for both sites. He also stated they would be saving 134 out of 140 trees at Station 22. He furthered the discussion by relating the impetus for the plant palate citing an inspirational beautiful Japanese maple tree onsite. Mr. Depew asked about the difference in plants chosen for a rain garden as opposed to other areas on a site. Mr. Hansen spoke about the adjustments in parking due to the change in programming for the building. Removing some of the parking spaces allowed for increased landscaping. ADA parking and the trash closure were revisited. It was stated that full screen buffers were planned for the northern border of the property.

**Bainbridge Island Fire Department Station 21 (PLN11791SPR/CUP)**

Mr. Bouma gave the overview of the plant palate for Station 21 stating there were more evergreens in this palate to provide structure and year round color. He also stated that because of visibility from the road, as well as the big, long apparatus bay, the landscaping would be a little less natural until further out toward the edge of the property. The grouping of trees placed strategically against the large southern wall of the apparatus bays was described. Mr. McNett asked how big the trees would be when they were planted. Placement of trees, benches and perhaps a covered bench at the entrance were discussed. Mr. Gutsche asked what percentage of stormwater was being managed on the site. The answer was 100% was being handled through rain gardens. Mr. Hansen then presented the promised updates.

**Charles Schmid, Citizen** – Asked whether trees within the buffer along Highway 305 would be removed. Mr. Bouma stated none of them would be removed.

Mr. Grainger asked if there had been any thought to connecting the public transportation drop-off point on Highway 305 to the sidewalk on Madison. Mr. Hansen stated a walk lane would be provided by the fog lane.

**Motion: I move that the Fire Stations 21 and 22 have met the contingencies to their approval.**

**Gutsche/McNett: Unanimously approved.**

**New/Old Business**

There was extensive discussion about how to really save trees on properties being developed and how to prevent clear cutting. Tree and building clusters and the efficacy of clustering were questioned. Mr. Grainger wondered why new subdivision developments did not have to come to the DRB. Mr. Peltier asked the DRB if they felt they had the time to take on expanded duties.



**Design Review Board  
Regularly Scheduled Meeting Minutes  
Monday, March 7, 2016**

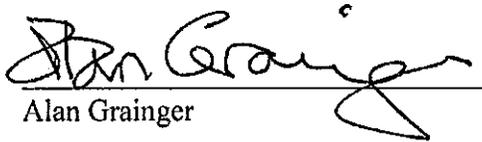
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He also asked them to look at the Land Use Element of the Comprehensive Plan and felt that might be a good place to expand the purview of the DRB. The DRB felt it would be helpful to have a work session with the new Planning Director when he arrives. Mr. Peltier thanked the DRB for their work and said he found their work interesting.

**Adjourn**

The meeting was adjourned at 4:56 PM.

Approved by:

  
Alan Grainger

  
Jane Rasely



# Bainbridge Island Fire Department

## Memo

January 30, 2016

TO: Josh Machen, Planning Department

FR: Assistant Chief Luke Carpenter, Fire Marshal

RE: BIFD Station 22

PLN14200 CUP

The submittal has been reviewed resulting in the following comments:

1. Proposed fire station must be equipped with fire sprinklers and fire alarms as per the adopted Code.
2. The project shall comply with the requirements of the adopted Fire Code and any applicable NFPA standards or other requirements as stipulated by the Fire Marshal.

**ATTACHMENT X**

APR 29  
~~JAN 15~~ 2016

Bainbridge Island Fire Department  
Station 21 and 22  
April 15, 2016  
Provided by Fischer Bouma Partnership

## **SPR Landscape Plan Supplemental Information**

This document is intended to supplement the Landscape drawings for Site Plan Review.

### **TREE & VEGETATION PROTECTION STRATEGIES**

Trees to be retained as shown on Tree Retention Plan will be protected as required using methods as described in BIMC 18.15.010 (below copied out of current code). These methods are also provided in the project details on Civil drawing sheet Nos. C2.0 and C5.1 and Specification Section 31 10 00.

#### 4. Protection during Construction and Development.

a. Intent. The intent of these regulations is to provide the best protection for significant trees and tree stands, including protection for trees on adjacent properties.

#### b. Requirements.

i. No cutting of significant trees shall be allowed on a site until the tree retention and planting plans have been approved by the director.

ii. In order to preserve future ecological function, the applicant shall identify areas of prohibited disturbance, generally corresponding to the dripline or critical root zone (as identified by a consulting arborist) of the significant trees and/or tree canopy of tree stands to be retained, buffers, areas of existing vegetation to be maintained, future rain gardens, and future planting areas larger than 400 square feet (i.e. landscape islands in parking lots). The prohibited disturbance areas shall be reviewed and approved by the director as part of the land use permit review process.

iii. A temporary five-foot-high chain link fence with tubular steel poles or "T" posts shall delineate the area of prohibited disturbance defined in subsection C.4.b.ii of this section, unless the director has approved the use of a four-foot-high plastic net fence as an



alternative. The fence shall be erected before construction starts and shall remain in place until construction has been completed, and shall at all times have affixed to it a sign indicating the protected area.

iv. No impervious surfaces, fill, excavation, vehicle operations, compaction, removal of native soil or storage of construction materials shall be permitted within the area defined by the required construction fencing. If avoiding construction and compaction in future planting areas is unavoidable, the landscape plan for the project shall include methods for aerating and/or augmenting compacted soil to prepare for new planting, pursuant to subsection H.2 of this section.

v. A rock well shall be constructed if the grade level around the tree is to be raised more than one foot. The inside diameter of the well shall be equal to the diameter of the dripline or critical root zone (as identified by a consulting arborist) of the tree or tree canopy of tree stands.

vi. The grade level shall not be lowered within the larger of (A) the dripline or critical root zone (as identified by a consulting arborist) of the tree, or the tree canopy of tree stands, or (B) the area recommended by a consulting arborist.

vii. Alternative protection methods may be used if recommended by a consulting arborist and determined by the director to provide equal or greater tree protection.

viii. Wherever this subsection C.4 allows or requires the involvement of a consulting arborist, that individual shall be selected from the city's list of current arborists certified by the American Society of Consulting Arborists and his or her services shall be paid for by the applicant.

## **VEGETATION CLEARING STRATEGIES**

Clearing of vegetation is controlled by Specification Section 31 10 00 Site Clearing being provided by the Civil Engineer.

Vegetation will be cleared by typical mechanical means in areas proposed for development. The contractor will be required to protect and preserve trees and vegetation which are outside the clearing limits or



indicated to be protected as indicated on the plans. Trees, underbrush, and all vegetation indicated to be cleared will be done in a manner to protect adjacent property and items on-site to remain.

### **TOPSOIL PROTECTION & RE-USE STRATEGIES**

Topsoil protection and re-use are controlled by Specification Sections 31 10 00 Site Clearing being provided by the Civil Engineer and Specification Section 32 29 00 Landscape Planting provided by the landscape architect, respectively.

Topsoil to remain in place will be protected through vegetation protection strategies and temporary erosion control measures implemented during construction. Native topsoil in areas to be developed will be removed to its full depth and stockpiled on site. All stockpiled topsoil will be covered. Stockpiles will be constructed in accordance with FWRC and WAC requirements. Side slopes will be appropriate for the material to prevent sloughing, erosion, or instability. Stockpiled soil should be screened so that it is free of clay clumps, rubble, cobbles, rubble or any other material that may limit plant growth. One hundred percent of the planting soil should pass through a ¾" sieve. See the following section for re-use strategies of amended (if necessary) native topsoil that has been stockpiled.

### **NATIVE SOIL AMMENDMENT STRATEGIES**

Native soil amendment strategy is controlled by Specification Section 32 29 00 Landscape Planting.

Native soil in areas to be developed will be stripped and stockpiled per the section above. On-site soils fall in two categories: 1) those stripped and stockpiled and 2) those that were undisturbed (where final grades match existing grades) and that will have enhancement plantings added to them. Both stockpiled and undisturbed native topsoil will then be tested to determine the necessary amendments. Based upon the test results, state recommendations for soil treatments and soil amendments are to be incorporated to produce satisfactory planting soils suitable for health, viable plants. Organic content for the soil shall also be tested and the soil amended so that it is 3-8% by weight per LOI.

Prior to placing a given depth of amended topsoil, the subgrade will be disturbed/loosened to a determined depth by scarification, discing, or ripping to enable a rototiller to fully incorporate a compost amendment. The amended topsoil will then be placed on the prepared subgrade to a depth determined by analysis of the existing soils on site. Due to the typical mineral composition of the glacial till soils in this region, excavated areas to be planted with trees will typically require a 12" to 18" depth of amended topsoil placed. Areas of shrub, groundcover, and turf planting will require less of a depth of amended topsoil placement.

### **PLANTING TIMES**

Planting time is controlled by Specification Section 32 29 00 Landscape Planting.

Planting should be performed between September 1 and May 31. Planting should not occur outside of this window or when the temperature is below 32 degrees F or above 80 degrees F. Planting should not occur when the soil is completely saturated or when the wind velocity is greater than 25 mph.



## **IRRIGATION**

Irrigation is controlled by Specification Section 32 81 00 Landscape Irrigation. New landscape will be irrigated temporarily for establishment or permanently depending on location and typology.

APR 29  
JAN 15 2016

Planning and  
Community Development



Katy Bigelow  
206.351.1375  
arboristkaty@gmail.com

April 19, 2016

ATTN: Jeff Bouma  
Fischer Bouma Partnership  
310 Madison Avenue South, Suite A  
Bainbridge Island, WA 98110

Chief Luke Carpenter at:  
Bainbridge Island Fire Department  
8895 Madison Ave.  
Bainbridge Island, WA 98110

Dear Mr. Bouma and Mr. Carpenter:

Thank you for having me evaluate trees at the redeveloping Fire Station 22 on Bainbridge Island. To evaluate the trees addressed in this memo I combined my field experience and education with current accepted practices as defined by the American National Standards Institute (ANSI) and the International Society of Arboriculture (ISA).

The tools I use to make an assessment are limited to a rubber mallet, binoculars, compass, laser pointer, hand brush, shovel and hand trowel unless otherwise noted. A visual tree assessment and other methods are only conclusive for the day of inspection and do not guarantee that conditions will remain the same in the future.

-----

I was asked by Mr. Bouma and Mr. Carpenter to assess significant trees in and near the buffer areas of the proposed redevelopment of the Fire Station 22 property. I was specifically asked to determine the health of the trees, if they were reasonable candidates to retain during construction that would retain vigor after construction, and to determine their drip lines corresponding to where tree protection would need to be installed before the project began. I completed my assessment on April 13, 2016.

I evaluated 21 trees growing in the buffer areas of the site. Two of the trees I evaluated (139a and 133a) were not located on the original survey but are noted on the Site Plan.

One of the trees I evaluated was in poor condition and should be removed (139a). The other trees are in fair or good condition and will likely stay viable with tree protection installed prior to construction after the project is complete. Refer to the Supporting Data for specific information on each tree.

**ATTACHMENT Z**

One mature Western red cedar growing on the southwest project corner is very close to an area slated to be cleared and graded. While cedars can tolerate some root disturbance during development activities, I strongly recommend that if this tree is retained that as much tree protection extending out from the tree trunk be extended to the east and north as possible. If roots are encountered while clearing and grading is occurring, roots smaller than five inches in diameter can be cleanly cut back to the project area with a sharp saw. If several larger than five inch diameter roots are encountered, the tree may lose stability and vigor. Do not leave exposed roots ripped or otherwise damaged.

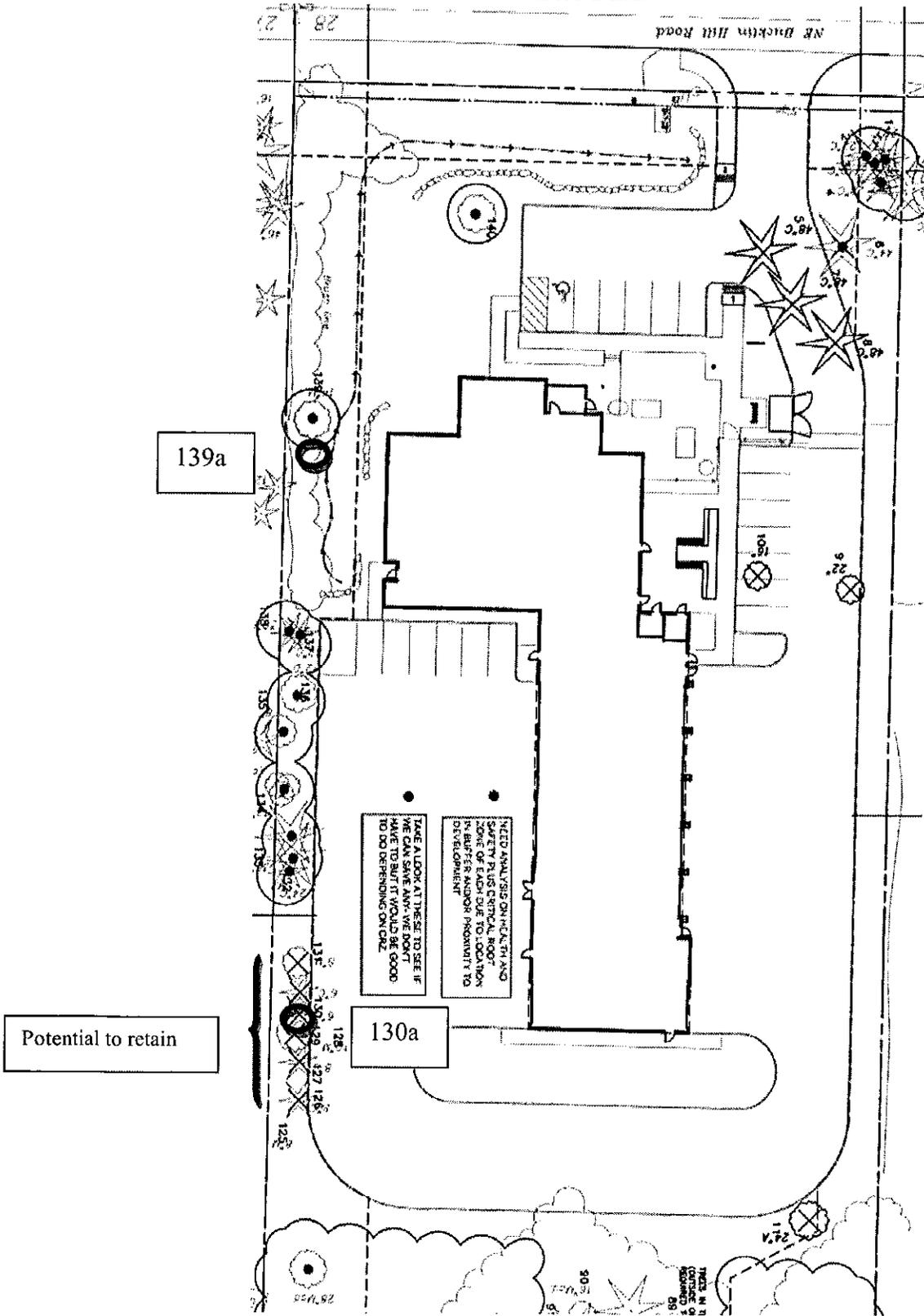
When possible, use tree protection fencing to surround trees close to each other instead of encircling individual trees. Buffer areas can be fenced off for tree protection at clearing boundaries. I strongly recommend installing tree protection fencing prior to the start of any clearing and grading activities.

Thank you very much for calling me for your arboricultural concerns.



*Katy Bigelow*  
*PNW ISA member # PN-3069AT*  
*PNW Certified Tree Risk Assessor # 199*  
*Registered Consulting Arborist® #490*

### Site Plan



### Supporting Data

TREE #	SPECIES	DBH	DRIP LINE	CURRENT HEALTH RATING	COMMENTS/ RECOMMENDATIONS
1	Western red cedar/ <i>Thuja plicata</i>	20.0"	*	Good	*Tree protection: N- into buffer, S into buffer, E 15', W to property line
2	Western red cedar/ <i>Thuja plicata</i>	21.0"	*	Good	*Tree protection: N- into buffer, S into buffer, E 15', W to property line
3	Western red cedar/ <i>Thuja plicata</i>	38.0"	*	Good	*Tree protection: N- into buffer, S into buffer, E 15', W to property line
4	Western red cedar/ <i>Thuja plicata</i>	19.0"	*	Good	*Tree protection: N- into buffer, S into buffer, E 15', W to property line
6	Western red cedar/ <i>Thuja plicata</i>	44.0"	*	Good	*Tree protection: at least seven feet to the east, into the buffer remaining sides.
140	Japanese maple/ <i>Acer palmatum</i>	14"	15' all directions	Fair	Tree has been heavily pruned in past - do not remove shrubs around base if possible. Install tree protection at existing curb on south and east sides.
139	Elm species/ <i>Ulmus species</i>	15.0"	W- 10'	Fair	Trunk bowed to north.
139a	Maple species/ <i>Acer species</i>	14.0"	-	Fair	Open wound at trunk base with rot. Not a good candidate for retention. Remove (not on survey).
138	Western red cedar/ <i>Thuja plicata</i>	30.0"	W - 6'	Fair	Open wound at 3'
137	Douglas-fir/ <i>Pseudotsuga menziesii</i>	30.0"	W - 10'	Fair	Retain
136	Scouler's willow/ <i>Salix scouleriana</i>	10.0"	-	Poor	Remove
135	Scouler's willow/ <i>Salix scouleriana</i>	6.0"	-	Poor	Remove

134	Scouler's willow/ <i>Salix scouleriana</i>	6", 7"	-	Poor	Remove
133	Douglas-fir/ <i>Pseudotsuga menziesii</i>	42.0"	W to existing asphalt	Good	Retain
133a	Western red cedar/ <i>Thuja plicata</i>	33.0"	W to existing asphalt	Good	Retain
132	Western red cedar/ <i>Thuja plicata</i>	27.0"	W to existing asphalt	Good	Retain
124	Pacific madrone/ <i>Arbutus menziesii</i>	12", 17"	10' all directions	Dying	Retain. Do not prune off dead trunk.
126	Yellow arborvitae/ <i>Thuja occidentalis</i>	6"	4' all directions	Good	Considered a large shrub. Potential to retain.
127	Yellow arborvitae/ <i>Thuja occidentalis</i>	6"	4' all directions	Good	Considered a large shrub. Potential to retain.
130	Yellow arborvitae/ <i>Thuja occidentalis</i>	6"	4' all directions	Good	Considered a large shrub. Potential to retain.
130a	Yellow arborvitae/ <i>Thuja occidentalis</i>	6"	4' all directions	Good	Considered a large shrub. Potential to retain.
<b>LEGEND</b>					
Green font = large shrubs, not trees.					
<b>Tree #:</b> Tree number corresponding with surveyed map number					
<b>Species:</b> Common and Latin tree name					
<b>DBH:</b> Trunk diameter(s) at 4.5' above average ground level.					
<b>Drip Line:</b> A horizontal area equal to the maximum extent of branches and leaves in a certain direction. Distance from trunk at which to place tree protection fencing.					
<b>Current Health Rating:</b> A description of general health ranging from dead, dying, hazard, poor, suppressed, fair, good, very good, to excellent.					
<b>Comments/Recommendations:</b> Comments relative to the recommendations concerning the tree being a good candidate for retention.					

## Assumptions, Limiting Conditions and General Waiver

I, Katy Bigelow, certify that:

I have personally inspected the tree(s) and or the property referred to in this report;

I have no current or prospective financial or other interest in the vegetation or the property which is the subject of this report and have no personal interest or bias in favor of or against any of the involved parties or their respective position(s), if any;

The analysis, opinions and conclusions stated herein are the product of my independent professional judgment and based on current scientific procedures and facts, and the foregoing report was prepared according to commercially reasonable and generally accepted arboricultural standards and practices for the Pacific Northwest and Puget Sound areas;

The information included in this report covers only those trees that were examined and reflects the condition of the trees as of the time and date of inspection;

This report and the opinions expressed herein are not intended, nor should they be construed, as any type of warranty or guarantee regarding the condition of the subject trees in the future;

Covenants, Conditions, and Restrictions (“CC&Rs”) may restrict the number, type and height of vegetation on the subject property, and I have made no investigation regarding whether the property is subject to such CC&Rs; and

To the best of my knowledge and belief, all statements and information in this report are true and correct and information provided by others is assumed to be true and correct.

I am not an attorney or engineer. This report does not cover these areas of expertise and represents advice only of arboricultural nature. Without limiting the generality of the preceding sentence, it is specifically understood that nothing contained in this report is intended as legal advice, or advice or opinions regarding soil stability or zoning laws, and this report should not be relied upon to take the place of such advice.



*Katy Bigelow*  
*PNW ISA member # PN-3069AT*  
*PNW Certified Tree Risk Assessor # 199*  
*Registered Consulting Arborist® #490*

# Memorandum

## **Bainbridge Island Fire Department Project - Tree Valuation**

This memo is in response to City review of the CU/SPR submittal and associated comments dated March 9, 2016 for both Stations 21 and 22.

Comment #2 in each response letter requires all Landscaping Plan submittal requirements as outlined in the Bainbridge Island Administrative Manual. Part 2.D.1.iv in the manual requires that a tree valuation be completed and submitted at this stage for all trees to be retained.

We contend that this requirement is excessive and an onerous burden to the Fire Department. This opinion was shared at the City Council meeting 3/22/2016 and they directed the Tree Committee to take another look at this requirement and revise. Jeff Bouma attended the Tree Committee meeting on 3/29/2016 and it appears that this requirement will change in some manner.

As such, we are requesting that this submittal requirement be delayed due to lack of clarity and likelihood of change. Instead of being a condition of the SPR submittal process, we propose to provide the valuation at a later date but prior to issuance of any building permit and the start of construction.

# MACKENZIE.

DESIGN DRIVEN | CLIENT FOCUSED

October 17, 2016

City of Bainbridge Island  
Attention: Janelle Hitch  
280 Madison Avenue N.  
Bainbridge Island, WA 98110

Re: **Bainbridge Island Fire Department - Station #22**  
*Parking Analysis Letter*  
Project Number 2150124.00

Dear Ms. Hitch:

Mackenzie has prepared this letter to satisfy City of Bainbridge parking requirements for the subject fire station application.

The 14,333-square-foot fire station is proposed at 7934 Bucklin Hill Road NE in Bainbridge Island, Washington. The proposed fire station will replace an existing 4,850-square-foot fire station at the same site. The existing fire station has 19 striped parking spaces.

The proposed fire station's site plan presents 19 striped parking spaces. The fire station will not provide a community room or other facilities for public use, so parking needs will be based on the fire station operation only. Table 18.15-020-1 of the City's Development Code does not include a specific minimum parking requirement for fire stations, but rather requires an adequate number of parking spaces to accommodate peak shift employees for governmental facilities. The proposed fire station will have eight (8) total employees over various shifts. As a worst-case scenario, it is assumed that all employee parking will need to be accommodated during shift changes, resulting in a peak demand of eight (8) parking spaces on-site. Emergency vehicles will not require passenger vehicle parking spaces since those vehicles will be stored in the apparatus bays.

It is anticipated the proposed 19 striped parking spaces will far exceed the minimum 8-parking space requirement, allowing for additional parking if needed.

Please let us know if you have any questions or comments regarding the information in this letter.

Sincerely,



Brent Ahrend  
Senior Associate | Traffic Engineer

Enclosure(s): Site Plan

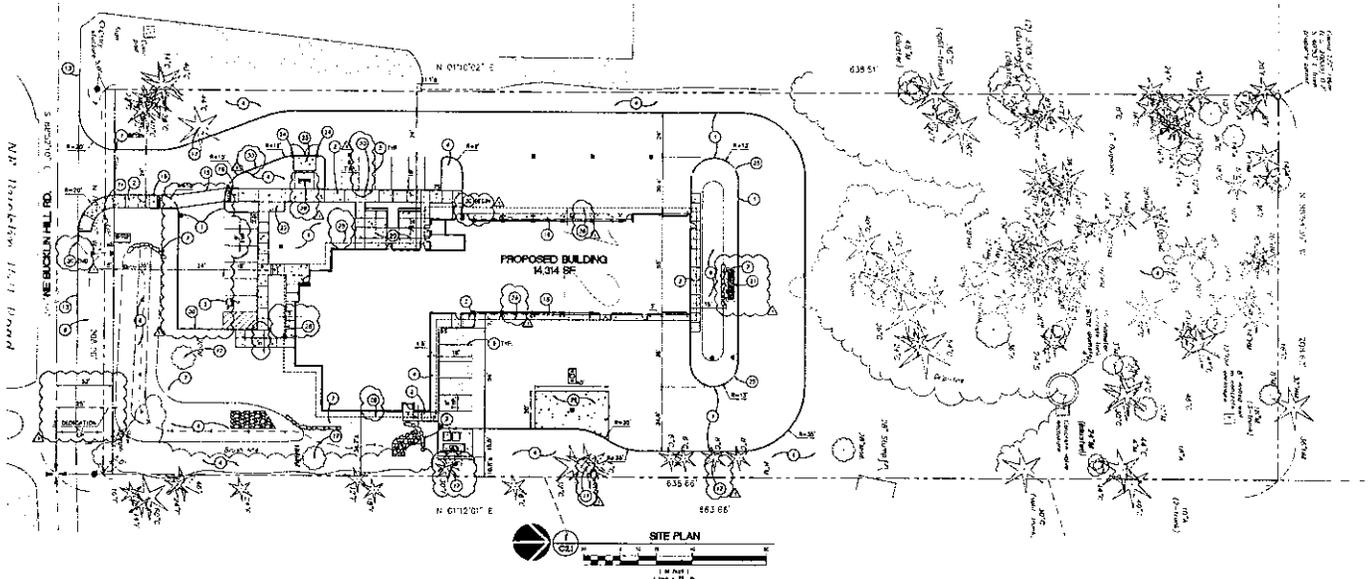
c: Michael Chen, Brett Hanson, Janet Jones - Mackenzie



P 503.224.9560 • F 503.224.9560  
ARCHITECTURE • INTERIORS • STRUCTURE  
Portland, Oregon • Vancouver, V

E Water Avenue, #100, Portland, OR 97214  
• TRANSPORTATION PLANNING • LANDSCAPE ARCHITECTURE

**ATTACHMENT AA**



**GENERAL NOTES**

1. ALL WORK SHALL COMPLY TO THE STANDARD SPECIFICATIONS AND THE REQUIREMENTS OF THE CITY OF BANBRIDGE PLANS AND THE CURRENT AMERICAN PUBLIC WORKS OBSERVATION STANDARDS FOR PUBLIC WORKS CONSTRUCTION.
2. THE WORKING CONDITIONS ARE GENERALLY UNFAVORABLE. THEY DO NOT HAVE ANY EFFECT, BECAUSE OF EROSION REQUIRED FOR INSTALLATION BY THE GRADE WORK. THEY DO NOT HAVE ANY OBVIOUS CONSTRUCTION BECAUSE OF THE LIMITED ACCESS TO THE SITE. TO COMPLETE THE PROJECT ALL LOCATIONS FOR WORK SHALL BE DETERMINED AND DOCUMENTED WITH PHOTOGRAPHS AND FIELD NOTES BEFORE ANY CONSTRUCTION. EXISTING UNDERGROUND UTILITIES SHALL BE LOCATED AND MARKED BY THE CONTRACTOR. ALL UTILITIES SHALL BE PROTECTED BY CONSTRUCTION. PROTECTIVE SHIELDING SHALL BE INSTALLED WITH APPROXIMATELY 18" MINIMUM CLEARANCE FROM EXISTING UTILITIES. ALL UTILITIES SHALL BE PROTECTED BY CONSTRUCTION. PROTECTIVE SHIELDING SHALL BE INSTALLED WITH APPROXIMATELY 18" MINIMUM CLEARANCE FROM EXISTING UTILITIES. ALL UTILITIES SHALL BE PROTECTED BY CONSTRUCTION. PROTECTIVE SHIELDING SHALL BE INSTALLED WITH APPROXIMATELY 18" MINIMUM CLEARANCE FROM EXISTING UTILITIES.
3. EROSION CONTROL MEASURES AND SEDIMENT CONTROL IS REQUIRED. EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED TO PREVENT EROSION AND SEDIMENTATION. THE EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED TO PREVENT EROSION AND SEDIMENTATION. THE EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED TO PREVENT EROSION AND SEDIMENTATION.
4. EXISTING EROSION CONTROL IS REQUIRED. EROSION SHALL BE CONTROLLED THROUGH THE WORK AREA AND SHALL BE MAINTAINED TO PREVENT EROSION AND SEDIMENTATION. THE EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED TO PREVENT EROSION AND SEDIMENTATION. THE EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED TO PREVENT EROSION AND SEDIMENTATION.
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6. EXISTING EROSION CONTROL IS REQUIRED. EROSION SHALL BE CONTROLLED THROUGH THE WORK AREA AND SHALL BE MAINTAINED TO PREVENT EROSION AND SEDIMENTATION. THE EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED TO PREVENT EROSION AND SEDIMENTATION. THE EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED TO PREVENT EROSION AND SEDIMENTATION.
7. BEFORE CONSTRUCTION IS AN ORDERED BY THE CITY OF BANBRIDGE. THE CONTRACTOR SHALL OBTAIN THE ORDER OF THE CITY OF BANBRIDGE. THE CONTRACTOR SHALL OBTAIN THE ORDER OF THE CITY OF BANBRIDGE. THE CONTRACTOR SHALL OBTAIN THE ORDER OF THE CITY OF BANBRIDGE.
8. REQUEST BY THE CONTRACTOR FOR CHANGES TO THE PLANS MUST BE APPROVED BY THE ENGINEER.

**CURB NOTE**

ALL CURB RISES TO BE 2.5 UNITS OVERSIDE NOTES

**KEYNOTES**

1. ENTRANCED CONCRETE CURB PER DETAIL 11/20/10
2. CONCRETE SIDEWALK PER DETAIL 11/20/10
3. 4" MIN ACCESSIBLE PARKING SPACES PER DETAIL 11/20/10
4. LANDSCAPE AREA SEE LANDSCAPE PLANS
5. 4" WIDE WALKWAY PARKING STRIPS
6. STORMWATER FACILITY SEE GRADING PLAN
7. ROOF WALL SEE GRADING PLAN
8. TEMPORARY FENCELINES
9. FUTURE 4" PAVED SHOULDER CONSTRUCTION BY CITY OF BANBRIDGE (PLAN)
10. DRIVE WAY STRIPS
11. EXISTING POWER POLE 15' MINIMUM PROTECT DUMPING CONSTRUCTION
12. CONCRETE RETAIN WALL SEE GRADING PLAN
13. EDGE OF ASPHALT
14. TRANSFORMER
15. 12" WIDE CROWNWAY STRIPS
16. SIDEWALK RAMP PER DETAIL 11/20/10 AND 11/20/10
17. EXISTING TREE TO REMAIN. PROTECT DUMPING CONSTRUCTION
18. CONCRETE APPROX. SEE PAVING LEGEND
19. CONCRETE FLOOR SEE GRADING PLAN FOR DETAILS
20. PROPOSED TRAIL WITH 2" MINIMUM CONCRETE FILL CONCRETE REQUIRED SEE WITH OTHER NOTES
21. TRAIL ENCLASURE
22. 4" WIDE BOLLARD PER DETAIL 11/20/10
23. 2" HOLE CURB BREAK PER DETAIL 11/20/10
24. CONCRETE WALKWAY
25. ACCESSIBLE ENTRANCE PER DETAIL 11/20/10
26. ROOF OVERHANG
27. SIDE PARKING
28. SIDEWALK RAMP PER DETAIL 11/20/10

**SITE DATA**

GROSS SITE AREA	133,000 SF (3,066 AC)
NET SITE AREA	132,000 SF (3,030 AC)
P&W FOOTPRINT	10,000 SF
BUILDING FOOTPRINT	10,000 SF
LANDSCAPE AREA	10,000 SF (0.23 AC)

**PARKING DATA**

PUBLIC REQUIRED	18 SPACES
PRIVATE	3 SPACES
TOTAL	21 SPACES (2,100 SQ FT)

**LEGEND**

CONCRETE SIDEWALK CURB	
PROPERTY LINE	
ROOF RETURN WALL	
LANDSCAPE	
4" WIDE WALKWAY	
EXISTING TREE TO REMAIN	

**PAVEMENT LEGEND**

SEE SPECIFICATIONS FOR ADDITIONAL PAVEMENT DETAILS	
4" ASPHALT	
4" AC OVER 4" ROAD BASE	
CONCRETE APPROX	
4" CONCRETE (1,000 PSI) OVER 4" ROAD BASE	

<p>MACKENZIE ENGINEERING 1000 1/2 ST SW PORTLAND, OR 97204</p>	<p>LANDSCAPE PROJECT: BANBRIDGE ISLAND FIRE DEPT STATION 22 2024/07/24 BANBRIDGE ISLAND, WA 98110</p>	<p><b>M.</b> www.mackenzie.com</p>	<p>Architecture + Interiors + Planning + Engineering</p>	<p>BANBRIDGE ISLAND FIRE DEPT DEPARTMENT 8885 MADISON AVE. BANBRIDGE, WA 98110</p>	<p>BANBRIDGE ISLAND FIRE DEPT STATION 22 7934 BUCKLIN HILL RD. NE</p>	<p>DATE: 10/07/2018 DRAWN BY: JTS CHECKED BY: TSW</p>	<p><b>SITE PLAN</b></p>	<p>REVISION 10/07/2018 2150124.00</p>	<p>PERMIT SET 06/30/2016</p>
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## CHAPTER 3 OPERATIONS AND MOBILITY



This chapter describes the traffic operations and current and future vehicle mobility for the City's roadway system. Mobility is the measure of how well vehicles can get around on the roadway system – the opposite of congestion. Island residents expect a high level of mobility that maintain the character of their community. The high levels of congestion experienced during peak periods, especially on and around SR 305, area common source of frustration for drivers.

While the focus of this chapter is on motorized level of service, it is recognized that providing for level of service for all modes is important for a viable transportation system. In some locations where constraints limit options, some modes may be favored over other modes. Transportation networks should provide for all modes of transportation as a system. For vehicular traffic transportation demand strategies may be an optimal approach.

Level of service standards are used to provide a basis for the mobility analysis. This Plan used planning and operational models developed by Transportation Solutions, Inc. in TransCAD and Synchro software, respectively, to analyze current conditions (based on traffic counts and existing roadway network information) and to forecast future levels of service (based on traffic generated by forecasted land use and roadway network changes). The structure of the roadway network was analyzed by reviewing the roadway classification system, connectivity, access, and road standards.

### Existing Roadway System

The section on existing conditions provides an analysis of the current operating conditions and provides a baseline for future comparisons. The City of Bainbridge Island's transportation system is made up of a network of roadways, pedestrian facilities, bikeways, the ferry terminal, and formal and informal trails. Each of these elements is important to the mobility or movement of people and goods within and to destinations beyond the Island. This chapter focuses on the roadway system only; the non-motorized, bus transit, and ferry systems are described in Chapters 6 & 7.



The roadway system is designed for the movement of people and goods throughout the community. Major regional transportation features of the Island include the Washington State Ferry Terminal, which connects Bainbridge Island to downtown Seattle; and State Route 305, which connects the Island to the Kitsap and Olympic Peninsula. State Route 305 is the Island's principal transportation corridor, providing an important north-south connection.



The State system is supported by a City roadway system that connects residential areas to the highway and retail and employment areas. The City's arterial, collector, and residential street system provides roadway connections and access to properties within the City.

### ***Travel Corridors***

The following important commuter, shopping, business, school, and freight/commercial corridors are identified for the Island:

- *Commuter Corridors* – SR 305, Winslow Way, Wyatt Way, Ferncliff Avenue, High School Road, Day Road, Blakely Avenue, Eagle Harbor Drive, Baker Hill Road, Miller Road, and North Madison Avenue.
- *Retail Corridors* – SR 305, Winslow Way, High School Road, Madison Avenue, Hildebrand , Wyatt Way, Lynwood Center Road, and Valley Road.
- *School Corridors* – High School Road, New Brooklyn Road, Sportsman's Club Road, Madison Avenue, Day Road, North Madison Avenue, and Blakely Avenue
- *Freight Corridors* – SR 305, Day Road, Miller Road, Fletcher Bay Road, Sportsman's Club Road, High School Road, Madison Avenue, and Winslow Way.

### ***Roadway Inventory***

The City's roadway system consists of approximately 140 miles of paved roads, and another 20 miles of unpaved roads. The City maintains a Geographic Information System (GIS) that includes the roadway system. The GIS database includes characteristics for each roadway segment, including length, pavement width, functional classification, posted speed, sidewalks, and transit and bicycle facilities. A spreadsheet is maintained that includes sign inventory information. The City periodically conducts an island-wide traffic count and develops volume and traffic speed information for its major roadways. This Plan was updated in 2014 with TSI traffic counts.

### ***Roadway Classifications***

Roadway functional classification is defined as the process by which streets and highways are grouped into classes, or systems, according to the character of traffic service that they are intended to provide. The City divides Island roadways into four functional classifications:



principal arterial, secondary arterial, collector, and local access roads. These classifications are described in Table 3-1.

**Table 3-1. Functional Classifications**

<b>Classification</b>	<b>Definition</b>
Principal Arterial	Carry the highest levels of traffic in the system at the greatest speed for the longest uninterrupted distance, often with some degree of access control. Used for through trips, and provide connections within the system.
Secondary Arterial	Carry high level of traffic at a moderate speed, sometimes for through trips. Often serve as access to high-intensity land uses such as major employers or larger commercial centers; provide connections within the system.
Collector	Connect traffic from residential roads to arterials at a lower speed, carrying lower levels of traffic than arterials. Serve neighborhood centers.
Local Access	Carry low levels of traffic at low speeds. Serve as access to residential and commercial areas and are not used for through trips.

Streets and highways are assigned one of these classes, depending on the character of the traffic (i.e., local or long distance) and the degree of land access that they allow. Typically, a trip will use a combination of different road classes, with each classification having a specific function with regard to access and travel speed. Arterials provide a high degree of mobility and less access, while local access roads provide a high level of access and less mobility. Collectors provide a balance between access and mobility and connect the system.

The following changes to roadway classifications since 2004 are included in this update to the IWTP: Halls Hill Road from Blakely Hill to Rockaway Bluff from Local Access to Collector, Wallace Way from Madison Avenue to Ericksen Avenue from Local Access to Collector, and Upper Farms Road from Collector to Local Access.

### **Road Standards**

The City of Bainbridge Island has established its roadway street and design standards as part of its *Engineering Design and Construction Standards and Specifications*. These standards set the minimum requirements for constructing roadways and are applicable to all new roadway construction and modifications to existing roadways within the City of Bainbridge Island. The road and street design standards follow the functional classification system described above and establish separate standards for designated centers and the conservation area of the Island.

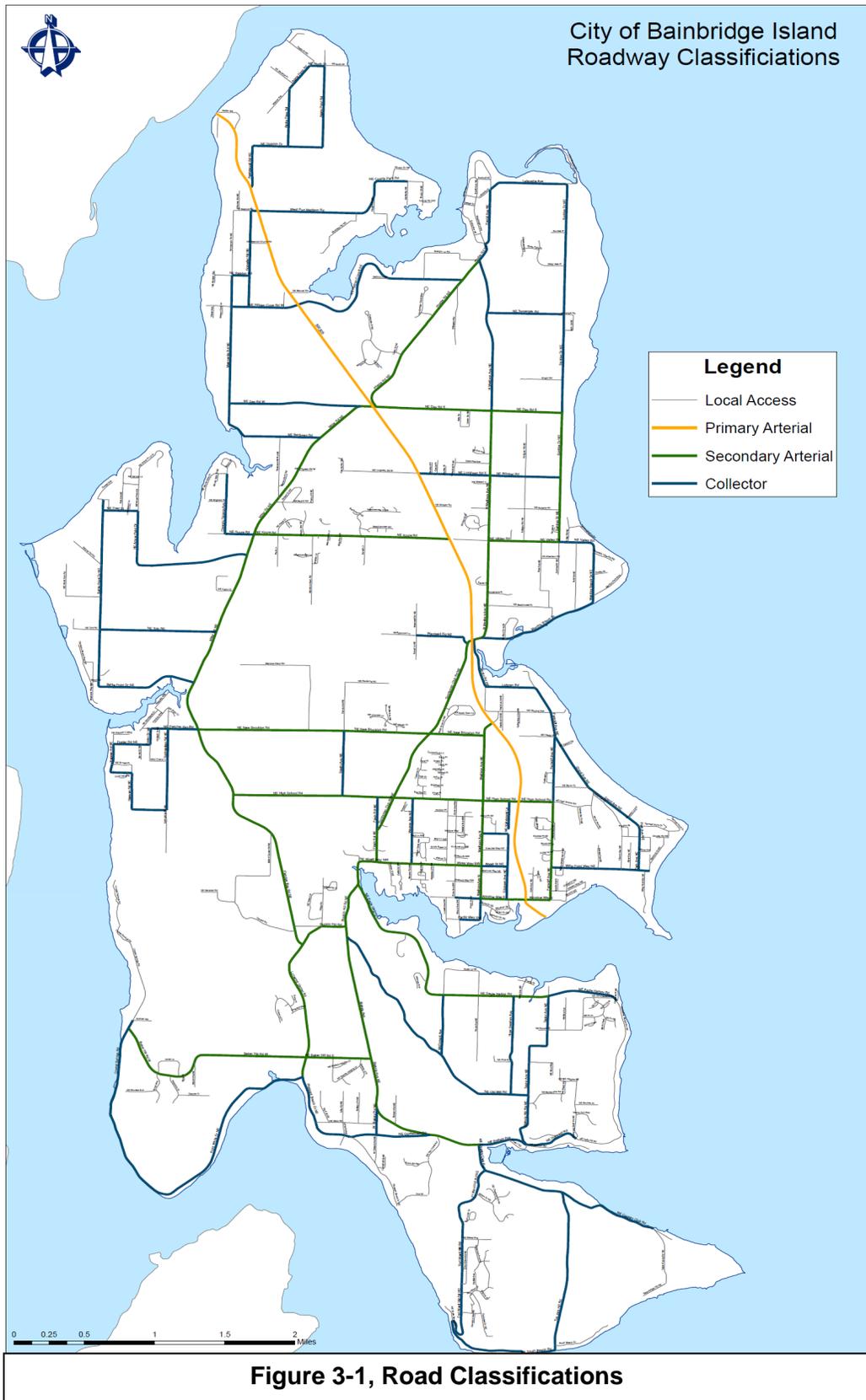
The City has both urban and suburban standards. Urban standards are intended to apply within the designated center of Winslow, the neighborhood centers including Lynwood, Island Center, and Rolling Bay, and the Day Road industrial Center. Urban standards apply in all locations with R2.9 and greater zoning and/or effective density. The City may require urban standards to be applied in other areas in close proximity for system continuity.



The roadway standards were created in 1997 and an update is needed to better address non-motorized elements and low impact development. The roadway standards should be updated within two years of the adoption of the IWTP.

## **Level of Service**

This section describes the Level of Service (LOS) standards used in this document. LOS provides a method for measuring the performance of the transportation system. The City uses a standard for LOS that determines if adequate mobility is being provided on the roadway system. LOS standards and method of measurement have been coordinated with Washington State Department of Transportation, Washington State Ferries, Kitsap County, and Kitsap Transit to ensure that standards used in this document are consistent, with these other entities.





**LOS Defined**

LOS is a measurement used in transportation planning to assess the operating performance of the transportation system. For roadways, LOS measures the degree of traffic congestion along a roadway varying from LOS A (free-flow traffic with minimal delays) to LOS F (highly-constrained traffic with long delays).

The Highway Capacity Manual (HCM) (Transportation Research Board, Special Report 209) establishes quantitative methodologies for determining level of service for differing types of facilities. The methodologies vary for intersections, roadways, freeway, and highway, but all follow the LOS A - F classification and provide a consistent method of measuring the performance of the transportation system. Table 3-2 describes the operation of the transportation system at each LOS ranking.

**Table 3-2: Level of Service Descriptions**

Level of Service	Description
LOS A	Free flow traffic conditions with very low delay at intersections.
LOS B	Reasonably unimpeded traffic operations with only short traffic delays at intersections.
LOS C	Stable operating conditions with average traffic delays at intersections
LOS D	Operating conditions result in lower travel speeds and higher delays at intersections.
LOS E	Travel speeds are substantially restricted with problems likely to occur at intersections.
LOS F	Roadway operations are over capacity with extreme delays likely at intersections.

LOS is measured differently for roadways and intersections. For roadways, LOS is measured as a function of traffic volume and roadway capacity. For intersections, LOS is measured as a function of vehicle delay in clearing the intersection.

**Roadway LOS Measurement**

Roadway LOS is measured by the relationship between traffic volume (V) and capacity (C) of the roadway. As the volume of traffic using the roadway approaches the capacity of the roadway (V/C approaching 1.0), the level of service deteriorates. Table 3-3 relates volume/capacity to LOS measurements for roadways.



**Table 3-3. Roadway Level of Service and Volume/Capacity Ratio**

<i>LOS</i>	<i>Volume/Capacity (V/C) Ratio</i>
<i>A</i>	<i>Less than 0.6</i>
<i>B</i>	<i>0.60 to less than 0.70</i>
<i>C</i>	<i>0.70 to less than 0.80</i>
<i>D</i>	<i>0.80 to less than 0.90</i>
<i>E</i>	<i>0.90 to less than 1.00</i>
<i>F</i>	<i>More than 1.00</i>

Traffic volumes can be counted or they can be calculated using the traffic model by analyzing land uses that are served by the roadway. Bainbridge Island’s roadway capacity policy is defined in the City Design and Construction Standards; see Table 3-4. No policy is currently defined for arterial roadway capacity. There is some inconsistency between the City’s current capacity policy and an engineering-based approach to roadway capacity calculation which would typically consider the physical structure of the roadway, including the number of lanes, type of intersection controls, widths of lanes and shoulders, and design speed. The City’s capacity standards will be reviewed and updated during the roadway design standard update process.

The roadway levels of service described in this Plan are based upon current capacity policy. In lieu of an arterial capacity policy, this Plan calculated arterial segment LOS based on an approach which is currently used by the City of Sammamish and which is consistent with the state of engineering practice.

**Table 3-4. Existing Roadway Capacity Policy**

<i>Functional Classification</i>	<i>Area Type</i>	<i>Capacity (*)</i>
<i>Secondary Arterial</i>	<i>Urban</i>	<i>&gt; 3,000</i>
<i>Secondary Arterial</i>	<i>Suburban</i>	<i>&gt;2,000</i>
<i>Collector</i>	<i>Urban</i>	<i>2,000 to 3,000</i>
<i>Collector</i>	<i>Suburban</i>	<i>1,000 to 2,000</i>
<i>Residential</i>	<i>Urban</i>	<i>&lt; 2,000</i>
<i>Residential</i>	<i>Suburban</i>	<i>&lt; 1,000</i>

\*\* Capacity is measured using the Average Daily Traffic (ADT)

To improve the LOS for a roadway, either the capacity must be increased or the volume of traffic using the road must be decreased. To increase the capacity, the City can look at several options such as roadway improvements ranging from adding signals or separated turn lanes to an intersection to roadway widening. To reduce traffic volumes, the City can explore options such as changing allowable land uses or modifying individual travel behavior. This section



focuses on capacity improvements. Chapter 7 discusses other travel modes and methods of transportation demand management.

**Intersection LOS measurement**

Intersection LOS is measured by the amount of delay experienced by a vehicle waiting to clear an intersection. Delay at a signalized intersection can be caused by waiting for the signal or waiting for the queue ahead to clear the signal. Delay at un-signalized intersections is caused by waiting for a break in traffic or waiting for a queue to clear the intersection. Table 3-6 shows the amount of delay used to determine LOS for signalized and un-signalized intersections. Roundabout-controlled intersections use the same LOS thresholds as signalized intersections.

**Table 3-6. Intersection LOS and Delay**

LOS	Signalized Delay per Vehicle (sec/veh)	Unsignalized Delay per Vehicle (sec/veh)
A	0-10	0-10
B	>10-20	>10-15
C	>20-35	>15-25
D	>35-55	>25-35
E	>55-80	>35-50
F	>80	>50

*Generally, speaking... Roadways that are LOS E or F fail the standard.*

*LOS D is okay for certain arterials and collectors in urban areas*

*LOS A, B or C are within the standard for all arterials and collectors*

Different delay standards are used for signalized (stop light controlled) and unsignalized (stop sign controlled) intersections. For signalized and all-way stop controlled intersections, the LOS is the amount of delay per vehicle caused by control and is reported for the intersection as a whole. For un-signalized intersections, where there are controls only on the minor approaches, the LOS is estimated by the average delay per vehicle and is reported for only minor approaches to the intersection.

**City LOS Standard**

The City of Bainbridge Island’s LOS standard designates the minimum operational performance of the roadway system that must be maintained. If traffic volumes cause a roadway to fall below the minimum LOS standard, improvements or other mitigation must be made to bring the facility back to the designated LOS standard. Level

of service standards are normally prescribed for the afternoon or p.m. peak hour (most congested hour) of the traffic system, which typically occurs between 4:45 and 5:45 in the evening on Bainbridge Island.

The recommended minimum LOS standard uses the City’s roadway classification system, and four zones that reflect the differences in the Island’s character: designated centers including Winslow and Neighborhood Centers, Conservation Area, and the SR 305 Corridor. Within each of these categories, individual minimum LOS standards were established for secondary



arterials, collectors, and residential roadways. These are shown in Figure 3-2 and described below.

***Winslow – (applies to roadways and intersections in the greater Winslow area)***

- Secondary Arterial – LOS D
- Collector – LOS D
- Local Access – LOS C

***Neighborhood Centers – (applies to roadways and intersections within the City-defined Centers of Rolling Bay, Island Center, and Lynwood Center)***

- Secondary Arterial – LOS D
- Collector – LOS C
- Local Access – LOS C

***Conservation Area – (applies to roadways and intersections in areas outside of the Winslow core and the Designated Centers – the remainder of the Island)***

- Secondary Arterial – LOS C
- Collector – LOS C
- Local Access – LOS B

***SR 305 Corridor – (applies to state highways and is established by the State)***

- All Roadways– LOS D

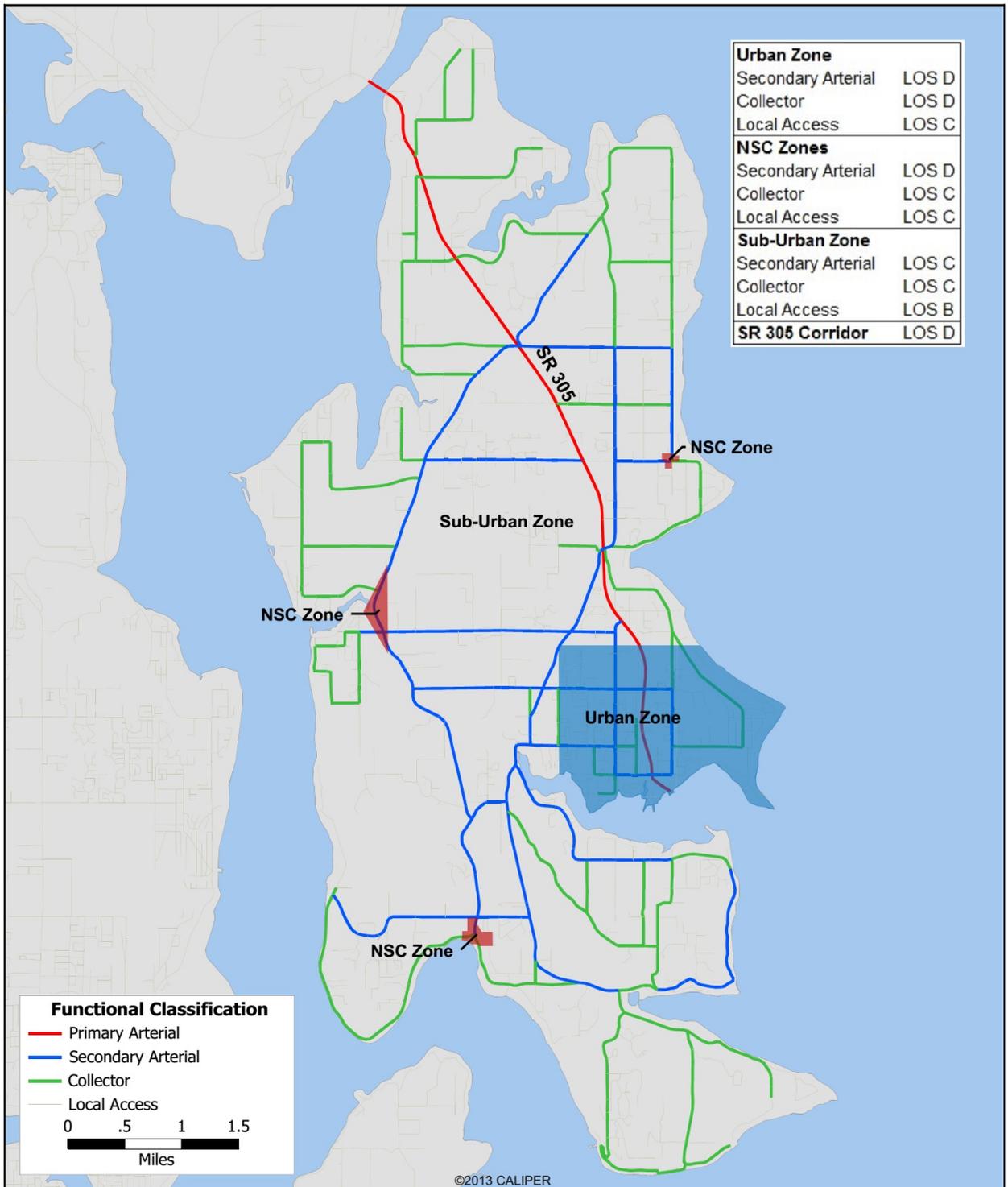
***SR-305 LOS Standard***

*The LOS standard for state facilities is set by the Washington State Department of Transportation as a Highway of Statewide Significance (HSS) under RCW 47.06.140. The HSS designation requires that SR-305 be evaluated using a LOS Standard designated by WSDOT. While WSDOT internally evaluates roadways using its own methodology, WSDOT has assigned a level of service standard for SR-305 as LOS D-mitigate for City planning purposes. This standard requires that congestion be mitigated when the peak period operation of the state facility falls below LOS D.*

***Non-Motorized LOS Standard***

The facility types and associated level of service for non-motorized transportation elements for secondary arterial and high volume collector (ADT 1500 or greater) roadways are established in Chapter 6, “Non-Motorized Systems” of this plan. The minimum Bicycle Level of Service (BLOS) and Pedestrian Level of Service (PLOS) for development in urban areas is level of service C. The minimum BLOS and PLOS for development in suburban areas is level of service D. PLOS and BLOS is calculated using the methodology in the latest edition of the Highway Capacity Manual. The 2010 Highway Capacity Manual (HCM 2010) provides a detailed methodology for calculating level of service for pedestrians and cyclists. The level of service is based on quality of facilities as well as traffic volume and speeds. LOS measures are graded A through F based on a numerical score with the letter A representing the highest grade facility.





**Figure 3-2**  
**Recommended Level of Service Standards**





## Existing Traffic Conditions

This section describes the traffic conditions for the 2014 Plan year. The Plan is based on traffic data collected for roadway segments in 2012 and intersection counts in 2014.

### ***Transportation Model***

A consultant, Transportation Solutions, Inc. (TSI) developed a citywide transportation model to estimate existing travel demand and to provide a tool for forecasting future travel demand on City roadways. Current and future travel demand were used as inputs to a citywide operational model, developed using Synchro software, to evaluate current and forecasted PM peak hour levels of service throughout the city's roadway network. The demand model is based upon the concept of vehicle trips; pedestrians and cyclist demand is not forecasted. Similarly, carpool, vanpool, or transit users are represented by single vehicles in the model.

For analysis of existing conditions, the TransCAD-based model used existing land use data from Kitsap County and Puget Sound Regional Council (PSRC), roadway information from the City, and TSI traffic counts to reproduce existing trips and their paths, from origin to destination, through the citywide roadway network.

Land use was collected from Kitsap County at the individual parcel level and aggregated to create 241 transportation analysis zones (TAZs) which covered the entirety of the City. Two external zones were created to represent travel demand at the ferry terminal and at the north end of the Island.

Trip generation was based upon existing land use and trip generation rates established by the *Institute of Transportation Engineers (ITE) Trip Generation Manual, 9<sup>th</sup> Edition* and calibrated based on knowledge of local conditions and travel patterns. It was observed during calibration, for example, that single-family trip generation rates on Bainbridge Island were lower than the nationally-calibrated averages published by ITE. This reduced single family trip rate may be associated with a growing percentage of retirees living on the Island. Peak hour ferry trip generation rates were estimated from the WSDOT Ferries Division *2013 Origin-Destination Travel Survey Report*.

For operational analysis, a citywide traffic model was developed in Synchro software, using roadway information obtained from the City, satellite and street-level imagery collected from Google Earth, and traffic counts collected in 2014 by TSI. Relevant roadway information for operational analysis included number of lanes, intersection channelization, traffic control devices, speed limits, and lane width. Observed PM peak hour traffic volumes were applied to the roadway network to calculate intersection levels of service.

### **Peak Hour Traffic Volumes**

The City of Bainbridge Island collects traffic count data on a periodic basis to assess changes in traffic patterns, to collect information for its concurrency program, and to track the operational characteristics of the Island. In 2012, the City contracted an update of Island-wide traffic counts and travel speeds. In 2014, the City contracted intersection counts. This information was utilized in the traffic model developed by TSI. The data are included in Appendix E of this report.

### **WSDOT Ferry Travel Survey**

Washington State Department of Transportation (WSDOT) conducts origin-destination (OD) surveys every six to seven years as a way to accurately capture and measure the travel



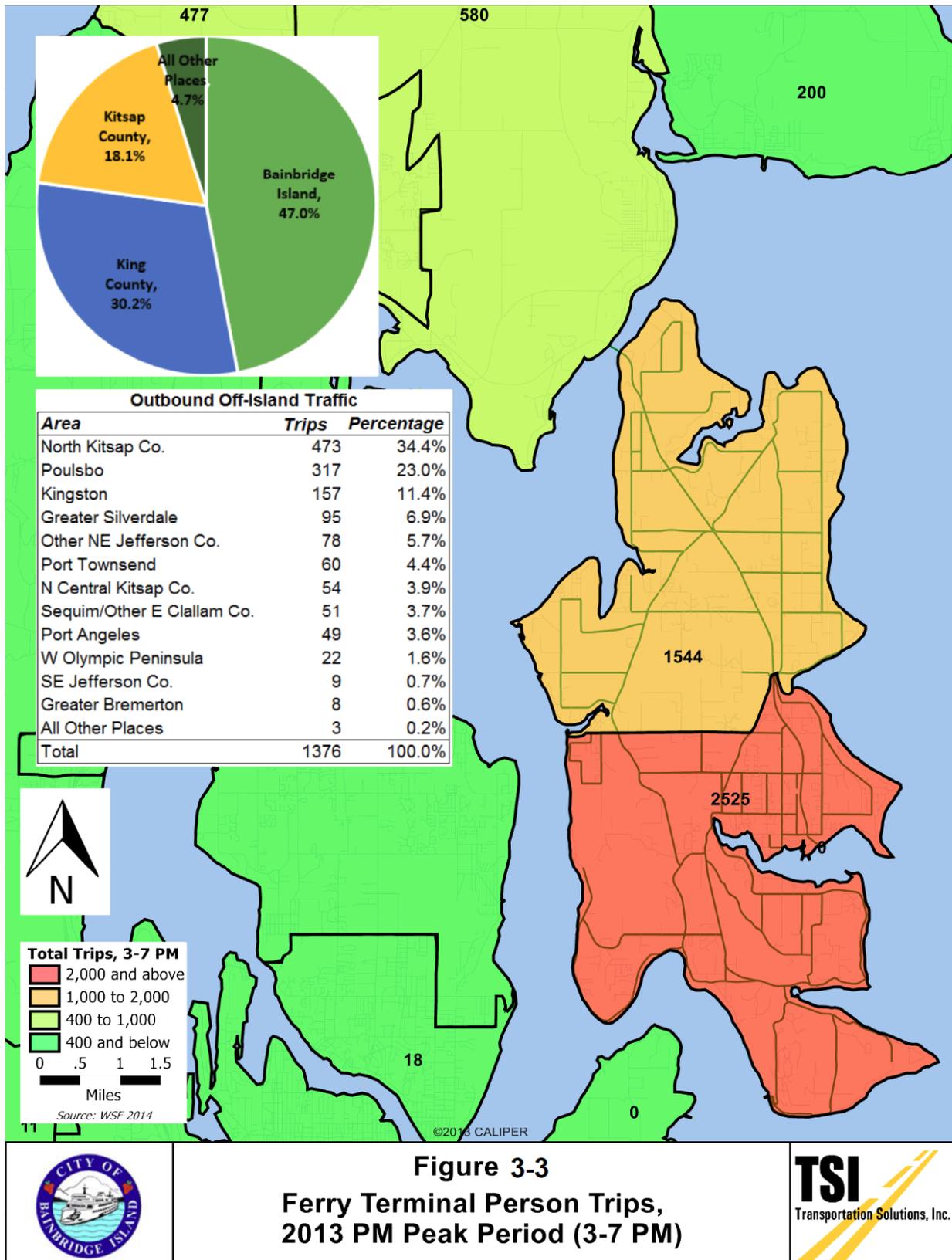
patterns of ferry passengers. Passengers were asked about their typical routes, how they get to and from ferry terminals, and the purpose of their trips.

Surveys were administered to ferry riders during weekdays and Saturdays in October 2013. Over 17,000 survey questionnaires were collected system-wide, with 92 percent of collected surveys sufficiently complete for analysis. Survey responses were used to develop a database of ferry user characteristics, including trip origin and destination patterns. TSI reviewed and processed survey results for the Seattle-Bainbridge route and used them as inputs to the citywide travel demand and traffic operations models.

Figure 4-3 summarizes survey findings for the Seattle-Bainbridge Island ferry.

Highlights of the survey results published in August 2014 are summarized below:

- Ferry ridership has declined slightly since 2006, with approximately 17,000 riders per day in 2013 compared to 18,000 riders per day in 2006. Vehicle boardings decreased by 7 percent during that period.
- The Seattle-Bainbridge route has shown an aging ridership, with the number of passengers over age 64 increasing from 8 percent in 2006 to 17 percent in 2013. System-wide, average passenger age increased from 42 in 1993 to 48 in 2006 and 49 in 2013. Currently 18 percent of riders are retired and another 14 percent are planning to retire in the next five years.
- Approximately 25 percent of weekday riders telecommute at least one day per week, up from 20 percent in 2006.
- The proportion of work- and school-related trips decreased and the proportion of recreation and shopping trips increased between 2006 and 2013.
- Of the 6,070 total (eastbound and westbound) ferry trips during the 3:00 to 7:00 PM weekday peak period, 67 percent had an origin or destination on Bainbridge Island, while the remaining 33 percent had off-Island trip ends. This indicates the WSF terminal's regional nature, with one in three travelers originating or destined for off-Island locations.
- The City of Poulsbo and other North Kitsap County locations accounted for 57% of the off-Island destinations. Other primary destinations included the cities of Kingston, Silverdale, Port Townsend, and Sequim. The results indicate that while much of off-Island traffic is coming from areas adjacent to Bainbridge Island, as many as 40% of off-Island drivers could take advantage of new or improved service to downtown Seattle from Kingston or Bremerton.
- Nearly 70 percent of total weekday PM peak period ferry trips are destined westbound, with the other 30 percent of trips destined primarily for locations within Seattle.



**Figure 3-3**  
**Ferry Terminal Person Trips,**  
**2013 PM Peak Period (3-7 PM)**



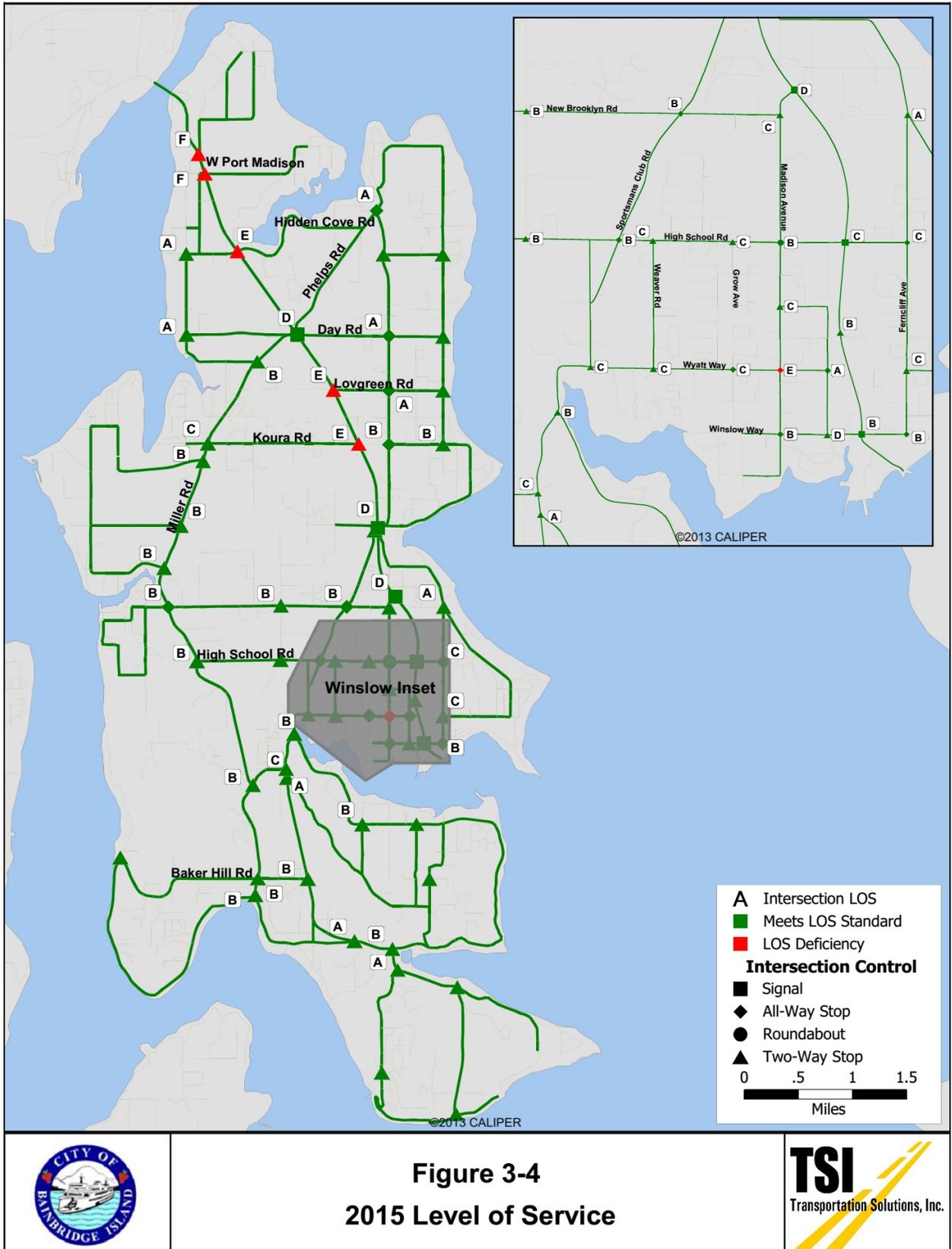


## Existing LOS

The travel demand model was calibrated using a process that compares the counted roadway volumes to modeled flows which are based on land use and roadway network data. The calibrated TransCAD model and Synchro intersection analysis software were used to determine the 2014 LOS for the intersections in the study area.

Figures 3-3 shows the 2014 LOS for the Island as a whole and for the Winslow area. The LOS for each intersection is shown by approach in Table 3-6. All intersections modeled on SR305 north of High School Road currently do not meet minimum LOS standards with the exception of the signal at Day Road. Day Road however is close to exceeding the standard. In urban areas, the Madison/Wyatt intersection currently fails the minimum LOS standard but will be improved to LOS A upon completion of a planned roundabout.

There are currently no roadway level of service failures.



**Figure 3-4**  
**2015 Level of Service**





**Table 3-6. Intersections PM Peak Hour LOS Deficiencies – 2014 Baseline**

Intersection	Control Type <sup>1</sup>	Delay <sup>2</sup> (s/veh)	LOS
Madison Ave N / Wyatt	AWSC	38.5	E
SR 305 / Koura Rd	TWSC	37.3	E
SR 305 / Lovgreen Rd	TWSC	38.9	E
SR 305 / NE Hidden Cove Rd	TWSC	48.3	E
SR 305 / Port Madison	TWSC	>180	F
SR 305 / Agatewood Rd	TWSC	>180	F

<sup>1</sup>TWSC = Two-Way Stop Control; AWSC = All-Way Stop Control; RAB = Roundabout; Signal = Signalized

<sup>2</sup>Average control delay for all movements. For TWSC, delay is reported for the movement with the highest delay.

## Future Traffic Conditions

This section identifies the land use forecast methodology and results used to identify the future needs and deficiencies of the transportation system. Two time periods were studied: 2021, representing the six-year short-term planning period, and 2035, representing the 20 year long-term planning period. 2035 matches the long term planning horizon of Puget Sound Regional Council (PSRC), the region’s major planning entity.

### Land Use Forecast

The transportation model used PSRC and Kitsap County land use forecasts to determine future PM peak hour trip growth by transportation analysis zone (TAZ). Trip growth forecasts were distributed and assigned to the future roadway network to generate expected future traffic growth citywide.

### Determination of Base Year Land Use

Base year land use was provided by Kitsap County in the form of GIS-based tax parcel data. These data were cleaned and refined based on recent satellite and street-level photography, then categorized according to the following modeled land use types:

- Single-Family Housing
- Multi-Family Housing
- Senior/Assisted/Retirement Housing
- Retail
- General Office
- Industrial and Manufacturing
- Warehouse/Utility/Storage
- Hotel
- Hospital/Nursing Home
- Park and Ride
- School
- Recreation/Entertainment
- Church

Land use data were subsequently aggregated to create 241 transportation analysis zones (TAZs), with each TAZ representing a distinct geographical trip generating unit in the travel



demand model. Table 3-7 describes the modeled 2014 land use quantities. The base year travel demand model was calibrated using 2014 traffic counts to establish a tool that reflects vehicle traffic and travel patterns for each of the TAZs.

**Table 3-7. 2014 Land Use**

Land Use Category	Quantity	Units
Single-Family Housing	8,517	Dwelling Units
Multi-Family Housing	1,311	Dwelling Units
Senior/Assisted/Retirement Housing	212	Dwelling Units
Retail	589	KSF*
General Office	316	KSF
Industrial and Manufacturing	163	KSF
Warehouse/Utility/Storage	226	KSF
Hotel	96	Rooms
Hospital/Nursing Home	69	KSF
Park and Ride	841	Stalls
School	3,355	Students
Recreation/Entertainment	207	KSF
Church	121	KSF

\* KSF equals one thousand square feet.

**Land Use Forecasts (2021 and 2035)**

The next step in the transportation modeling process was to incorporate land use forecasts to the calibrated base year travel demand model in order to establish 2021 and 2035 traffic forecasts.

In order to convert regional 2035 land use forecasts to the level of detail required by the citywide transportation model, housing and employment growth forecasts were geographically distributed to the TAZ level according to zoning and estimated land capacity. Employment growth forecasts were converted to gross floor area or equivalent modeled units using relationships established by the Institute of Transportation Engineers, U.S. Department of Energy, and San Diego Association of Governments.

Table 3-8 shows the citywide residential and employment forecasts used in this Plan.



**Table 3-8. 2021 and 2035 Forecasts**

	Households	% Change from Base	Employees	% Change from Base
2014 Base Year	10,152	--	8,600	--
2021 Forecast	11,346	12%	9,321	8%
2035 Forecast	13,248	30%	10,587	23%

Growth in households is assumed to occur at an annual rate of approximately 1.3 percent per year during the planning period. Employment growth is expected at 1.7 percent per year. The 2035 forecasts assigned a moderate rate of growth throughout the Island with the greatest commercial growth in the designated Neighborhood Centers, industrial growth focused in areas currently zoned business/industrial, and residential housing growth occurring in areas where the greatest potential for new housing under the existing zoning could occur. The 2021 forecasts were based on a straight-line interpolation of growth for each TAZ, with the assumption that the distribution of employment and housing would be proportionate to the 2035 scenario.

## Future Traffic Operations

This section describes the future traffic conditions on the City’s roadway system for 2021 and 2035. Future traffic conditions were estimated for 2021 and 2035 using the results of the land use and employment forecasts, roadway network information, and the calibrated travel demand model (including calibrated trip generation, distribution, and traffic assignment sub-models).

### 2021 Traffic Forecast

The 2021 traffic forecast was developed by applying a linear interpolation of forecasted 2035 land use growth to the calibrated base year planning model. Forecasted traffic growth was then applied to the Synchro traffic operations model to analyze 2021 levels of service. Where LOS was shown to fall below the minimum LOS threshold by 2021, mitigating improvements were added to the road network. This section describes the results of the 2021 analysis.

### 2021 LOS

The traffic model produces a forecast of 2021 traffic conditions, which are shown in Figure 3-5. Results of the 2021 forecast show continued heavy congestion and poor level of service along SR305. At locations other than SR305, there are only a few minor LOS deficiencies.

### Roadway LOS

Roadway Segment LOS at sections of Eagle Harbor Drive and Miller Road are expected to decline. Shoulder widening project are included in the City’s short term (6 year) capital improvement plan for these locations.

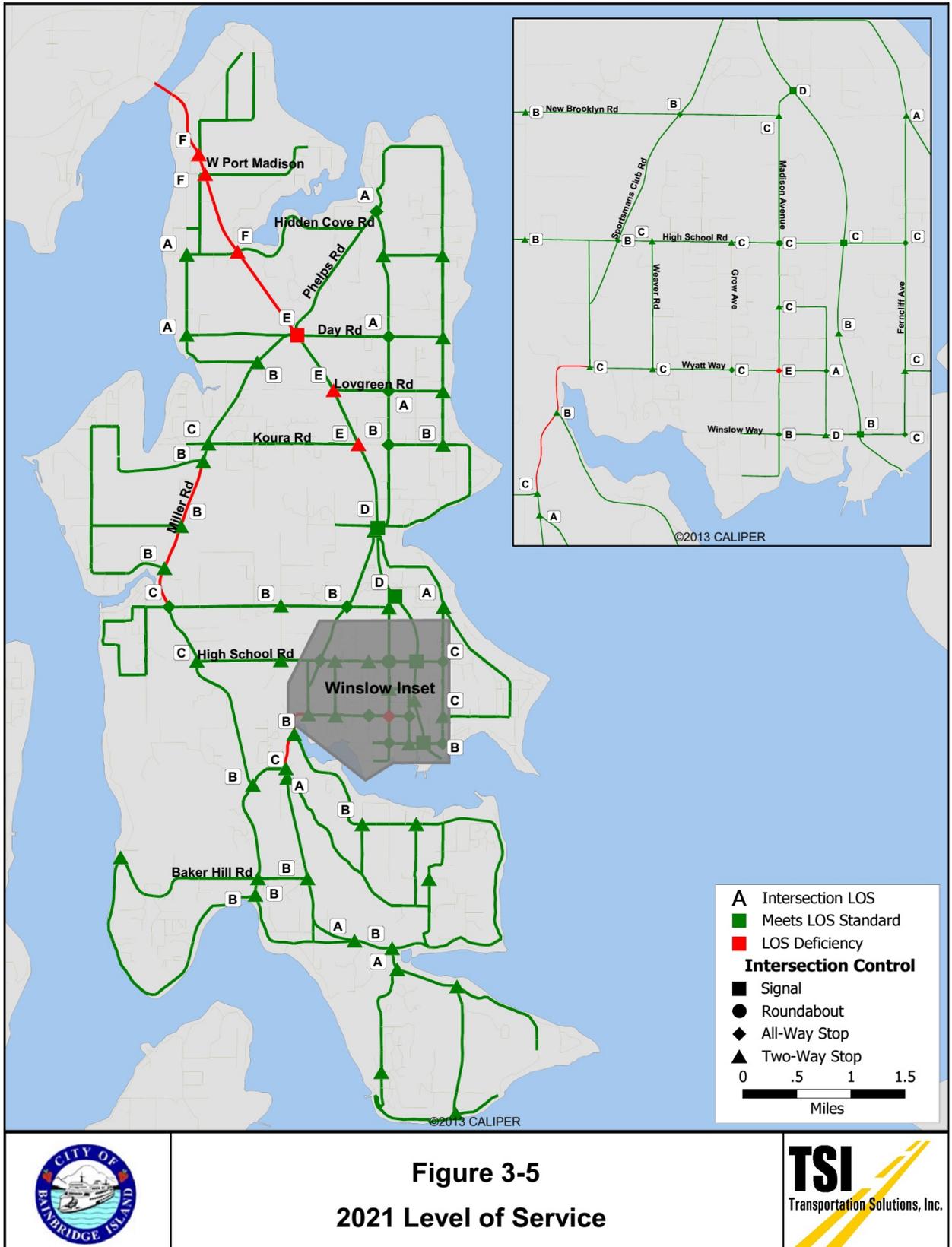
Along the SR305 corridor, north of Sportsman’s Club Rd., roadway capacity, in addition to poor intersection operation, is predicted to become an impediment to traffic flow and contribute to congestion.



### ***Intersection LOS***

The traffic model was used to identify locations where intersections may be the cause of poor operations. Table 3-9 shows the results of the 2021 Plan year intersection LOS analysis. Without mitigation, one intersection at Madison Avenue N / Wyatt Way NE fails to meet the minimum LOS standards. The intersection of Winslow Way/ Ericksen Ave. is forecasted to decline to LOS D.

On SR 305, the intersections at Agatewood Road, Seabold Road, Hidden Cove Road, Lovegreen Road, and Koura Road all fail to meet the minimum standard. By the 2021 forecast year, SR 305 corridor congestion continues to deteriorate with the intersection at Hidden Cove Rd falling from LOS E to LOS F. The intersection at Day Road is anticipated fail. The poor operation of the highway intersections, if not addressed, will increasingly be a barrier to cross-Island traffic, impacting operations of the City's roadway system as a whole.





## 2016-2021 Mitigation

Each intersection and roadway segment identified as below the minimum LOS standard in 2021 was studied to see if mitigation actions could improve the intersection LOS to the minimum standard. Targeted roadway improvements can correct an intersection or roadway that fails to meet the minimum LOS standard.

### City Mitigation

For intersections in the City’s roadway system where the expected LOS is below the minimum standard, the following mitigation is proposed:

- *Madison Avenue/ Wyatt Way* – An intersection control improvement such as a signal or a roundabout would improve the intersection to LOS B. The intersection will be studied to determine what specific improvement should be constructed. A round-about may be one alternative. An improvement project is currently programed in the City’s CIP for Wyatt Way, including the intersection.
- *Eagle Harbor Drive from Wyatt to Blakely* - Shoulder improvements for non-motorized users are recommended. An improvement project is currently programed in the City’s CIP.
- *Miller Road from New Brooklyn to Arrow Point* – Shoulder improvements for non-motorized users are recommended. An improvement project is currently programed in the City’s CIP for this segment.

### WSDOT Mitigation

Six SR 305 intersections and roadway segments north of Day Road currently fail to meet LOS and will continue to deteriorate. Refer to chapter 4 of this Plan for recommendations.

**Table 3-9 Intersections PM Peak Hour LOS Deficiencies – 2021 Forecast**

Intersection	Control Type <sup>1</sup>	2021 Delay <sup>2</sup> (s/veh)	2021 LOS	Possible Mitigation
Madison Ave N / Wyatt	AWSC	44.2	E	Roundabout or signal
SR 305 / Koura Rd	TWSC	43.5	E	SR 305 Corridor Improvements
SR 305 / Lovgreen Rd	TWSC	39.4	E	
SR 305 / Day Rd	Signal	60.1	E	
SR 305 / Hidden Cove Rd	TWSC	>180	F	
SR 305 / Port Madison	TWSC	>180	F	
SR 305 / Agatewood Rd	TWSC	>180	F	

<sup>1</sup>TWSC = Two-Way Stop Control; AWSC = All-Way Stop Control; RAB = Roundabout; Signal = Signalized

<sup>2</sup>Average control delay for all movements. For TWSC, delay is reported for the movement with the highest delay.



**Table 3-10. Street Segment PM Peak Hour LOS Deficiencies – 2021 Forecast**

Segment	From	To	V/C	LOS
SR305	Day Rd	Hidden Cove Rd	0.94	E
SR305	Hidden Cove Rd	Seabold Church Rd	0.96	E
SR305	Seabold Church Rd	Seabold/W Port Madison	0.93	E
SR305	Seabold/W Port Madison	Agatewood Rd	0.99	E
SR305	Agatewood Rd	Reitan Rd	0.98	E
Bucklin Hill Rd	Blakely Ave	Eagle Harbor Dr	0.84	D
Miller Rd	New Brooklyn Rd	Battle Point Dr	0.99	E
Miller Rd	Battle Point Dr	Tolo Rd	0.84	D
Miller Rd	Tolo Rd	Arrow Point Dr	0.85	D
Eagle Harbor Dr	Bucklin Hill Rd	Finch Rd	0.84	D

### 2035 Traffic Forecast

The analysis of 2035 traffic conditions provides a long-range view of how the roadway system will operate on the Island. The 2035 traffic forecast considers housing and employment growth forecasted by PSRC and by Kitsap County, as well as any roadway network changes that would impact traffic operations. This section describes the results of the 2035 analysis.

### 2021-2035 Model Forecast Improvements

Few projects have been programmed into the traffic model to be constructed between 2021 and 2035. The City’s traffic plan has not been updated since 2004 and was not formally adopted. The State has recently begun longer term planning for the SR305 and other corridors.

### 2035 LOS

The traffic model produces a forecast of 2035 traffic conditions, which are shown in Figure 3-6. Results of the 2035 forecast show continued heavy congestion and poor level of service along SR305 and some minor intersection problems in the Winslow area.

#### Roadway LOS

Analysis of the expected traffic in 2035 shows that most of the City’s roadway system would continue to meet the minimum LOS standards with the roadway system in Winslow, including SR 305 intersections, generally operating acceptably. Based on the City’s existing capacity policy, some roadway LOS failures would still exist on Eagle Harbor Drive and Miller Road.

For the 2035 forecast year, LOS on SR305 from Day Road to the north end of the Island is expected to continue to decline, if roadway segment capacity improvements, in addition to intersection operation improvements, are not addressed.

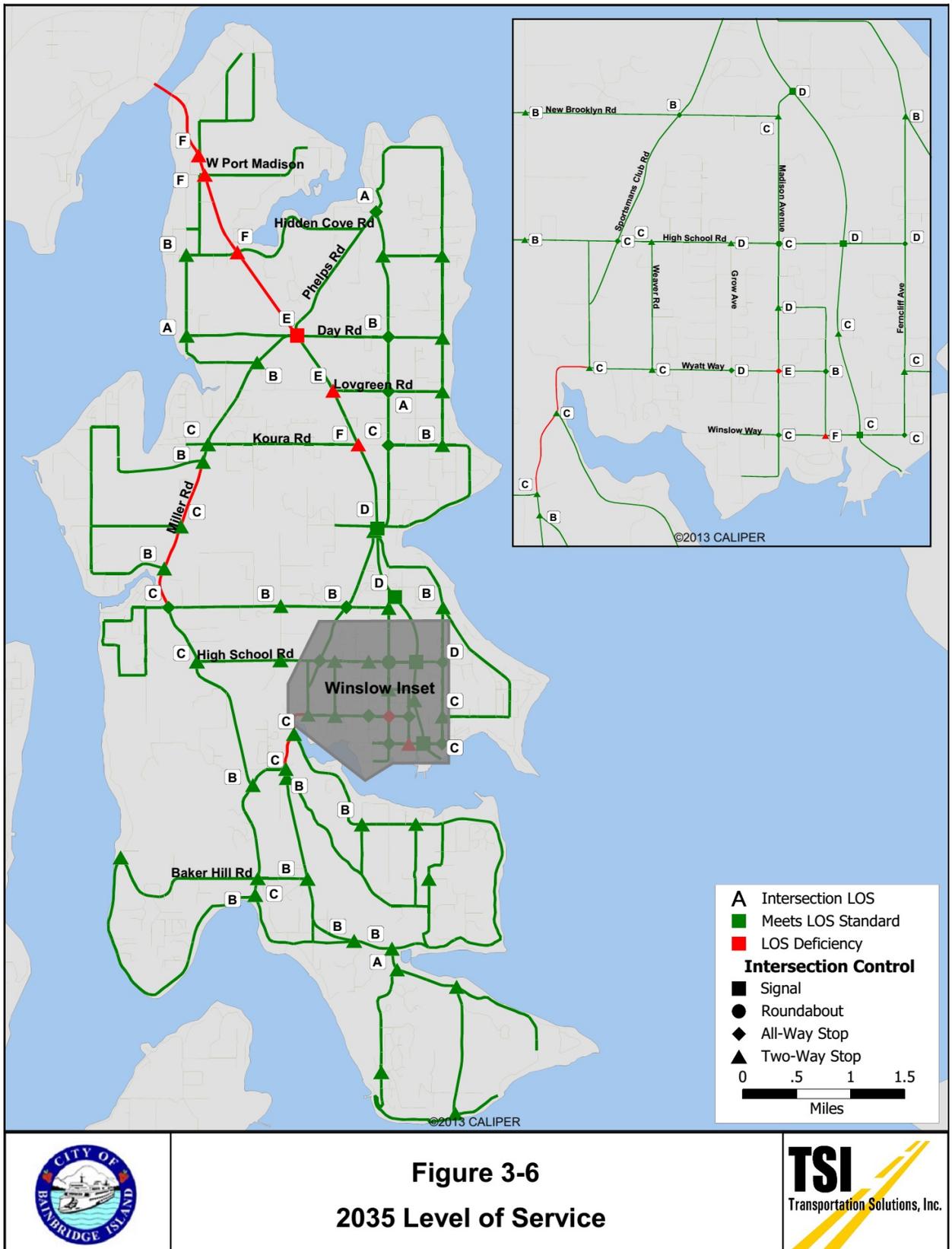
#### Intersection LOS

The intersection analysis results from the 2035 Plan year are shown in Tables 3-11 and 3-12. Assuming the identified short term planning horizon improvements are provided in the Winslow



Area, further intersection improvements are needed or anticipated. At the intersection of Winslow Way and Erickson restricted turning movements are advised to maintain LOS.

By 2035, the increase in traffic on SR 305 is expected to result in continued deterioration of intersection operations. Excessive delay would occur at nearly all of the intersections north of Day Road. The intersections at SR 305 and Koura Road would further deteriorate from LOS E to LOS F.





**Table 3-11. Intersections PM Peak Hour LOS Analysis – 2035 Forecast**

Intersection	Control Type <sup>1</sup>	2035 Delay <sup>2</sup> (s/veh)	2035 LOS	Possible Mitigation
Madison Ave N / Wyatt	AWSC	42.9	E	Roundabout or signal
Winslow Way / Erickson Ave	TWSC	64.4	F	Access restrictions / RIRO
SR 305 / Koura Rd*	TWSC	51.2	F	SR 305 Corridor Improvements <sup>3</sup>
SR 305 / Lovgreen Rd <sup>4</sup>	TWSC	45.1	E	
SR 305 / Day Rd	Signal	78.7	E	
SR 305 / Hidden Cove Rd <sup>4</sup>	TWSC	>180	F	
SR 305 / Port Madison	TWSC	>180	F	
SR 305 / Agatewood Rd	TWSC	>180	F	

<sup>1</sup>TWSC = Two-Way Stop Control; AWSC = All-Way Stop Control; RAB = Roundabout; Signal = Signalized

<sup>2</sup>Average control delay for all movements. For TWSC, delay is reported for the movement with the highest delay.

<sup>3</sup>Specific corridor improvements identified below

<sup>4</sup>Alternative access to SR 305 is provided for locations w/ right in and right out (RIRO) access during PM peak hour:

- Koura Rd access via Miller Rd
- Lovgreen Rd access via N Madison Ave or Miller Rd
- Hidden Cove access via Phelps Rd, Seabold Rd or Day Rd

**Table 3-12. Street Segment PM Peak Hour LOS Analysis – 2035 Forecast**

Segment	From	To	V/C	LOS
SR305	Day Rd	Hidden Cove Rd	0.95	E
SR305	Hidden Cove Rd	Seabold Church Rd	1.03	F
SR305	Seabold Church Rd	Seabold/W Port Madison	1.01	F
SR305	Seabold/W Port Madison	Agatewood Rd	1.05	F
SR305	Agatewood Rd	Reitan Rd	1.04	F
Bucklin Hill Rd	Blakely Ave	Eagle Harbor Dr	0.86	D
Miller Rd	New Brooklyn Rd	Battle Point Dr	0.97	E
Miller Rd	Battle Point Dr	Tolo Rd	0.81	D
Miller Rd	Tolo Rd	Arrow Point Dr	0.82	D
Eagle Harbor Dr	Bucklin Hill Rd	Finch Rd	0.85	D

### 2021-2035 Mitigation

Mitigating the LOS for the City intersections would require minor improvements which can be programmed into the City's future transportation improvements program. The increased traffic volume expected to use SR 305 in 2035 would overwhelm the existing facility, resulting in a situation that cannot easily be mitigated.



### *City Mitigation*

Improvement to intersection channelization and/or intersection control can mitigate the substandard LOS at all of the City intersections. The following project are proposed to improve LOS at the identified substandard intersection:

- *Ericksen Avenue at Winslow Way* – An intersection control improvement such as prohibiting left turns during peak traffic hours is recommended.

### *WSDOT Mitigation*

Roadway segments along the seven-mile SR 305 corridor within the study area will operate at LOS F. This problem is based on lack of roadway capacity that affects the intersection operation as well, making it extremely difficult to mitigate individual locations. Any mitigation that is proposed would need to be examined on a corridor basis, and would need to be consistent with WSDOT operational objectives, as well as City’s goals and objectives with regard to traffic operations, environmental and community character concerns. An individual solution for each problem location would not provide an adequate assessment of the corridor-wide issues that are present on the highway.

There are a number of possible solutions that could be proposed to mitigate the corridor. In order to adequately explore possible solutions, a special study was performed for this corridor. The results of the study are explained in Chapter 4.

## **Other Mobility Issues**

There other issues that affect the mobility of traffic on the roadway network. These issues include factors that influence how traffic operates and connects to the City’s roadway system. The three areas discussed in this section include the connectivity of the roadway system, access management, and special study areas identified by the Steering Committee.

### ***Connectivity***

Connectivity is defined as the level of connections between roadways in a transportation system. In concept, connectivity describes the efficiency of travel between any two points on the roadway system. A high level of connectivity is characterized by a well-developed street network, available alternative routes, quick response times for emergency vehicles, good mobility for pedestrians and bicyclists, and an efficient use of the roadway system. A low level of connectivity is characterized by numerous dead-end streets, cul-de-sacs, and roadways that do not connect, resulting in poor response times for emergency vehicles, circuitous routing of pedestrian and bicycle travel, and inefficiencies in traffic flow. Low connectivity can also result in interrupted access to areas in the event of a road closure such as a traffic accident or landslide, and can cause a high level of congestion and bypass traffic on the available streets.

On Bainbridge Island, an example of an area with relatively high connectivity is the Winslow area, where the street network is more developed and few streets end in dead-ends or cul-de-sacs. However; there are areas in Winslow where there are “super blocks” which inhibit connectivity. Many parts of the conservation area have low connectivity.



Connectivity improvements are usually undertaken to solve potential safety problems or to improve traffic flow. New connections can be constructed to provide alternative access in areas where there is only one roadway serving many homes or businesses, where the existing road is unstable due to steep slopes or erosion, or where an alternative route is needed to provide relief to an overly congested route.

Seventeen connectivity projects have been identified across the Island to be developed as traffic and other needs dictate. These are shown in Figure 3-10 (general area of connection shown with star) and described in Figure 3-11. The potential connections shown are recommended for development by the Steering Committee. The recommendations were developed by looking at the needs of schools, fire and emergency medical response, and other public facilities, as well as access to landlocked properties. Each potential connection will be considered separately as traffic patterns and emergency response times warrant, will be studied to identify potential impacts, and will include discussions with affected property owners. Connections will be included with other nearby projects if possible. Connectivity improvements are not included in this Plan's 2035 traffic model.

### ***Access Management***

Access management is the control of the number and location of access points along a roadway, in order to provide access to property, maximize safety for all roadway users, and optimize roadway operations. Access management is especially important on arterial roadways and highways.

Access management is generally implemented on roadways for three reasons: to improve roadway operations, to improve safety, and to improve access to properties. Roadways operate best when all vehicles travel in a straight line. Conflict points occur when the path of one vehicle crosses the path of another. These can be at intersections, driveways, or at other locations where vehicles turn. Vehicles that slow to make turning movements, accommodate merging traffic, or allow crossing traffic flows all contribute to the reduction in the number of cars that can travel through a corridor. Reducing conflict points increases capacity and traffic speeds.

Multiple conflict points not only slow traffic and reduce roadway capacity, but also increase the potential for crashes. Rear-end and turning vehicle collisions can be minimized through the use of access management strategies that reduce conflict points. Too many conflict points can also interfere with access to properties by making it difficult for vehicles to turn across traffic, or by restricting turning movements. Access management can also improve access to individual properties by organizing driveways at locations where turning movements are safer and easier.

On Bainbridge Island, access is a major issue along SR 305 corridor, particularly north of Hidden Cove Road. Along this stretch of the highway there are multiple driveways and streets where the only access to properties is via the State SR 305.

Techniques that can be applied to increase the mobility and safety of a travel corridor vary from development of shared access points to the installation of medians or other turning restrictions.

Control techniques fall into two categories: driveway access and roadway operation. Driveway access controls prescribe the number and location of driveways for properties along a roadway



segment. Roadway operation controls provide for access to properties and cross streets. The following list identifies the techniques included in each category:

*Driveway Access Controls* / internal circulation between parcels

- shared driveways
- limits on number, spacing, and size of driveways
- consolidation of access for adjacent parcels
- use of one-way driveways
- right-in/right-out (RIRO) access
- development of access driveways on minor streets

*Roadway Operation Controls:*

- refuge lanes or two-way continuous left turn lanes
- turning movement limitations through signage and channelization
- construction of deceleration lanes
- raised medians that limit left turns
- traffic signals at high volume locations
- provisions for U-turns

The State of Washington supports the use of access management strategies to protect its key roadways and travel corridors. RCW 47.50.010 requires that access be managed along all state facilities:

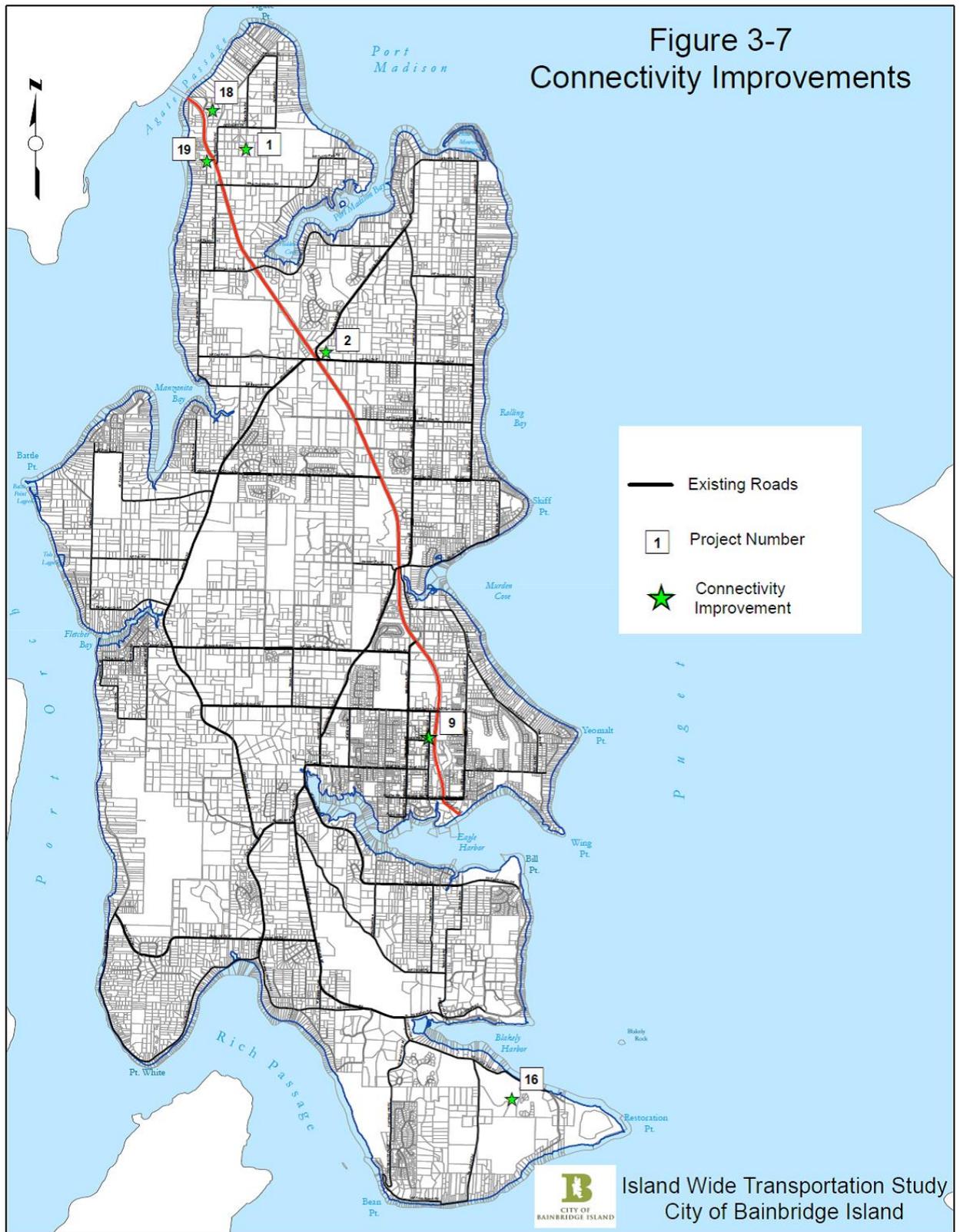
*“Regulation of access to the state highway system is necessary in order to protect the public health, safety, and welfare, to preserve the functional integrity of the state highway system, and to promote the safe and efficient movement of people and goods within the state.”*

While access management may not solve the corridor’s congestion problems, adoption of access management strategies and practices will increase the efficiency and safety of the corridor.

The City of Bainbridge Island does not currently have a formal access management program. Some aspects of access management, such as number and location of driveways and internal parcel circulation, are monitored by the Public Works Department during the site plan review process.

WSDOT manages access on state highways, including SR 305 as it crosses the Island. This highway is classified as *Partial Access Control*, which has the following definition: “Access approaches are permitted for selected public streets, roads, some crossings, and existing private driveways. No commercial approaches are permitted and no direct access if Public Street or road access is available.”







## **Figure 3-7**

### **Guide To Potential Connectivity Improvements**

This figure identifies potential connectivity opportunities. As part of the 2004 Island-wide Transportation Study, a special study was undertaken to identify motorized connectivity opportunities. Most of the identified locations have their origins in this study. Refer to the write up in the former study for commentary.

The guide to potential connectivity is focused on vehicular connectivity. Where this plan has identified locations for non-motorized connectivity in the Planned Facilities Maps (Maps C and D) in Chapter 6, commentary is provided.

Many of the potential connections are at locations where there is existing unopened or under-utilized City Rights-of-Way, or where existing rights-of-ways is in close proximity.

#15 was removed due to its distance from existing rights-of-way and location being within a Park. #19 and #20 were added as possibilities to improve access to SR305.

If and when there is further interest in developing connectivity, further study and public involvement is needed.

1. **Agate Pass Road** – The extension of Agate Pass Road between Dolphin Road and W. Port Madison Road would provide a secondary access to the area and lessen traffic impacts and delay at the intersection of Agatewood Road/SR 305. This location could also be improved to serve as a non-motorized route connecting two walkable areas of the Island.
2. **Phelps Road** – The realignment of Phelps Road, east of current intersection with Day Road would improve the intersection’s geometrics and intersection spacing from Day Road/SR 305.
3. **Not used.**
4. **Not used.**
5. **Not used.**
6. **Not used.**
7. **Not used.**
8. **Not used.**
9. **Ericksen Avenue** – The connection between Ericksen Avenue and Hildebrand Lane would eliminate the existing connection through the bank parking lot and improve the mobility of the transportation system. There is an existing non-motorized route at this location.



10. **Not used.**
11. **Not used.**
12. **Not used.**
13. **Not used.** A non-motorized route is designated in this plan at this location.
14. **Not used.** A greenway is planned for this location to enhance the exiting non-motorized route.
15. **Not used–**
16. **Country Club Road** – The connection between Country Club Road and Toe Jam Hill Road would provide an access around a potential shoreline erosion area.
17. **Not used.**
18. **Reitan Road** - Providing an access on both sides of the highway is recommended to maintain reliable access to the neighborhood as the only access is from SR305. This improvement would allow limited access for a section of SR305.
19. **Agate Beach Lane** - Providing a frontage road to link this and other properties fronting SR305 is recommended to maintain reliable access. This improvement would allow limited access for a section of SR305. This improvement would also provide an alternative route to SR305 for non-motorized users. The existing Rotary welcoming park is located in the State's ROW and may need to be altered to provide for this improvement.
20. **Lovgreen Road** – A connection to Miller Road would provide alternative access to SR305, maintaining reliable access to the neighborhood and to SR305 via Miller Road. A non-motorized trail currently exists in this location.

## CHAPTER 4 SR 305

State Route 305 is the State Highway System's primary connection via the Washington State Ferry (WSF) between Seattle and the Kitsap Peninsula. Traffic during the morning and evening peak travel hours is congested resulting in long delays. This chapter reviews the issues associated with SR 305 and its impact to the City's roadway system. The chapter also describes a special study that was performed, and recommendations for future actions.

### Summary of SR 305 Issues

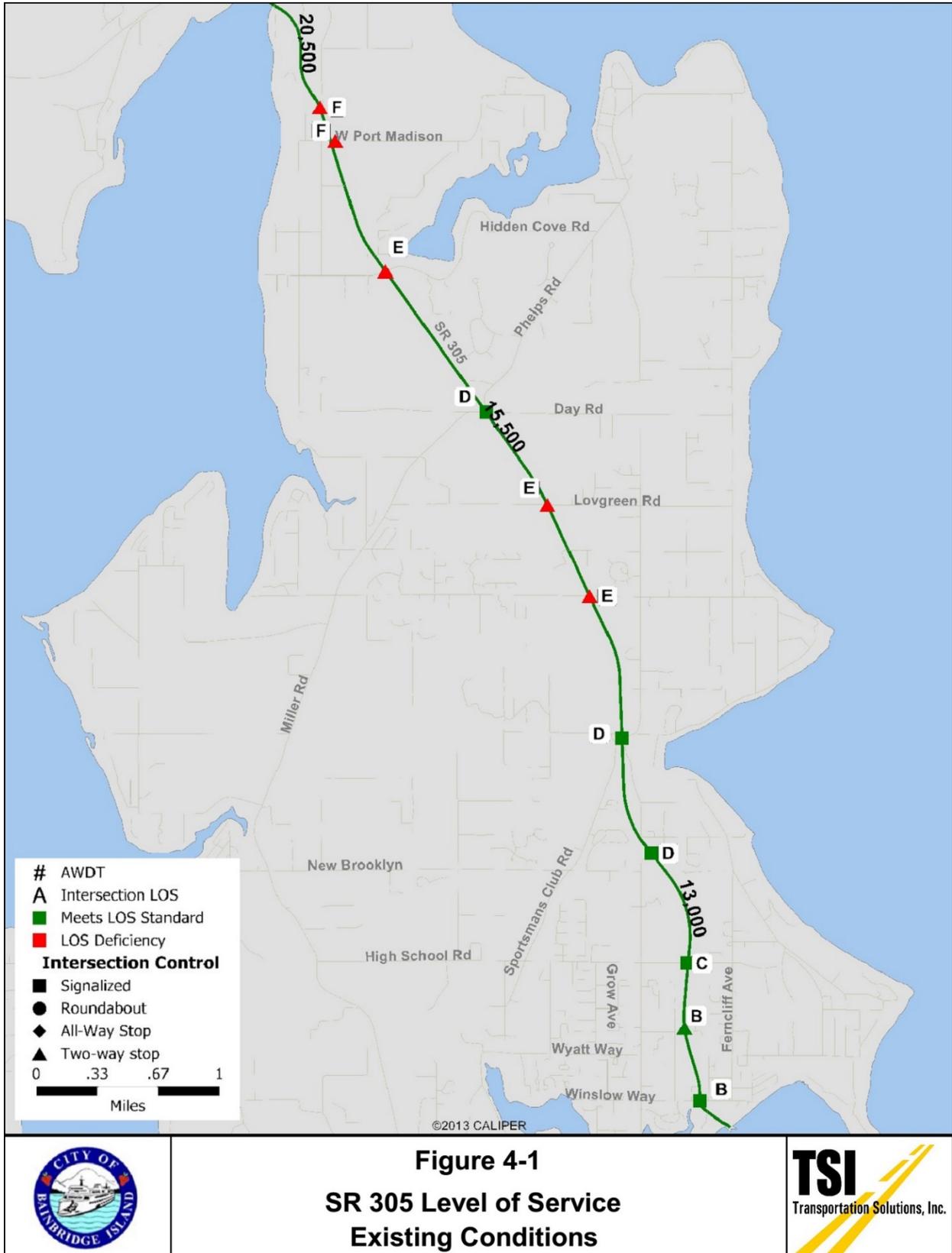
SR 305 is significant to the City's roadway system as the major north-south travel corridor on the Island, not only for through traffic traveling to and from the ferry dock, but also for Island residents and employees. The Comprehensive Plan goals and policies address the LOS standard, access to the Island via the Agate Pass Bridge, improvements to the highway, impacts to the highway from the City's Comprehensive Plan elements, and off-Island improvements that affect on-Island traffic.

As a state highway, WSDOT is the agency responsible for the operation and maintenance of SR 305. This means that WSDOT sets the minimum LOS standard and is responsible for the funding and implementation of any improvements to the highway. According to WSDOT policy, control of the highway within a City's corporate limits can be transferred to the City if its population is greater than 22,500. Bainbridge Island exceeded this population threshold in 2010 with a population of 23,025, according to US Census data. As a result, some of the responsibility for highway improvements could shift to the City, however because SR 305 is a regional facility and is listed as a Highway of Statewide Significance, some responsibility could also remain with the WSDOT.

### SR 305 LOS Impacts

The traffic analysis (described in Chapter 3) shows that current conditions on SR 305 do not meet the WSDOT minimum LOS standards, and future traffic will be even worse. Currently, along the SR305 Corridor all collector street intersections fail and one secondary arterial intersection (Koura Rd.) does not meet level of service standards. The PM peak hour average speed along the seven-mile corridor is currently 16 miles per hour, with several roadway segments operating below the average speed. The problem is most severe at the north end of the Island, where there are large back-ups beginning at the Suquamish Way intersection and Agate Pass Bridge. By 2021, all of these locations will have failed LOS. Additionally, by 2035 the Day Road intersection will be LOS D and approaching falling below standard. The corridor is forecasted to operate with an average speed of 14 mph by 2035, which is less than one-third the posted 45 mph speed limit at the north end of the Island. The expected level of service for the highway without improvement – described as the No Action alternative as shown for the 2015 and 2035 years in Figures 4-1 and 4-2.







**Figure 4-2**  
**SR 305 Level of Service**  
**2035 No Action**



### **What Makes SR 305 Different?**

The traffic issues on SR 305 are different than the issues associated with the rest of the Island’s roadway system for several reasons. First, the highway facility is owned and operated by the WSDOT. This is significant because WSDOT is the lead agency and would have primary decision-making and financial responsibility for improvements to the highway. Second, even though the highway functions as a main north-south corridor for Island travel, it is also heavily used by regional traffic, especially by vehicles traveling to and from the ferry terminal in Winslow. Because the WSF controls the ferry schedule, they have a great deal of influence on when and how much ferry traffic is using the highway. Third, the highway experiences substandard levels of service over most of the seven-mile length of the highway on the Island and the Agate Pass Bridge. Improvements to the highway would require several large projects that could be expected to require significant time to complete the planning, design, and construction of each, as well as a significant financial outlay.

### **SR 305 Special Study**

Because of the major issues associated with SR 305 improvements, a preliminary study was undertaken to determine what kind of possible improvements could resolve the traffic issues without looking into the environmental, financial, or other issues associated with the improvements. The goal of the study was to identify possible improvements along SR305 to compare their effectiveness to improve mobility along the corridor, improve permeability across the corridor, and provide reliable access to neighborhoods whose only access is from SR305. Based on this information, the NMTAC and City Staff, include recommendations in the IWTP to better position the City to advocate for improvements.



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Because SR 305 is a state facility, all improvements would require a commitment by WSDOT to be constructed. The City could participate in the improvements in order to improve mobility and level of service for the City roadway system.

### **Special Study Alternatives**

Three preliminary alternatives were developed to examine different future scenarios to see if there is a way to overcome the SR 305 operational deficiencies. Alternatives for at grade signalized intersections, at grade roundabouts, and separated grade intersection improvements. Refer to Figures 4-3, 4-4, and 4-5.

### **Special Study Results and recommendations for further study**

The three improvement alternatives were analyzed and compared to see how well they were able to meet LOS minimum standards. The special study compares at-grade and separated grade alternatives. Both at grade and separated grade alternatives maintain an acceptable LOS at intersections. However, in some locations alternative longer routes would need to be taken to access intersections meeting LOS standards. Additional intersection improvements could be evaluated in a more comprehensive plan. Roadway level of service failures are not

mitigated in either of the two alternatives but would require additional roadway capacity along the SR305 corridor (e.g. in the form of added travel lanes) or decreased volume. Note that it is assumed in the analysis that the SR305 intersection at Suquamish Way will continue to be improved so as not to have a ripple effect on Island intersection locations.

Further study is needed to design alignments and develop reliable cost estimates to plan for maintaining adequate level of service both currently and in the next 20 years along SR 305. Grade separated alternatives would be significantly more costly to implement than at grade alternatives. Both alternatives achieve acceptable LOS. Therefore, it may be difficult to justify the additional cost of grade-separated alternatives, especially larger interchanges. Some combination of intersection improvements and limited access is needed to reduce congestion and provide for reliable access. It may be practical to incorporate less extensive grade separation options for both motorized and/or non- motorized modes to maintain permeability along the corridor.

The SR305 corridor as it exists today and with any future improvements has a significant impact on many aspects of transportation on Bainbridge Island. Further study should be inclusive of and comprehensive to address all aspects. The following issues have been identified for inclusion in further study of the corridor:

- Operations of adjoining roadway networks and connectivity – The study should consider the effectiveness of the adjacent roadway networks along the corridor. There may be opportunities to mitigate cut-through-traffic and improve connectivity. There may be impacts to circulation and neighborhoods.
- Corridor Permeability – The 2004 IWTS included a special study that looked at two improvement scenarios. Permeability for all modes remains a key consideration for any scenario.
- Maintaining reliable access for neighborhoods – For many neighborhoods, such as in the Agate Pass and West Port Madison areas, the only access is from roadways that connect to SR305. Maintaining reliable access is an important aspect of any scenario.
- Sound to Olympics Trail and Intra-Island Trails – The City envisions a network of regional and sub-regional separated pathways along and crossing the SR305 corridor. The existing and potentially wider highway presents a barrier to many users. Permeability for active modes of transportation is a key consideration for intersection and other improvements.
- Bus Transit – Improving efficiency of and access to transit along the corridor is an important aspect that should be studied and integrated into all scenarios. Collaboration with Kitsap Transit is needed to explore possibilities.

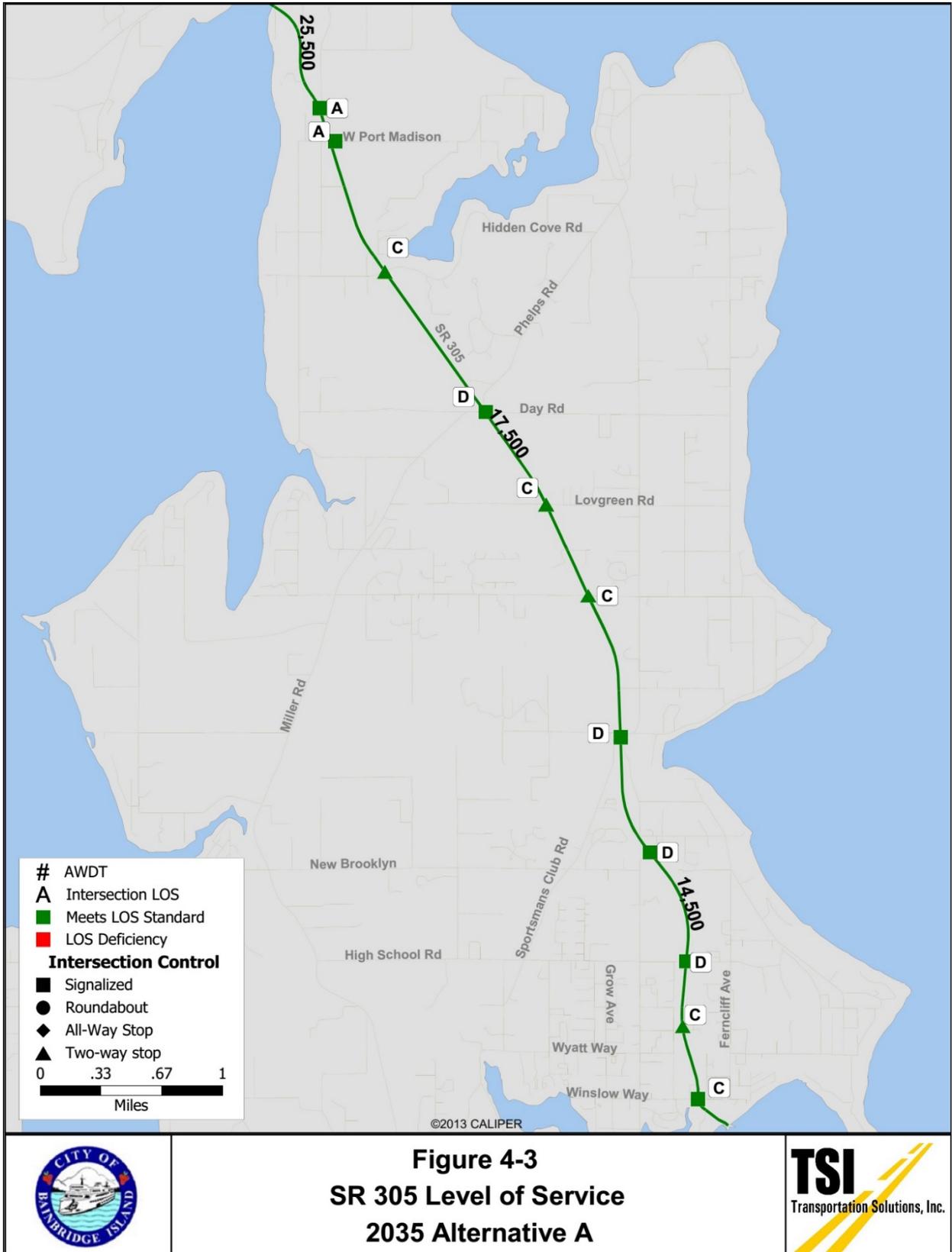
## **Other SR 305 Issues**

The deficient level of service is the most significant issue currently affecting the City's transportation system. The bridge, park and ride, and off-Island improvement issues will be addressed in future studies in conjunction with an overall plan for SR 305 improvements. The City should take a leadership role in initiating studies to develop improvement projects and not defer to WSDOT's timeline and priorities. The City should partner with Kitsap Transit and others to reduce vehicular demand on the Highway.

## **Past Improvements to SR 305**

Since the 2004 IWTS, WSDOT has implemented a number of intersection projects including the following:

- Signal improvements at N. Madison.
- Signal timing optimization for peak hour ferry offloading at the Winslow Way intersection
- Signal timing optimization for the Day road intersection to improve access from Day and Miller.
- Bike through lane on right improvements to the north and south legs of the intersections at Madison, Sportsman's Club/ N. Madison, and Day Roads.
- Right hand turn lane improvement to the south leg of the Suquamish Intersection, including bicycle lane, sidewalk, and crossing improvements.





**Figure 4-4**  
**SR 305 Level of Service**  
**2035 Alternative B**





**Figure 4-5**  
**SR 305 Level of Service**  
**2035 Alternative C**



## SR305 Recommendations:

A number of interim and long term recommendations are as follows based on the special Study.

### ***Interim Improvements:***

The following interim improvements are recommended at the time of this Report for the next 6 years:

- .
- Advocate for WSDOT to include “do not block” intersection signage at intersections north of Day Road, Hidden Cove, West Port Madison, and Agate Point in the above WSDOT project.
- Intersection improvements at West Port Madison eliminating access to Seabold and providing a receiving lane (similar to Agate Pass) for south bound traffic. The intent of this proposal is to reduce cut through traffic in the Seabold neighborhood and improve access to SR305 from West Port Madison Road.
- Advocate for consistent 8 foot or wider paved shoulders along the full length of the corridor to accommodate cyclists and pedestrians.
- Advocate for the Sound to Olympics Trail and its branch trails.
- Advocate for improved non-vehicular access to ferry and bus transit including park and ride and bike parking opportunities both on and off island.

### ***Long Term Recommendations:***

The following long term improvement projects are recommended:

- Advocate for continued improvements at the intersection to Suquamish to address north-south mobility/capacity.
- Advocate for capacity improvements to roadway segments north of the Miller Road intersection. Alternatives may include HOV lanes, a reversible HOV lane, or shoulder use by HOV's. Consider accommodation for bus rapid transit.
- Advocate for Agate Pass Bridge replacement.
- Advocate for a separated pathway for non-motorized users in conjunction with other improvements.
- Advocate for limited access improvements at Reitan in conjunction with the bridge replacement. This would include access for Reitan and possibly connection frontage roads from both sides of the highway in conjunction with the bridge replacement.
- Advocate for intersection improvements at Agate Point & West Port Madison to restore access to these “highway locked” areas. A joint signal may be the most economical solution, if spaced evenly with adjacent signals allowing for signal synchronization. This could interrupt the continuous traffic at peak hours should the WSDOT proposed round-about be constructed at Suquamish Way. Note that this signal could be programmed to flash yellow/ red during non-peak hours.
- Advocate for intersection improvements at Day Road. Improvements to accommodate additional (4 lanes) in the north-south direction at the signalized intersection would help with queuing for operational efficiency. The Phelps Road intersection with Day Road is in close proximity to SR305. If funding can be secured for a two lane round-about it may be a preferred solution to address this complexity. With either a wider signalized intersection or two lane round-about, additional facility investments would also be needed to accommodate pedestrians and cyclists.

The above recommendations are based on information from the special study that was included in the update of this Plan. The special study was limited to the LOS data developed using the updated traffic data and traffic model. Further study and preliminary design and engaging the community in a process for decision making is recommended prior to developing and prioritizing specific improvement projects. The priorities for funding are based on reducing traffic congestion on SR305 and maintaining access at intersection locations with no alternative access.

At the time of the writing of this Plan a gas tax increase had been passed by the State Legislature. The City of Bainbridge, along with Kitsap County, the Suquamish Tribe, and the City of Poulsbo are organizing a multi-agency effort to plan improvements for the corridor. WSDOT is undertaking a State-wide effort for planning corridors including SR305, called “Corridor Sketches”.

New State funding may provide for intersection improvement at Suquamish Way and as much as 6 million dollars of improvements on Bainbridge Island. The level of funding for Bainbridge could address intersection improvement and other related work at the Day Road intersection, the Agatewood/ W. Port Madison intersections, and possibly some limited access roadway improvements. At other intersections along SR305 where there are alternative routes to access SR305 access restrictions would be employed for peak hours until additional funding can be secured.

## CHAPTER 6 NON-MOTORIZED SYSTEMS



Non-motorized Users – people walking, cycling, horseback riding, and using wheelchairs – have an important place in Bainbridge Island’s transportation system. Many peak hour commuting trips as well as other trips are made walking or riding. Having non-motorized choices available is important to many Island residents. Facilities that accommodate non-motorized users provide for safety, mobility, support development density, encourage healthy lifestyles, reduce

impact to the environment, and ultimately provide for improved quality of life for Island residents, workers, and visitors.

### Background / History

Non-motorized modes of transportation have been and continue to be an integral part of Island life. From the late 1800’s to the early 1900’s, the main transportation to the Island was provided by a small fleet of steam ships referred to as the “mosquito fleet”. Roads originated at or near the “mosquito fleet” docks. Early residents walked, rode horses, and biked before the proliferation of automotive transportation. Auto ferry service was brought to the Island in the 1920’s at Agate Passage. The Agate Pass Bridge was constructed in 1950. Auto ferry service to Seattle followed in 1951. With the onset of the golden age of the automobile, reliance on non-motorized transportation declined in most places. However, walkability, biking, and horse-friendly neighborhoods remained an attractive part of the Bainbridge lifestyle. Walking and biking continued to be an important aspect of mobility within and nearby the Town of Winslow and other outlying Island town centers. With reliable transportation to Seattle, a commuter culture developed and Bainbridge evolved to be more a more conservation area. With increasing population, bus transit linking residential areas to the ferry terminal became an important element of the transportation system. In more recent times, greater awareness of health and environment, have made walking and biking more attractive modes of transportation.

The entire Island incorporated as the City of Bainbridge Island in 1991. Since incorporation, there has been a greater emphasis on non-motorized transportation planning. Following the development of the 2003 Island-Wide Transportation Plan, non-motorized transportation became a significant driver of the City’s Capital Improvement Program. The City has invested heavily in non-motorized improvements over the past decade. The following is a summary of major milestones in the City’s non-motorized planning and implementation:

- Inclusion of bicycle system planning and maps in the Transportation Element of the 1992 Comprehensive Plan.
- Development of a Trail System Master Plan in 1994.
- Recommendations for sidewalk and bicycle improvements in the 1995 Winslow Master Plan.



- Formation of a Non-Motorized Transportation Advisory Committee (NMTAC) to advise Council and support staff in December of 2002.
- Drafting of an island-wide Non-Motorized Transportation Plan in 2003. This plan included a comprehensive set of policies and goals that were later adopted in the City's Comprehensive Plan. Extensive Island-wide non-motorized existing and planned facilities maps were developed. These maps were subsequently adopted in the City's Comprehensive Plan and have evolved through several comprehensive plan updates.
- Inclusion of extensive non-motorized planning in the transportation element of the City's 2006 Comprehensive Plan following the 2003 Non-Motorized Plan.
- Formation of the Core 40 Program to provide a 40-mile integrated shoulder network for bicycles island-wide in 2007. The delivery of several Core 40 projects, including Bucklin Hill and North Madison.
- Delivery of capital improvement projects (mostly grant funded) in the Winslow area providing pedestrian and/or bicycle facilities including; Bjune, Ericksen, Ferncliff, High School, Madison, and Winslow Way.

In the 2004 Island-Wide Transportation Study, the 2003 Non-Motorized Plan was included as a separate volume. In this update, the Island-Wide Transportation Plan, includes the Non-Motorized Plan.



### **System Overview, Inventory, and Attractions**

The City's existing non-motorized transportation system consists of sidewalks, bike lanes, and trails. The City's existing non-motorized facilities are shown in Maps A and B.

Sidewalks are prevalent in Winslow and Lynwood. The city's network of shoulders on arterial streets is largely built out in Winslow. Outside of Winslow only a few roadways have paved shoulders for cyclists.

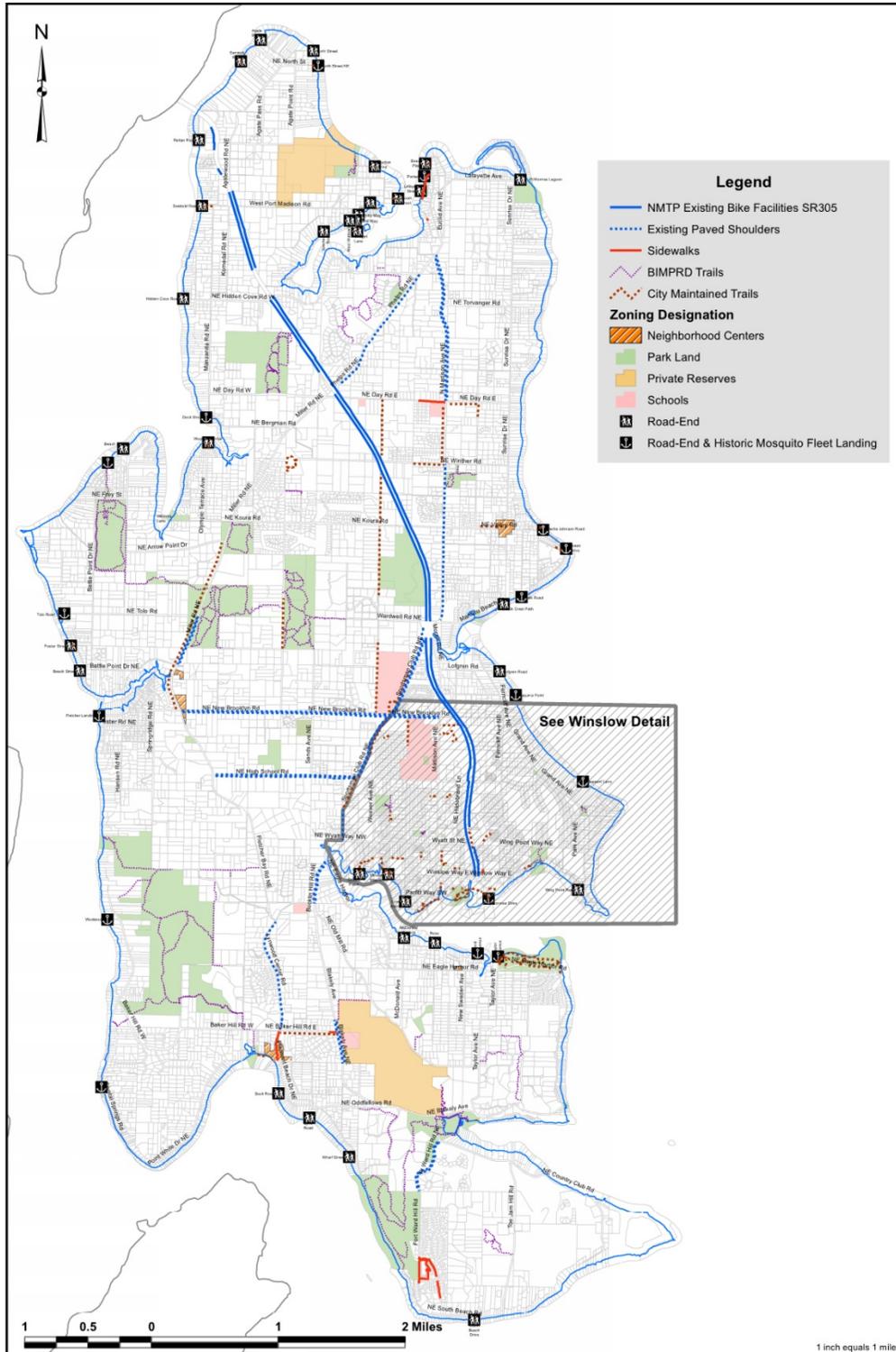
Most City trails of significant length are located within the City's rights-of-way. Other City trails connect to or through neighborhoods in formalized easements. City trails are mostly gravel surfaced and constructed to 6 feet in width although many neighborhood trails are smaller in width. The Bainbridge Island Metropolitan Parks and Recreation District (Parks District) owns and operates a network of trails within, between, and connecting to Parks that comprises most of the length of trails on the island.

There is a huge potential to improve non-motorized access to transit, goods and services, to provide recreational opportunities on Bainbridge Island and to improve the quality of life for citizens. The following nodes are identified for consideration:

- ~ Ferry Terminal
- ~ Agate Pass Bridge
- ~ Winslow
- ~ Designated centers of Day Road, Island, Lynwood, and Rolling Bay
- ~ Residential neighborhoods
- ~ Schools
- ~ Churches
- ~ Parks
- ~ Road ends and shorelines
- ~ Equestrian facilities

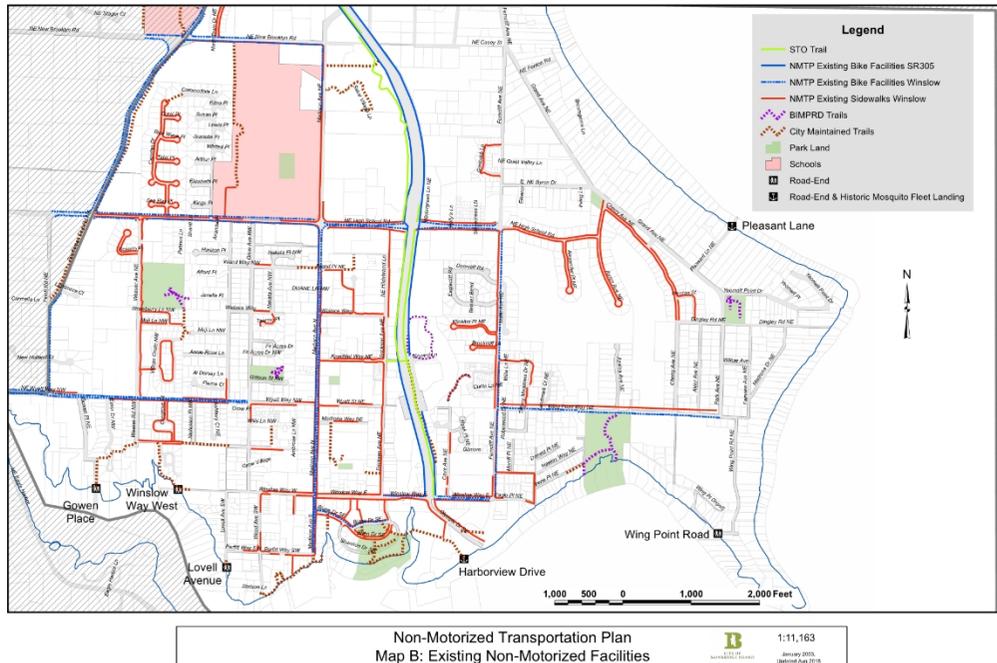


City of Bainbridge Island – Island Wide Transportation Plan  
 Chapter 6 Non-Motorized System



Non-Motorized System Plan  
 January 2003 Updated May 2016 Map A: Existing Non-Motorized Facilities





### Barriers to use and Connectivity Improvements

Barriers are physical characteristics of a transportation system that limit or restrict mobility for non-motorized users. Some common barriers on the Island are as follows:

- Inadequate maintenance including lack of shoulder sweeping for cyclists, joints at settled sidewalk panels, and poor trail surfaces in need of re-grading and compaction;
- Deficiencies in design such as lack of ADA compliant ramps, facilities that are not of adequate width to be comfortable for many users, and facilities with materials that are not ADA compliant;
- Discontinuities in system networks such as gaps in sidewalks or roadway shoulders, or bike lanes;
- Inadequate facilities at roadway intersections;
- Lack of facilities when systems do not exist or do not extend far enough to meet needs;
- Physical barriers such as naturally occurring ravines or existing developed properties that do not provide for access.



To address barriers and other limitations on non-motorized connectivity across the Island, connectivity improvements are identified in a set of figures and tables which are intended to be living documents updated as new areas are identified and considered warranted by the Public Works Department / Director.

Table 6-1 Identified barriers on SR305 and on City roadways.

Table 6-1, Roadway Network Barriers		
1	SR305 at Vineyard Lane	A separated grade crossing is needed to unite the two sides of Winslow that are divided by the SR305 superblock between Winslow Way and High School Road.
2	SR305 Signalized Crossings	Wide crossings can be a barrier to some users; As capacity improvements are made to SR305, medians, islands, and other pedestrian related improvements should be provided.
3	SR305 Shoulders	Shoulder widening is needed to address gaps between Hidden Cove Rd and the Agate Pass Bridge.
4	City Secondary arterial and collector roadways	Where pedestrians and cyclists do not exist, shoulders and/or separated pathways are needed. Many of these areas are identified for improvements shown in Map E.

### Non-Motorized Travel Routes and Network

The vision and goals for non-motorized transportation are established in the Transportation Element of the City's Comprehensive Plan. To meet the vision and mobility and connectivity goals in the Transportation Element of the Comprehensive Plan, a comprehensive network is defined in this section.

Providing facilities for accommodation of non-motorized modes of transportation has consistently ranked high on past City surveys. The City Council appointed the NMTAC to work with staff to plan and assist with the implementation of non-motorized improvements and other work related to furthering non-motorized transportation.



This section provides describes the current needs as understood at this time by the NMTAC and what the best opportunities are given geographical, existing development, and other constraints in providing for those needs.

The over-arching goal embodied in the non-motorized vision and the first non-motorized goal is to provide a network of transportation facilities that provide non-motorized modes of travel for the greatest number and widest range of the traveling public.

The NMTAC considers the following mobility challenges to be high priorities:

- A. Accommodating a wide range of non-motorized users of all ages and abilities.
- B. Providing connectivity to the ferry terminal and the Winslow.
- C. Providing safe routes to schools.
- D. Providing connectivity to designated centers and neighborhoods across the island for all modes.
- E. Improving safety for cyclists and walkers on the Island’s secondary arterial roadways.
- F. Improving usability and accessibility of sidewalks in Winslow.
- G. Removing barriers and closing gaps in networks addressing the above priorities. This includes but is not limited to SR305 and other higher volume streets.

Context sensitive solutions for non-motorized modes will depend upon site specific conditions such as existing and planned land uses, the location of origins and destinations such as schools and parks, motor vehicle speeds and volume, and the overall network connectivity.

The non-motorized transportation system is seeks to create a network of facilities that makes it safe for all ages and abilities of people to get around their neighborhoods and the island without a car. This will require facilities that will be evaluated for the particular context but may also include:

- A. Sidewalks and bicycle lanes along streets in the Island’s town centers.
- B. Separated non-motorized facilities that provide a non-motorized transportation option for a wide range of people walking, riding bikes, riding horses, or using wheelchairs. This pathway network is envisioned to connect to the City’s sidewalk and bike lane infrastructure and connect to main destinations like the ferry terminal, Agate Pass Bridge, Winslow, designated centers, schools, parks, shoreline street ends, equestrian facilities, and other amenities. These facilities will vary depending on purpose but include:



1. The Sound to Olympics (STO) trail, which serves as a centralized spine for non-motorized users and a 12-foot wide separated multi-use path connecting the Bainbridge Island Ferry Terminal to the Agate Pass Bridge and linking to other regional locations,
  2. Intra-island trails, which are 10-foot wide separated multi-use pathways to link designated centers, schools, and parks.
  3. Connecting pathways, which are 6-foot wide trails built to City standards that provide local connectivity and link to the regional and inter-island trails. Additionally the system will integrate with Bainbridge Island Metropolitan Parks District Trails that provide both intra-island and local connectivity.
- C. Road shoulders can provide connectivity for commuter and more experienced cyclists, as envisioned in the City's Core 40 Program. The Core 40 goal is to provide an integrated network of shoulders for cyclists that when combined with multi-use trails and lower volume roadways provides 40 miles of bicycle routes on the Island.
- D. On low-volume neighborhood streets, specific non-motorized infrastructure may not be necessary if vehicular speeds are low (20-25 mph).

This combination of facilities is envisioned to make up a functional network that provides connectivity to the attractions previously identified and mobility for the greatest number and widest range of users.

Sidewalks, Shoulders, Multi-use Trails, and Connecting Pathway planned facilities are identified and located in attached Maps C and D. These facilities are integrated to optimize connectivity for alternative modes of transportation for users of all ages and abilities.

In 2007 the City developed a vision for a network of shoulders to provide connectivity for cyclists across the Island. This network was named the Core 40 Network. The intent is to provide shoulder improvements on the Island's aerial roadways to achieve connectivity to 40 or more miles of roadways for cyclists; refer to Map G. Refer to Map D for identified equestrian routes.

Refer to Maps C and D for trail connection zones. Trail connection zones are identified as opposed to specificity of routes to allow flexibility. The City's past practice has been to acquire easements for trails from private property owners on a voluntary basis or when there is significant development.

Table 6-2 identifies potential connectivity for trails. The focus of this table is for regional and intra-island multi-use pathways and roadway shoulder improvements. These maps depict one set of possibilities for intra-island trails for the purposes of demonstrating connectivity that may be achieved by an integrated trail network. Some connectivity is identified for connecting pathways that are branches of regional and intra-island trails. Local connectivity is beyond the scope of what is



listed.

1	Sound to Olympics Trail Separated Grade Crossing at Vineyard Lane	A non-motorized bridge to connect the center of Winslow which is divided by SR305, requiring easements for accommodating a non-motorized bridge and its approaches.
2	Sound to Olympics Trail at Hildebrand Shopping Area	A 12-foot wide paved pathway to serve as a cross-connecting route at the north end of the designated center Winslow area.
3	Sound to Olympics Trail_north of High School Rd	A 12-foot wide paved pathway is envisioned to serve as non-motorized transportation corridor a connecting the Winslow Area north to the Agate Pass Bridge and Kitsap County. This route would connect to Transit, Schools, and Parks facilities.
4	Waterfront Trail connector at Harbor Drive	A 10-foot separated pathway to connect the Waterfront Trail to the ferry terminal. Permission is needed from WSF to use the area west of the roadway for a separated pathway.
5	Cave Avenue Trail connector	A 6-foot wide connecting pathway to connect local neighborhoods to the STO trail and the center of Winslow. Easements may be needed in the vicinity of the ravine for access from the STO trail to Ferncliff Avenue near Wing Point Way.
6	Knechtel Trail connectors	A network of 6-foot wide connecting pathways and low volume local access roadways to connect local neighborhoods to the center of Winslow and the STO trail. Easements are needed from private property owners to link local access to the roadway for east – west connection from STO trail to Weaver.
7	Schools Intra-Island Trail	A 10-foot wide paved pathway is envisioned to serve as an east to west cross connecting route at the north end of the designated area Winslow area. This route would connect to schools and parks



		facilities and also serve as a transportation corridor. Formalized routes and easements are needed from the Parks District at the “Central Park” and the School District at the High School campus and the City’s Suzuki property.
8	Wardwell Intra-Island Trail	A 10-foot wide paved pathway is envisioned to serve as a route connecting points north to the Winslow area school and parks facilities. Formalized route and easement are needed from the School District at the Middle School campus.
9.	Shepard Intra-island Trail	A network of 10-foot wide paved pathways and low volume streets along this corridor to better accommodate non-motorized use. Easements will be needed from private property owners to link local access roadway for east – west connection from Weaver to Finch.
10.	Head of the Bay shoulders and trail	6-foot wide paved shoulders are needed along this corridor. Additional right-of-way may be needed from fronting property owners to widen the roadway and mitigate for wetland impacts.
11	Bucklin Hill Road	6-foot wide paved shoulders are envisioned along this corridor. Additional right-of-way is needed to widen the roadway and drainage for shoulder improvements.
12	Lost Valley Intra-island Trail	A 10-foot wide paved pathway through the Lost Valley. The trail would provide a more direct route to the west from the Winslow area at lesser grades than surrounding road networks. Easements are needed at the east end of the proposed trail to connect through to Fletcher Bay Road.
13	Lynwood Center Intra-Island Trail	A 10-foot wide paved pathway separated from the roadway on the east side of Fletcher Bay Rd and Lynwood Center Rd. This pathway would provide non-motorized connectivity south to Lynwood Center. Easements are needed along the east side of Fletcher Bay Road.
14	North Island	A continuous trail network is envisioned connecting Wardwell road on the south end to Lovgreen Rd at



	Expeditionary Intra-Island Trail	the north along mostly unopened rights of way. This system would connect with Megs Farm Park Land trails.
15	Mandus Olson Corridor  Intra-Island Trail	A continuous network of trails and low volume roadways is envisioned to link to the Lost Valley at the south and the STO Expeditionary Trail / Lovgreen Rd at the north.

Table 6-3 identifies gaps and deficiencies in sidewalks in Winslow. This information is used to facilitate the planning of the City’s sidewalk infill program and pedestrian elements for capital improvement projects.

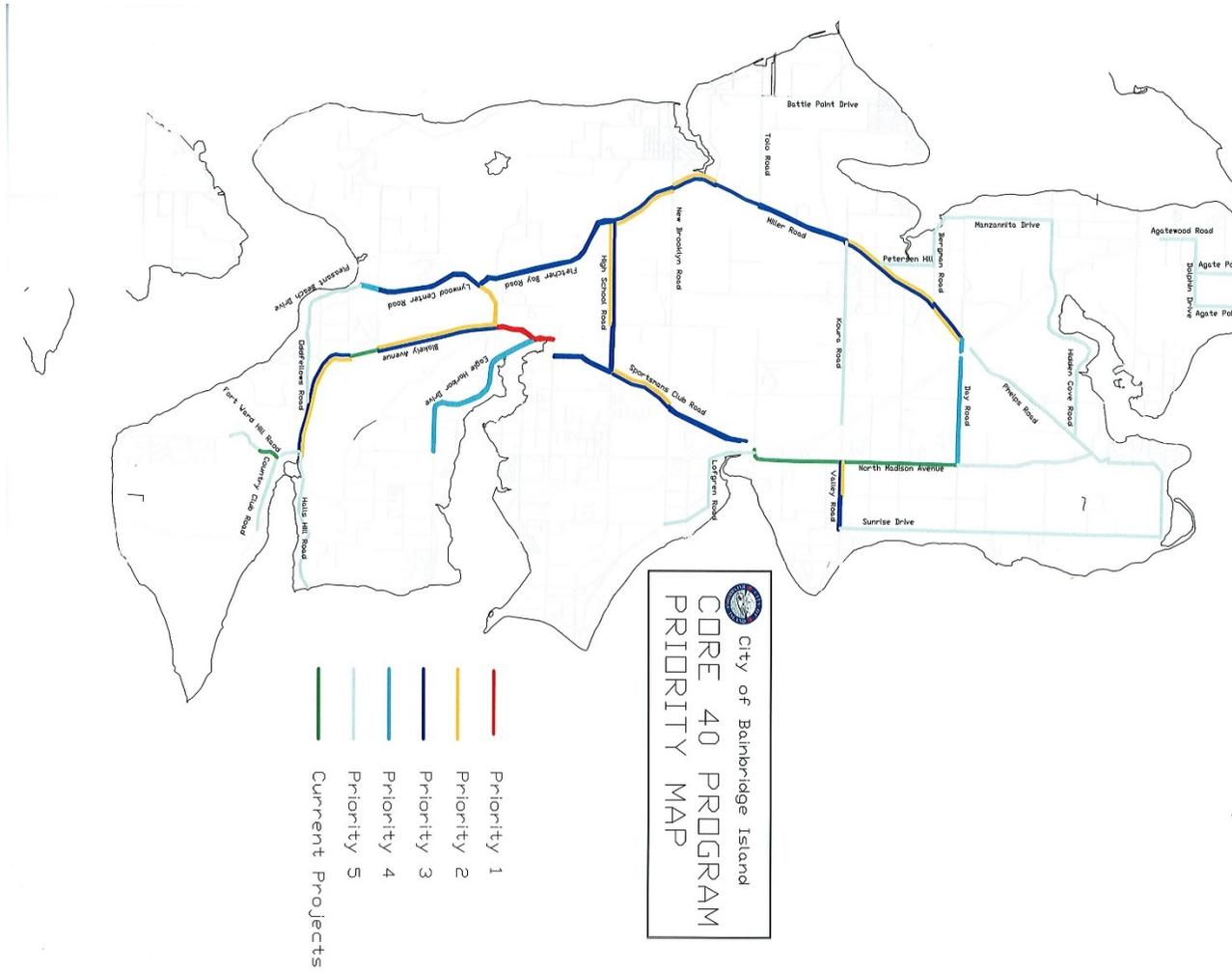
Table 6-3, Winslow Area sidewalk gaps and deficiencies		
1	Madison Avenue from Wyatt Way to High School Rd	The existing 4-foot plus wide sidewalk is not adequate to accommodate a range of users.
2	Madison Avenue from Winslow Way to Wyatt Way	Sidewalk ramps not to current standards
3	Madison Avenue from Winslow Way to Parfitt Way	Sidewalk ramps not to current standards
4	Wyatt Way from Ericksen to Madison Ave	Sidewalk needed both sides
5	Wyatt Way from Madison Ave to Lovell	Sidewalks and bike lanes needed
6	Wyatt Way from Lovell to Weaver	Sidewalk is needed on north side to fill in the current gap.



7	Winslow Way from Madison Ave to Grow Ave	Existing sidewalks are incomplete for roadway segment. Complete sidewalks are needed on both sides.
8	Grow Ave from Winslow Way to Wyatt Way	Sidewalk needed. Possible greenway (bike & ped prioritized roadway).
9	Grow Ave from Wyatt Way to High School Rd	Sidewalk needed. Possible greenway (bike & ped prioritized roadway)
10	Wood Ave from Grow Ave to Parfitt Way	Sidewalks are incomplete on both sides.
11.	Cave Avenue	Gap in sidewalk on east side.
12.	Waterfront Park Trail at Harbor Drive	The sidewalk is narrow along a steep street grade. A separated pathway on the ferry property to the east with switchbacks would improve accessibility for persons with disabilities and cyclists.
13.	Waterfront Park Bridge and approaches	The bridge needs to be widened to accommodate cyclists and resurfaced for all users.
14.	Trail from Parfitt Way to Finch Place	The existing gravel trail serves an area that is used by many senior citizens and is inconsistent in width and surfaced with gravel



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## Facility Types

The system maps identify facility types for roadway shoulders and trails. Refer to Recommended Capital Improvement Plan Maps for Regional and Intra-island trail designations.

Sidewalks are not depicted on system maps. Sidewalks are required per City Design and Construction standards in designated centers.

Shoulders are required at locations shown in system maps. Minimum shoulder widths are designated as 3-foot asphalt paved plus a one foot or greater gravel ballasted edge / curb offset distance (Type C) or 5-foot asphalt paved plus a one foot or greater gravel ballasted edge / curb offset distance (Type B).

Type B shoulders are intended to provide limited space for non-motorized users when vehicles are traveling in each direction. This facility type is best suited for roadways with low traffic volumes when the frequency of conflict is low and where drivers can most often maneuver to provide additional room for non-motorized users.

Type C shoulders are intended to provide space that is adequate to accommodate cyclists riding with traffic and pedestrians walking facing traffic.

Trails: Regional Trails, Intra-island Trails, and some Connecting Pathways are shown in system plan maps. Connecting Pathways may be required in locations not depicted in the system plan maps to preserve existing connectivity or provide connectivity to facilities. The City's minimum trail width is 6-feet wide. Where Type A facilities (Regional Trails, Inter-island Trails) are designated 10-foot-wide trails minimum plus 1 foot or greater ballasted shoulders. All trail facilities are to be hard surfaced. Trails along roadways should be separated from the vehicular traveled way.

## Levels of Service

Bicycle Level of Service (BLOS) and Pedestrian Level of Service (PLOS) are established for each of the facility types for Secondary Arterial Streets and high Volume Collector Streets over 1500ADT with posted speeds up to 35mph..

Table 6-4, Non-motorized LOS Guideline

LOS	Description
A	Separation from vehicular modes that is comfortable for the majority of users. Minimum 7 feet of separation or curb with 3 feet of separation..



B	Separation from vehicle modes that may not be comfortable for some users. Minimum curb or two feet of separation.
C	Space provided for non-motorized mode. Meets AASHTO minimums.
D	Space provided for non-motorized mode but may be sub standard and not considered a non-motorized facility.

Table 6-5, Non-Motorized Level of Service for Designated Centers		
Facility Description	BLOS	PLOS
10-foot wide multi-use pathway separated 7 or more feet from the roadway or separated by physical barrier	A	A
6-foot wide trail separated 7 or more feet from the roadway	C	A
5-foot wide sidewalk or trail with curb and gutter and planter strip 3 or more feet wide	N/A	A
5-foot wide sidewalk	N/A	B
5-foot wide paved shoulder w/ 2 foot buffer	B	C
5-foot wide paved shoulder (6 foot total width)	C	C

Table 6-6, Non-motorized Levels of Service for Conservation Areas		
Facility Description	BLOS	PLOS
10-foot wide multi-use pathway separated 7 or more feet from the roadway or separated by physical barrier	A	A
6-foot wide trail separated 7 or more feet from the roadway	C	A



5-foot wide paved shoulder w/ 2 foot buffer	B	C
5-foot wide paved shoulder (6 foot total width)	C	C
3-foot wide shoulder *	N/A	D
6-foot wide shoulder	N/A	C
8-foot wide shoulder	N/A	B

\* 3 foot shoulders are not intended as a non-motorized facility but may provide space to avoid run out into a ditch or vegetation for non-motorized users, as well as recovery for vehicular traffic.



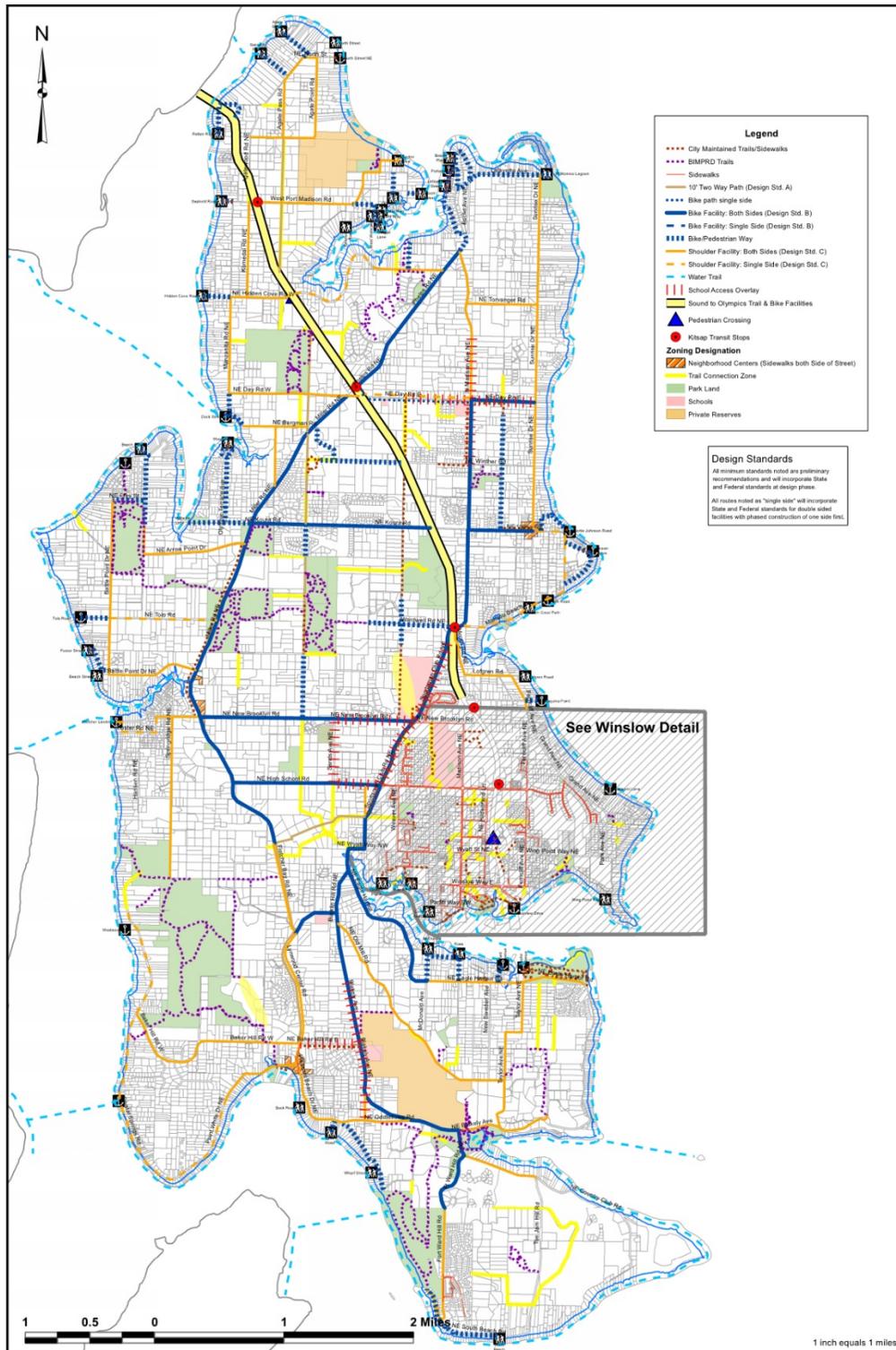
## Frontage and Other Required Improvements

Non-motorized improvements are required along with other infrastructure improvements for all development. The following table identifies the level of improvements required that have been determined to be roughly proportional with the scale of development.

Table 6-7, Improvement Requirements	
Development Type	Required facilities:
Development or re-development of a residential lot.	ROW dedication and easements. Sidewalk and shoulder infill and reconstruction to meet current standards.
Short Plats 2 to 4 lots in size, multi-family development exceeding 4 units, and all other development / re-redevelopment.	In addition to the above, the construction of sidewalk and shoulder extensions, and construction or reconstruction of trails up to 6 feet in width.
Long Plats of 5 lots or more and development of all other properties greater than 20,000 gross building square feet in aggregate.	In addition to above, the construction or reconstruction for all facilities including multi-use trails.

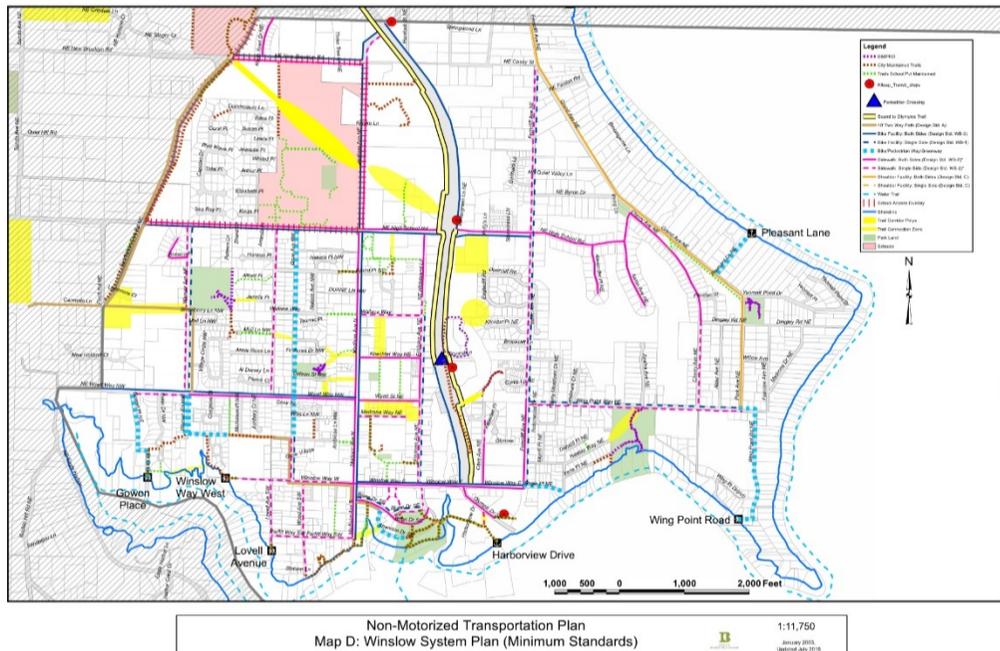


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Non-Motorized System Plan  
 Map C: (Minimum Standards)

January 2003 Updated July 2016



### Implementation, Prioritization, and Funding

This section elaborates on specific measures to further the non-motorized implementation goals in the Transportation Element of the Comprehensive Plan.

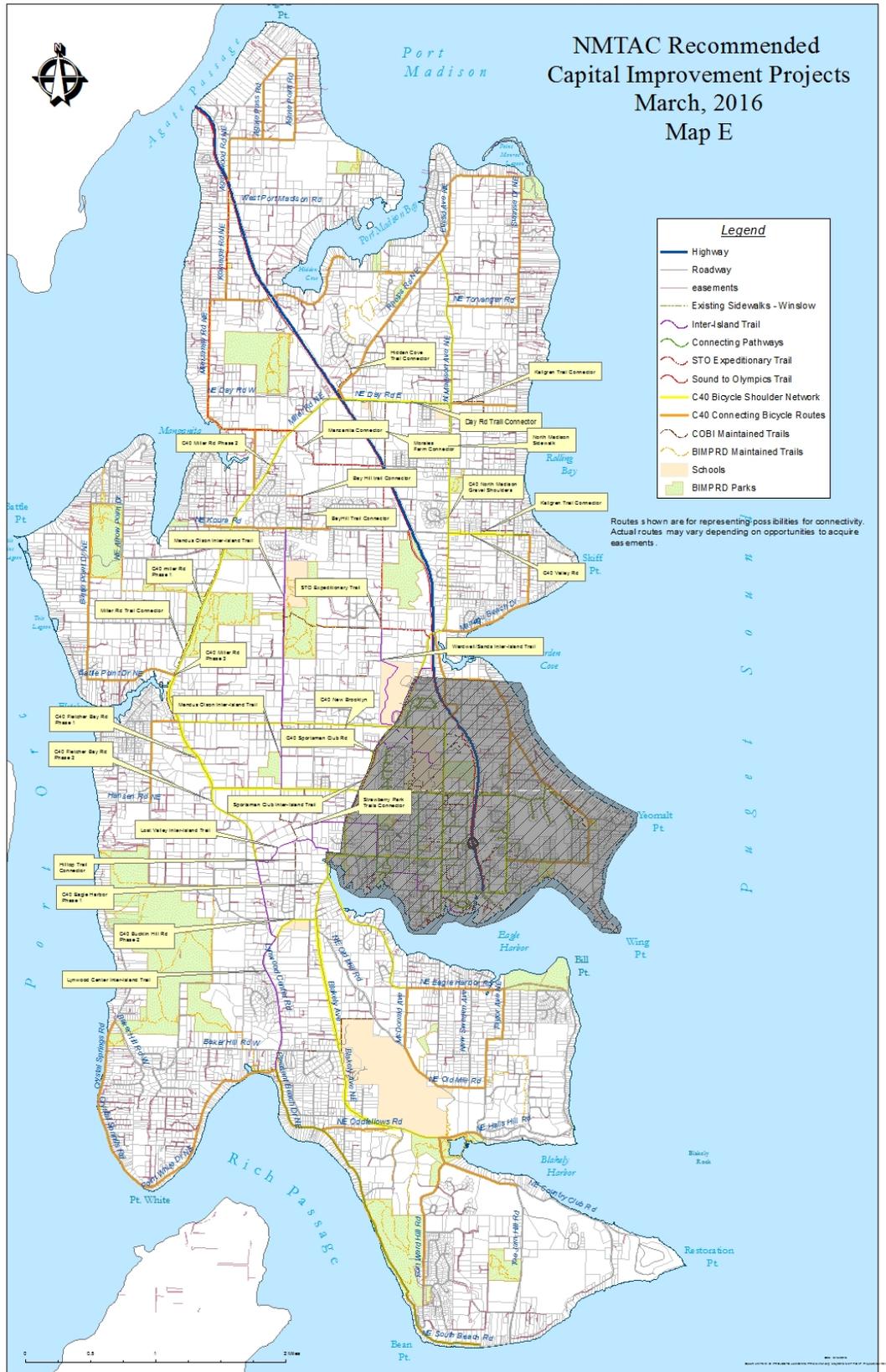
- A. As opportunities are identified, develop proposals to update the Municipal Code to increase the ability to obtain non-motorized facilities in accordance with the IWTP and consistent with the goals in the Transportation Element of the Comprehensive Plan with non-motorized projects.
- B. Support community efforts to develop new regulations requiring the construction of non-motorized facilities by development.
- C. All commercial and residential projects that reach the design and review thresholds set in the Municipal Code shall be reviewed for compliance with the goals, policies, and standards in the Transportation Element of the Comprehensive Plan, the Islandwide Transportation Plan and other adopted Plans.
- D. Facilitate the NMTAC review of development projects with potential for non-motorized elements and provide opportunity for early input in designs.
- E. As properties develop, secure right of way dedication for frontage improvements on City streets and easements for regional and intra-island multi-use trails (20 feet or more) and connecting pathways within and between neighborhoods (15 feet or more).



- F. Support opportunities to secure new easements or renegotiate existing easements (example: utility access agreements).
- G. Provide mechanisms for funding, prioritizing, and implementing projects to develop non-motorized facilities identified in this plan. Identify and prioritize specific non-motorized projects in the City's transportation planning including but not limited to the IWTP and the Capital Facilities Plan to assure their completion.
- H. Actively pursue various funding sources, such as available grant and bond initiatives for priority projects. Pursue joint funding opportunities with the School District, Parks District, and Department of Transportation. Provide flexibility in the program as needed to be competitive.
- I. Support the development of a non-motorized bond measure to fund regional and intra-island trails, Core 40 shoulder improvements, and other island-wide non-motorized improvements.
- J. Support involvement of the NMTAC in transportation planning and capital improvement planning. Important aspects of this work include developing and prioritizing projects, and collaborating to develop grant applications and secure funding.
- K. Support involvement of the NMTAC in public outreach and the development of transportation improvement projects.
- L. Incorporate non-motorized improvements into capital improvement projects. Consideration to be given to the context of each site in developing designs.
- M. Study maintenance needs and include budget recommendations in Operations and Maintenance to provide for new facilities and improved level of service of all facilities.

### **Non-Motorized Improvement Plan**

Programs and projects to achieve the proposed Non-motorized Transportation System Plan are identified in Maps E and F.







significant safety enhancement. Examples include the use of sharrows adjacent to angle parking and at transition areas from bike lanes to shared lanes on Winslow Way.

- E. Provide separation for non-motorized from vehicular uses at higher speed (over 30mph) and higher volume (over 2000 ADT) motorized traffic locations. When separation is not practical, alternative routes should be provided to accommodate users of all ages and abilities. A particular emphasis for separated facilities is on roads connecting to schools and along SR305.
- F. Consider lowering speed limits of secondary and collector street with significant bicycle and or pedestrian traffic that lack non-motorized facilities.
- G. Postwalking and biking warning signs on roadways in high non-motorized use areas lacking adequate facilities.
- H. Incorporating traffic calming elements such as narrow lanes (9-10 feet depending on roadway classification), chicanes or winding roadways, and maintaining native vegetation or providing street trees in all designs. Consider speed humps, and / or raised crosswalks at local accessstreets with a desired speed limit of 20mph when there are large vehicular traffic generators or very high volumes of pedestrians.
- I. Provide street lighting of secondary arterials and collector streets in designated centers and marked crosswalks on arterial streets.
- J. Provide bicycle-activated sensors at signal locations.
- K. Avoid placement of utility facilities, such as manhole covers and utility poles, within non-motorized travelways.
- L. Design of new parking lots and garages to include covered bike storage / parking facilities. Where existing bicycle parking is sufficient and conveniently located, the City Engineer may omit this requirement.
- M. When bike racks are required for commercial development and public facilities, the racks shall be conveniently located to the building entrance, appropriately designed to be compatible with the design and development of the site, and sheltered from inclement weather.



## Standards

The City’s existing Design and Construction Standards were developed in 1997 and have not been updated to include all of the non-motorized elements identified in the 2003 Non-Motorized Transportation Plan. It is recommended that this document be updated following the update of the Island-Wide Transportation Plan and the City’s Comprehensive Plan.

Refer to the table below showing a list of considerations for updating the Design and Construction Standards.

Table 6-8, Standards Recommendations	
Standards 1	Maintain narrow 10-foot lanes on major roadways.
Standards 2	Require pedestrian facilities to be maintained at grade at driveway entrances.
Standards 3	Require sidewalks to be built to the back of the right-of-way along arterial and collector streets.
Standards 4	Require planter strips for increased pedestrian separation from traffic
Standards 5	Minimum bike lane width on secondary arterial and major collectors is 5 feet. An additional one-foot clearance of the curb to be provided at curb and gutter locations. Consider buffered bike lanes.
Standards 6	Require paved driveway approaches at all driveways serving more than 3 households for all categories of projects.
Standards 7	Develop standards for shared use path, buffered separated multi-use path, intra-island trail, etc.
Standards 8	Utility structure covers are to be located out of the sidewalk and shoulders used by cyclists unless impractical and any deviation requires approval by the City Engineer. Covers to have flush, skid, and lock down characteristics suitable for cycle use.
Standards 9	Tenant improvements and remodels trigger frontage improvements to meet current ADA standards.



### **Preservation and Maintenance**

Existing and future non-motorized facilities need to be preserved and maintained to ensure continued usefulness. As the system grows, so does the demand for resources to maintain it. Facilities deteriorate over time and the City needs to plan for expenditures to repair and / or reconstruct these assets.

Areas of emphasis for maintenance:

- o Annual raised sidewalk grinding or replacement of sidewalk panels to address deficient disability access.
- o Annual sidewalk and cross walk power washing where needed to maintain slip resistance and contrasting color.
- o Monthly sweeping of separated pathways.
- o Annual cleaning of separated pathways.
- o Seasonal brush cutting of trails.
- o Annual grading and graveling of unpaved trails where needed to address unevenness and traction issues.
- o Maintenance of roadway surfacing to consider serviceability of shoulders for cyclists when prioritizing repairs.
- o Trimming of roadside brush to maintain use of shoulders for cyclists and pedestrians.
- o Monthly shoulder / bike lane sweeping with higher frequency at problem areas.
- o Repair and adjustment of lids and grates to maintain even surfaces for cyclists and pedestrians.
- o Annual pavement marking maintenance of cross walks, bike lane symbols, and other surface markings.
- o Washing and replacement of signage such as no-parking signs, way finding signs, and others.



## Education, Encouragement and Enforcement

The NMTAC, supported by City Public Works, Planning, and Police Staff, and in coordination with School District, Parks District, Fire District, Kitsap County Health District, and community groups, will work to further the education goals of this Plan. This includes developing programs, or adopting programs used successfully elsewhere, to encourage use of non-motorized modes and promote safety.

- Listen to the community to identify transportation system deficiencies and opportunities for improvement
- Coordinate and support programs and projects that encourage active modes of transportation
- Support community outreach and involvement for the development of transportation projects
- Support safe routes to school programs
- Support “Adopt-a-Trail” and “Adopt a Route” programs
- Develop and distribute guide maps and provide wayfinding signage. Public non-motorized facilities such as trails should be identified with signage in order to designate routes and access points. This is especially important where facilities are adjacent to or run through easements on private property.

The NMTAC and City routinely support the following efforts:

- ‘Bainbridge Shares the Road’ program and signage.
- League of American Bicyclists ‘bicycle friendly community’ designation.
- [Walking, Cycling, and Paddling Map](#) supported on the City’s web site.
- [Walking Map of Winslow](#), produced by Sustainable Bainbridge and supported on the City’s website.
- Map of accessibility features in the Winslow area, produced in cooperation with the Kitsap County Accessible Communities Advisory Committee.
- Participating in ‘Bike to School Day’.
- Community engagement for connectivity opportunities and easements.



- Participating in public outreach involvement opportunities for City transportation projects.
- Coordinating with the Police Department to identify areas with higher non-motorized use that may need education and enforcement emphasis for safety due to collision history, speeding, observed poor behaviors by either motorized and/or non-motorized users.
- Promoting police bicycle patrols for enforcing laws for cyclists and patrolling multi-use pathways.

**ORDINANCE NO. 2016-28**

**AN ORDINANCE** of the City of Bainbridge Island, Washington, amending Bainbridge Island Municipal Code Titles 1, 2, 12, 13, 15 and 18 to adopt the state-required LID regulations that will require all development to meet the updated Department of Ecology (DOE) Stormwater Management Manual.

**WHEREAS**, the City must adopt state-required LID regulations that will require all development to meet the updated DOE Stormwater Management Manual by December 31, 2016; and

**WHEREAS**, the City contracted with a consulting firm, Herrera, to assist the City in identifying which sections of the municipal code would need to be updated to meet DOE requirements; and

**WHEREAS**, the City Council amended the purpose of the Ad Hoc Tree Committee to include review of Low Impact Development regulations; and

**WHEREAS**, the Planning Commission discussed Ordinance No. 2016-28 at a study session on October 27, 2016 and held a public hearing on **XXXX**, 2016; and

**WHEREAS**, the City Council discussed Ordinance No. 2016-28 at a study session on **XXXX** and held a public hearing on **XXXX**, 2016; and

**WHEREAS**, notice was given on **XXXX**, 2016 to the Office of Community Development at the Washington State Department of Commerce in conformance with RCW 36.70A.106;

**NOW THEREFORE, THE CITY COUNCIL OF THE CITY OF BAINBRIDGE ISLAND, WASHINGTON, DOES ORDAIN, AS FOLLOWS:**

**Section 1.** Section 1.28.010 of the Bainbridge Island Municipal Code is amended to read as follows:

1.28.010 Miscellaneous fees charged.

A. The city shall charge an appropriate fee in the amount established by the city by resolution for the following services provided or permits issued by the city:

1. Boundary line adjustment review;
2. Bulkheads and seawalls – repairs and new bulkheads;
3. Forest practices review;
4. Grading permits;
5. Clearing permits;
6. Open space review;
7. Preapplication conferences (this fee is not refundable);

8. Public dance permits;
9. Visa/citizen/immigration document processing;
10. Escheat transaction processing;
11. Notary services;
12. Concealed weapons permit duplicates;
13. House moving permits;
14. Written reports to escrow companies;
15. Reports to insurance companies;
16. Fingerprint card processing;
17. Administrative code interpretation;
18. Buffer reduction or averaging review;
19. Land use consultation;
20. Vegetation management plan review;
21. Planned unit development applications; ~~and~~
22. Extensions of planned unit development applications;:-
- [23. Stormwater site plan review fees;](#)
- [24. Construction inspection fees; and](#)
- [25. Post construction stormwater facility inspection fees.](#)

**Section 2.** Section 2.16.020.Q.4.b of the Bainbridge Island Municipal Code is amended to read as follows:

- b. Innovative Site Development. Evaluation will review:
  - i. Water Quality and Conservation. Projects use methods to decrease water usage and improve stormwater runoff quality through an integrated approach to stormwater management such as greywater use, stormwater collection in cisterns, ~~green~~ vegetated roofs and covered parking. All HDDP projects will follow the Department of Ecology’s 2012 Stormwater Management Manual for Western Washington, as amended in December 2014.

**Section 3.** Table 2.16.020.Q-3 of the Bainbridge Island Municipal Code is amended to read as shown in Exhibit A.

**Section 4.** Section 2.16.040.D of the Bainbridge Island Municipal Code is amended to read as follows:

D. Design Process.

1. Site Analysis.
  - a. An applicant shall first conduct a site analysis identifying existing watercourses/wetlands, significant trees and vegetation, critical areas and other natural features, and open space in accordance with the design process, and development standards of [BIMC 15.20](#) and BIMC 18.12.020 if applicable.

b. An applicant for a site plan and design review proposal shall prepare maps, site plan(s) and studies (as specified in the submittal requirements) to show how the proposal promotes the purpose and meets the standards of the zoning district and chapter.

**Section 5.** Section 2.16.040.E of the Bainbridge Island Municipal Code is amended to read as follows:

E. Decision Criteria. The director and planning commission shall base their respective recommendations or decisions on site plan and design review applications on the following criteria:

1. The site plan and design is in conformance with applicable code provisions and development standards of the applicable zoning district, unless a standard has been modified as a housing design demonstration project pursuant to BIMC 2.16.020.Q;
2. The locations of the buildings and structures, open spaces, landscaping, pedestrian, bicycle and vehicular circulation systems are adequate, safe, efficient and in conformance with the nonmotorized transportation plan;
3. The Kitsap County health district has determined that the site plan and design meets the following decision criteria:
  - a. The proposal conforms to current standards regarding domestic water supply and sewage disposal; or if the proposal is not to be served by public sewers, then the lot has sufficient area and soil, topographic and drainage characteristics to permit an on-site sewage disposal system.
  - b. If the health district recommends approval of the application with respect to those items in subsection E.3.a of this section, the health district shall so advise the director.
  - c. If the health district recommends disapproval of the application, it shall provide a written explanation to the director.
4. The city engineer has determined that the site plan and design meets the following decision criteria:
  - a. The site plan and design conforms to regulations concerning drainage in Chapters 15.20 and 15.21 BIMC; and
  - b. The site plan and design will not cause an undue burden on the drainage basin or water quality and will not unreasonably interfere with the use and enjoyment of properties downstream; and
  - c. The streets and pedestrian ways as proposed align with and are otherwise coordinated with streets serving adjacent properties; and

- d. The streets and pedestrian ways as proposed are adequate to accommodate anticipated traffic; and
  - e. If the site will rely on public water or sewer services, there is capacity in the water or sewer system (as applicable) to serve the site, and the applicable service(s) can be made available at the site; and
  - f. The site plan and design conforms to the “City of Bainbridge Island ~~Engineering-Design and Development-Construction Standards Manual~~,” unless the city engineer has approved a variation to the road standards in that document based on his or her determination that the variation meets the purposes of BIMC Title 18.
5. The site plan and design is consistent with all applicable design guidelines in BIMC Title 18, unless strict adherence to a guideline has been modified as a housing design demonstration project pursuant to BIMC 2.16.020.Q;
  6. No harmful or unhealthful conditions are likely to result from the proposed site plan;
  7. The site plan and design is in conformance with the comprehensive plan and other applicable adopted community plans;
  8. Any property subject to site plan and design review that contains a critical area or buffer, as defined in Chapter 16.20 BIMC, conforms to all requirements of that chapter;
  9. Any property subject to site plan and design review that is within shoreline jurisdiction, as defined in Chapter 16.12 BIMC, conforms to all requirements of that chapter;
  10. If the applicant is providing privately owned open space and is requesting credit against dedications for park and recreation facilities required by BIMC 17.20.020.C, the requirements of BIMC 17.20.020.D have been met;
  11. The site plan and design has been prepared consistent with the purpose of the site design review process and open space goals;

~~12. For applications in the B/I zoning district, the site plan and development proposal include means to integrate and re-use on-site storm water as site amenities.~~

**Section 6.** Chapter 12.38 of the Bainbridge Island Municipal Code is amended to read as follows:

12.38.010 Definitions.

A. “Right(s)-of-way (ROW)” means the public property limits, whether in fee simple or easement, identified for public use and/or facilities.

B. “Roadway” means the road wearing surface, including shoulders, ~~and any drainage system~~ conveyance system, and stormwater treatment and/or flow control facilities (in

[accordance with BIMC 15.20](#)) constructed to protect the adjoining properties and the road base.

C. “Road” means the wearing surface only.

D. “Minimum maintenance” means the grading of unpaved roads, to be performed a maximum of twice a year, within budgetary constraints. (Ord. 2003-22 § 15, 2003; Ord. 94-11 §§ 1, 10, 1994)

12.38.020 Existing public right-of-way.

Maintenance of existing public ROW shall be as follows:

A. Paved Roads [and Permeable Pavement Roads](#). The city will maintain all paved [and permeable pavement](#) roads in accordance with city procedures and within budgetary constraints.

B. Unpaved Roads. The city will perform minimum maintenance on unpaved roads in accordance with city procedures and within budgetary constraints. (Ord. 2001-29 § 1, 2001; Ord. 94-11 § 2, 1994)

[C. Refer to BIMC 15.20 for pavement maintenance exemptions to determine if stormwater requirements will be triggered.](#)

12.38.030 Upgrading unpaved roads.

Upgrading of unpaved roads in public right-of-way, at the request of the adjoining property owners, shall be done through the local improvement district (~~LID~~) process, property owner funding, or with other sources of funds. [Refer to BIMC 15.20 for the pavement maintenance exemptions to determine if stormwater requirements will be triggered. Permeable pavement is preferred where feasible for upgraded surfaces.](#) Upon completion of the upgrade project to city standards, the city will accept the road for perpetual maintenance in accordance with BIMC 12.38.020.A. (Ord. 2001-29 § 2, 2001; Ord. 94-11 § 3, 1994)

**Section 7.** Title 13 of the Bainbridge Island Municipal Code is amended to read as shown in Exhibit B.

**Section 8.** Title 15 of the Bainbridge Island Municipal Code is amended to read as shown in Exhibit C.

**Section 9.** Section 18.06.060 of the Bainbridge Island Municipal Code is amended to read as follows:

C. Natural Resource Protection Standards. The following provisions are intended to supplement those natural resource protection standards and requirements contained in BIMC Titles 16 (Environment) and 17 (Subdivisions), and specifically to supplement those provisions in Chapters 16.12 (Shoreline Master Program) and 16.20 BIMC (Critical Areas), which remain the primary source of regulation in those areas. In the event of a conflict between the provisions of this section and the provisions of BIMC Title 15

(Buildings and Construction) or Chapter 16.12 or 16.20 BIMC, the provisions of BIMC Title 15 or 16 shall apply.

1. Drainage. Surface and storm water shall be managed in accordance with ~~the management standards in Chapter BIMC 15.20 BIMC. Storm water runoff shall be detained and disposed of on site or disposed of in a system designed for such runoff and that does not flood or damage adjacent properties. Systems designed for runoff detention and control shall comply with specifications provided by the city and shall be subject to its review and approval, and shall, moreover, comply with Chapter 15.20 BIMC, pertaining to community facilities.~~

**Section 10.** Section 18.06.070 of the Bainbridge Island Municipal Code is amended to read as follows:

B. Performance Standards. All uses allowed in the WD-I district shall conform to the performance standards listed in BIMC 18.06.050 as well as the following:

1. Pollution. Pollution prevention and water quality protection shall be required of all development and operations of facilities that are located within the shoreline jurisdiction by employing current best management practices and best available facilities practices and procedures for marine facilities ~~provided by the Washington State Department of Ecology~~ in accordance with BIMC 15.20 and 15.22.

2. Noise. See BIMC 18.06.030.B.1.

3. Lighting. Lighting standards set forth in BIMC 18.15.040 shall apply to the WD-I district. (Ord. 2011-02 § 2 (Exh. A), 2011)

**Section 10.** Section 18.09.030.J of the Bainbridge Island Municipal Code is amended to read as follows:

7. Temporary Seasonal Sales.

a. Temporary outdoor sales are permitted in any zone district subject to compliance with this section.

b. A temporary use permit for temporary outdoor sales shall not exceed a term of 30 consecutive days, and only two permits within a calendar year may be granted for each principal permitted or approved conditional use.

c. All temporary outdoor sales activity shall comply with the following requirements:

i. The location of the temporary sales activity shall allow customers to drive into an existing off-street parking area. No temporary outdoor sales may interrupt the flow of traffic on public streets or access ways into a shopping area.

ii. The applicant shall demonstrate there will be adequate parking for the existing use as well as the temporary outdoor sales. The director may modify this requirement if the applicant can otherwise demonstrate that adequate parking for the existing and proposed uses will be provided.

iii. The fire department shall approve all proposals for temporary outdoor sales from a tent.

iv. The area occupied by the temporary sales activity, plus any adjacent clear area required by the fire code, shall occupy no more than 20 percent of any required off-street parking spaces or area.

v. In the MUTC, HSR, NSC, and B/I districts, all trucks or tents and associated parking shall be located on asphalt, concrete, permeable pavement, or an equivalent surface unless the applicant demonstrates no adverse effect on drainage, access, or the intent of this code, as determined by the director.

**Section 11.** Table 18.12.040 of the Bainbridge Island Municipal Code is amended to read as follows:

**Table 18.12.040: Permitted Setback/Height Modifications**

Type of Encroachment	Encroachment Permitted	Conditions
<b>Permitted Setback Modifications</b>		
Fence or combined fence and berm up to 6 feet high	In any required setback subject to applicable regulations in BIMC Title 15	Except as provided in BIMC 18.12.030.F.2, BIMC 18.12.040.B, and Chapter 16.12 BIMC
Nonscreening fences or combined nonscreening fence and berm up to 8 feet high	In any required setback subject to applicable regulations in BIMC Title 15	Except as provided in Chapter 16.12 BIMC
Chimneys, flues, awnings, bay windows, and greenhouse windows	Up to 18 inches into any required setback	
Covered porches, bay windows and eaves within the Ericksen Avenue overlay district	Up to five feet into the front yard	Bay windows must be cantilevered outward from the wall, and may not result in any portion of the building floor area extending into the setback
Any structures, including but not limited to uncovered steps, porches, and decks less than or equal to 30 inches in height	Up to 2 feet into front and side setbacks. Up to 5 feet into required rear setbacks.	
Eaves	May extend up to 24 inches in any required setback except shoreline structure setback	
At or near grade structures such as uncovered patios, sidewalks, and driveways	In any required setback	May not exceed 4 inches in height
Signs	In any required setback	Must conform to Chapter 15.08 BIMC
Utilities accessory to a single-family residence	In any required setback	
Composting bins	In side or rear setback areas	
<u>Rain garden/swales</u> <u>Bioretention/ rain gardens</u>	<del>In side or rear</del> <u>In any required setbacks</u>	<u>In accordance with BIMC 15.20.</u>
<u>Rainwater harvesting barrels</u> <u>Rain barrels/ cisterns</u>	<del>In side or rear setbacks</del> <u>In any required setbacks</u>	<u>In accordance with BIMC 15.20.</u>

Type of Encroachment	Encroachment Permitted	Conditions
Wall-mounted on-demand hot water heaters	Up to 18 inches into side or rear setbacks	Permitted if buffered or enclosed to prevent noise impacts to neighboring properties
Below-ground geothermal equipment	In any required setback	Permitted if any excavated areas are promptly re-landscaped after installation is complete
Rockeries and retaining walls less than 4 feet in height	In any required setback	Qualified geotechnical engineer determination, and city concurrence, that it is necessary for slope stabilization
<b>Permitted Height Modifications</b>		
Small wind energy generators	Up to 18 in. above the maximum building height in the district	
Solar panels	Up to 18 in. above the maximum building height in the district	
Noncommercial, nonparabolic antennas affixed to noncommercial communication towers	Up to 50 feet in height above grade	
One flagpole per parcel	Up to 45 feet in height above grade	
Distribution utility poles	Up to 55 feet in height above grade	Replacement poles over 55 feet in height, see BMC 18.09.030.F.2.b. For new distribution utility facilities or corridors, see Table 18.09.020. Poles shall not be moved more than 20 feet from the original location unless permitted under BMC 18.09.030.F.2.b.
Transmission utility poles	Up to a 25 percent increase above existing pole height above grade with a maximum height of 100 feet	Replacement poles over the 25 percent increase or 100 feet in height, see BMC 18.09.030.F.2.b. For new transmission utility facilities or corridors, see Table 18.09.020. Poles shall not be moved more than 20 feet from the original location unless permitted under BMC 18.09.030.F.2.b.
Utility structures existing on the effective date of the ordinance codified in this subsection	Existing height	May also be replaced or modified; provided, that the structure is not larger or taller than the original structure and is not moved more than 20 feet from its original location

**Section 12.** Section 18.12.050 of the Bainbridge Island Municipal Code is amended to read as follows:

K. Lot Coverage. “Lot coverage” means that portion of the total lot area covered by buildings, excluding up to 24 inches of eaves on each side of the building, any building or portion of building located below predevelopment and finished grade. Any portion of a slatted or solid deck located more than five feet above grade shall be counted towards lot coverage. Also excluded are ground-mounted accessory small wind energy generators, solar panels, composting bins, rainwater harvesting barrels/cisterns, and covers designed to shade ground-mounted heat pumps and air conditioners to increase their efficiency.

**Section 13.** Section 18.15.010.A of the Bainbridge Island Municipal Code is amended to read as follows:

18.15.010 Landscaping, screening, and tree retention, protection and replacement. All development shall comply with the following regulations addressing landscaping and screening unless other applicable regulations require additional or different forms of landscaping or screening, in which case the more specific standard or criteria shall govern.

A. Purpose.

1. General. The purpose of this section is to preserve the landscape character of the community, link the Island's natural amenities with landscape greenbelts along scenic roads, improve the aesthetic quality of the built environment, promote retention and protection of existing vegetation, reduce the impacts of development on wetlands, streams and the natural environment, enhance the value of current and future development and increase privacy for residential zones, and encourage preservation of significant and heritage trees by:

- a. Retaining existing vegetation, tree stands and significant trees by incorporating them into the site design.
- b. Incorporating native vegetation and drought resistant plant material into new landscape developments.
- c. Providing vegetated screening between different intensities of residential uses, and between development and roads.
- d. Providing visual relief of parking areas in the neighborhood service centers, the Winslow Mixed Use Town Center, and the light manufacturing, (water-dependent) industrial, High School Road and urban multifamily districts.
- e. Providing vegetated screening between residential and nonresidential areas.
- f. Preserve, protect, and enhance critical areas.
- g. Protect the natural forested areas.

2. Specific Zone Districts.

- a. For single-family residential short plats and subdivisions in residential districts, the additional specific intent is to ~~preserve, protect and enhance critical areas, protect the natural forested areas and~~ preserve the greenbelts along designated scenic roadway corridors.
- b. In the R-8 and R-14 multifamily residential districts, the additional specific intent is to screen urban multifamily projects from adjacent lower density residential properties and to soften the appearance of surface parking areas.

c. For nonresidential uses outside the Winslow Mixed Use Town Center, High School Road I and II, NSC, B/I, and WD-I districts, the additional specific intent is to retain the natural landscape qualities of the Island by retaining existing vegetated buffers to screen views of structures and parking areas and to buffer between areas of high and low intensity uses.

d. In the Winslow Mixed Use Town Center central core and ferry terminal overlay districts, the additional specific intent is to provide an urban character by incorporating landscape standards; and to provide landscape development to screen uses from single-family residential properties and to soften the appearance of surface parking areas.

e. In the Winslow Mixed Use Town Center, Ericksen Avenue and Madison overlay districts, the additional specific intent is to retain the character of landscape front yards; and to provide landscape development to screen uses from single-family residential properties; and to soften the appearance of surface parking areas.

f. In the Winslow Mixed Use Town Center gateway overlay district, the additional specific intent is to retain the greenbelt located adjacent to SR 305 consistent with the greenways plan and to provide landscape development to screen uses from single-family residential properties.

g. In the High School Road I and II districts, the additional specific intent is to provide landscape development to screen uses from adjacent single-family residential properties and to soften the appearance of surface parking areas.

h. In the NSC district, the additional specific intent is to incorporate landscape standards that support pedestrian scale neighborhood uses compatible with the intensity of the surrounding residential neighborhood; to minimize the impact of lighting, noise and views of surface parking areas; and to provide a buffer between higher and lower intensity uses.

i. In the B/I district, the additional specific intent is to provide a year-round vegetated screen and a noise and site lighting buffer of industrial development from adjacent nonindustrial properties and roadways.

j. In the WD-I district, the additional specific intent is to provide landscape development that screens parking lots and large structures, but allows visual access to the shoreline and small scale active industrial facilities.

**Section 14.** Section 18.15.010.C of the Bainbridge Island Municipal Code is amended to read as follows:

4. Protection During Construction and Development.

a. Intent. The intent of these regulations is to provide the best protection for existing vegetation, trees and tree stands, including protection for trees on adjacent properties, [protect LID BMPs during construction and development](#)

activities, and to preserve the ecological function of the landscaping area by protecting existing soil.

b. Requirements.

i. No cutting of trees shall be allowed on a site until the tree retention and planting plans have been approved by the director and a clearing, grading or building permit issued.

ii. In order to preserve future ecological function, the applicant shall identify areas of prohibited disturbance, generally corresponding to the dripline or critical root zone (as identified by a consulting arborist) of the existing vegetation, trees and/or tree canopy of tree stands to be retained, buffers, areas of existing vegetation to be maintained, future rain gardens, and future planting areas larger than 400 square feet (i.e., landscape islands in parking lots). The prohibited disturbance areas shall be reviewed and approved by the director as part of the land use permit review process.

iii. A temporary five-foot-high chain link fence with tubular steel poles or “T” posts shall delineate the area of prohibited disturbance defined in subsection C.4.b.ii of this section, unless the director has approved the use of a four-foot-high plastic net fence as an alternative. The fence shall be erected and inspected by city staff before clearing, grading and/or construction permits are issued and shall remain in place until construction has been completed, and shall at all times have affixed to it a sign indicating the protected area.

iv. No impervious surfaces, fill, excavation, vehicle operations, compaction, removal of native soil or storage of construction materials shall be permitted within the area defined by the required construction fencing. If avoiding construction and compaction in future planting areas is unavoidable, the landscape plan for the project shall include methods for aerating and/or augmenting compacted soil to prepare for new planting, pursuant to subsection H.2 of this section.

v. A rock well shall be constructed if the grade level around the tree is to be raised more than one foot. The inside diameter of the well shall be equal to the diameter of the dripline or critical root zone (as identified by a consulting arborist) of the tree or tree canopy of tree stands.

vi. The grade level shall not be lowered within the larger of (A) the dripline or critical root zone (as identified by a consulting arborist) of the tree, or the tree canopy of tree stands, or (B) the area recommended by a consulting arborist.

vii. Alternative protection methods may be used if recommended by a consulting arborist and determined by the director to provide equal or greater tree protection.

viii. Wherever this subsection C.4 allows or requires the involvement of a consulting arborist, that individual shall be selected from the city’s list of

current arborists certified by the American Society of Consulting Arborists and his or her services shall be paid for by the applicant.

ix. LID BMPs shall be protected during construction and development activities in accordance with BIMC 15.20.

**Section 15.** Section 18.15.010.H of the Bainbridge Island Municipal Code is amended to read as follows:

#### H. Planting Requirements.

1. Intent. The intent of this section is to encourage the use of native species and recommend planting conditions adaptive to Bainbridge Island.
2. Requirements. Landscape designs shall conform to the following provisions:
  - a. Areas not devoted to landscape required by this chapter, parking, structures and other site improvements are encouraged to be planted or remain in existing vegetation.
  - b. New plant materials shall include native species or nonnative species that have adapted to the climatic conditions of the coastal region of the Puget Sound region.
  - c. New plant materials shall consist of drought resistant species, except where site conditions within the required landscape areas assure adequate moisture for growth.
  - d. New tree plantings shall be a minimum of two inches in caliper if deciduous or six feet in height if evergreen. New shrubs planted in roadside or perimeter buffers shall be of a variety that achieves a minimum six feet height at maturity. Soil planting types and depth shall be sufficient for tree planting.
  - e. When the width of any landscape strip is 20 feet or greater, the required trees shall be staggered in two or more rows.
  - f. Existing vegetation may be used to augment new plantings to meet the standards of this chapter.
  - g. Grass may be used as a ground cover where existing or amended soil conditions assure adequate moisture for growth.
  - h. Ground cover areas shall contain at least two inches of composted organic mulch at finish grade to minimize evaporation. Mulch shall consist of materials such as composted yard waste, composted sawdust, and/or manure that are fully composted.

i. Existing and/or compacted soils ~~may need to be augmented with fully composted organic material or aerated.~~ shall be amended in accordance with BIMC 15.20.

j. Specific submittal requirements for landscaping plans (tree protection, retention and planting plans) are included in the city's administrative manual.

**Section 16.** Section 18.15.010.J of the Bainbridge Island Municipal Code is amended to read as follows:

J. Maintenance.

1. Intent. All new landscape plantings and significant trees and tree stands to be retained shall be maintained to preserve the Island's forested character.

2. Requirements.

a. All landscaping, significant trees and tree stands shall be maintained ~~for the life of the project.~~

~~b. All landscaped areas required by this chapter, significant trees and tree stands, except within critical areas or their protective buffers (defined in Chapter 16.20 BIMC), should be maintained~~ in a healthy growing condition.

~~b.~~ Landscape areas shall be kept free of trash.

~~c.~~ All plant material shall be managed by pruning so that plant growth does not conflict with public utilities, restrict pedestrian or vehicular access, or create a traffic hazard.

**Section 17.** Section 18.15.020.B of the Bainbridge Island Municipal Code is amended to read as follows:

B. General Requirements.

1. Driveways, parking, and walkways shall accommodate pedestrians, motor vehicles and bicycles used by occupants or visitors of a structure or use. Location is subject to review of the planning and engineering departments.

2. No building permit shall be issued until the applicant has submitted satisfactory plans demonstrating that required parking facilities will be provided and maintained.

3. Unless authorized by a conditional use permit or this title, the use of property in a residential zone for commercial parking is prohibited.

4. All driveways and other parking areas ~~except those serving single family residences,~~ shall be surfaced with permanent materials ~~such as asphalt, concrete, unit pavers, or pervious materials~~ acceptable to the public works department, and shall be

designed to manage stormwater runoff in accordance with ~~dispose of surface water, and pollutants from motor vehicles as provided in the~~ BIMC 15.20.

~~5. Parking lots may be gravel if (a) the parking lot contains less than 10,000 square feet, or (b) the parking lot has less than a five percent slope. Residential parcels are encouraged to have two-track driveways (also known as Hollywood or wheel strip driveways).~~

6. Unless approved by the director, only a single access to public right-of-way is allowed for an individual lot. More than one access may be allowed by the director if the director determines, based on drawings or other information submitted by the applicant, that (a) the proposed site access includes measures that mitigate any identified negative impacts or effects that would result from the additional access point(s); and (b) the additional access point(s) will improve on-site or off-site traffic flow or is necessary for, or will help facilitate, compliance with other requirements of this chapter.

7. Joint use of required access ways with adjacent properties is encouraged. The director may approve joint access if the applicant demonstrates to the satisfaction of the director that the joint access (a) will promote the orderly development of the surrounding area; or (b) will help reduce or avoid cumulative adverse impacts that would result from each property accessing the right-of-way separately; and (c) will not create a safety hazard.

8. With the exception of single-family and duplex buildings on individual lots, access and parking spaces shall be designed so that no backing movement by a vehicle, except emergency and service and delivery vehicles, shall be allowed onto a public right-of-way; provided, that the director may waive this requirement where no reasonable design alternative exists.

9. No parking space may block access to other parking spaces unless tandem parking has been approved for a single residence or individual dwelling units of a multifamily structure.

10. On-street parking created or designated in conjunction with and adjacent to a project may be included in the parking space calculation upon approval of the director.

11. When a new commercial or mixed use development is required to provide parking for more than 25 cars, at least one parking space near the entrance must be reserved and signed for use by a shared-car program or electric vehicle charging station.

12. For all development except for single-family residential, the required parking for two or more complementary uses may be reduced up to 50 percent when provided by a common parking lot, but may not be reduced below the highest parking requirement. The reduction shall be authorized by the issuance of a conditional use permit.

**Section 18.** Section 18.15.020.G of the Bainbridge Island Municipal Code is amended to read as follows:

G. Commercial Parking or Commercial Parking Businesses, Other than Ferry Commuter Parking and Noncommuter Ferry Parking. Commercial parking may be developed for general public use at no fee, or as a commercial parking business. Commercial parking businesses must comply with provisions of Chapter 5.10 BIMC.

1. Surface Parking Lots. Surface parking lots for commercial parking only, developed by public or private concerns, or developed by a public or cooperative commercial effort shall be treated as special cases under Table 18.15.020-2 and are permitted in the core, gateway, and ferry terminal districts, providing:

- a. Parking lots shall be sited on parcels within 200 feet of Winslow Way or lower Madison (south of Wyatt).
- b. Parking lots shall not be sited adjacent to a parcel containing a parking lot or structure in which parking is the primary use.
- c. Parking lots shall not exceed 30 spaces.
- d. As a condition of development application approval, the property owner shall include a plan for designating parking for only noncommuter use and shall demonstrate how restriction of spaces for noncommuter parking will be enforced. Failure to enforce shall subject the owner to the provisions of Chapter 1.26 BIMC.

[e. Surface parking lots shall integrate LID BMPs in accordance with BIMC 15.20.](#)

**Section 19.** Section 18.15.020.J of the Bainbridge Island Municipal Code is amended to read as follows:

J. Design Standards.

1. Parking Space and Aisle Dimensions. Except as provided in subsection J.1.e of this section, parking lots shall be designed according to Table 18.15.020-3. Space depth shall be measured exclusive of access drives, aisles and other physical obstructions. Small car spaces may total no more than 30 percent of the required number.

- a. Parking lots shall have direct access to a street or road easement and shall provide unobstructed access driveways exclusive of the required parking areas.
- b. Multifamily and nonresidential developments shall use access standards as shown in Table 18.15.020-3.
- c. Where possible, single-family residences shall share access drives.

d. Access drive widths for single-family residences shall be determined by the city engineer or fire marshal.

e. For parking located in structures, columns or other structural elements may encroach into the parking space a maximum of six inches on a side; provided, that no wall, post, guardrail, or other element shall obstruct car door opening or the exitway of persons from a parked vehicle.

**Table 18.15.020-3: Parking Space and Lot Design and Dimensions [1]**

<b>A Parking Angle</b>	<b>B Stall Width (ft.)</b>	<b>C [2] Stall Depth (ft.)</b>	<b>D Aisle Width (paved surface ft.)</b>	<b>Direction of Travel</b>
45°	7.5	<del>16</del> 15	11	1-way
	8.5	<del>19</del> 18	13	1-way
	7.5	<del>16</del> 15	18	2-way
	8.5	<del>19</del> 18	20	2-way
60°	7.5	<del>16</del> 15	14	1-way
	8.5	<del>20.5</del> 19.5	14.5	1-way
	7.5	<del>16</del> 17	20	2-way
	8.5	<del>20.5</del> 19.5	20	2-way
75°	7.5	<del>16</del> 15	17.5	1-way
	8.5	<del>21</del> 20	18.5	1-way
	7.5	<del>16</del> 15	20	2-way
	8.5	<del>21</del> 20	20	2-way
90°	7.5	<del>16</del> 15	20	2-way
	8.5	<del>20</del> 19	24	2-way
Parallel	<u>20</u>	<u>8.5</u>	<u>12</u>	<u>1-way</u>
	<u>20</u>	<u>8.5</u>	<u>12</u>	<u>2-way</u>

[1] The first line of each category (e.g., 45 degrees, one-way travel) indicates the dimensions for compact cars.

[2] Where wheel stops are required, they shall be placed 18 inches from the end of stall. Landscaping may be located between the wheel stop and the end of the stall. Landscaping so located shall be in addition to, and not part of, any landscaping required by this title.

2. Grades. Where parking spaces are designated, grades shall not exceed six percent. Driveways and driving lanes between separate groups of parking shall not exceed 14 percent. Parking areas on sloping lots shall be laid out so that parked cars lie perpendicular to the slope. Where existing grades on property proposed for a parking

lot exceed 10 percent, the city may require a topographic survey to show existing and proposed grades.

3. Landscaping.

a. Parking lots shall be landscaped in accordance with BIMC 18.15.010.F.

b. ~~Permeable paving materials are permitted and encouraged~~ Permeable pavement is preferred in both accessory and primary parking lots. The following permeable paving material types of permeable pavement have been found to perform well in the Puget Sound climate when properly designed: ~~porous pervious~~ concrete, porous asphalt, plastic grid systems, and interlocking permeable pavers.

**Section 20.** Section 18.15.030 of the Bainbridge Island Municipal Code is amended to read as follows:

18.15.030 Mobility and access.

The intent of this section is to improve mobility and access for pedestrians, bicyclists, and transit users in Bainbridge Island. All development shall comply with the development standards of RCW 58.17.110(1) and all long, short, and large lot subdivisions shall comply with the road and pedestrian access standards in BIMC 17.12.040.E.

A. Circulation and Walkways. The following standards shall apply to multifamily and nonresidential development.

1. Parking lots and driveways shall provide well-defined, safe and efficient circulation for motor vehicles, bicycles and pedestrians.

2. Landscaped islands with raised curbs shall be used to define entrances from public rights-of-way, define pedestrian walkways from the public rights-of-way to all buildings, define ends of parking aisles and indicate the pattern of circulation. Curb cuts or grates can be incorporated to allow water to enter stormwater facilities and LID BMPs.

3. Pedestrian walkways shall be provided around buildings to the extent necessary to assure safe access to the building from parking areas and the public right-of-way. Where appropriate, as determined by the approving body, pedestrian walkways may be required to assure safe access to adjacent properties.

4. Internal walkways shall be surfaced with nonskid hard surfaces, such as permeable pavement, meet accessibility requirements and be designed to provide a minimum of five feet of unobstructed width. Where walkways cross vehicular driving lanes, the walkways shall be constructed of contrasting materials or with maintained painted markings. Walkways shall be curbed and raised six inches above adjacent vehicular surface grade, except where the walkway crosses vehicular driving lanes or is required to meet accessibility standards and at inlets to stormwater facilities and LID BMPs.

5. To provide connectivity between adjacent trails/walkways, pedestrian walkways may be required.

**Section 21.** Chapter 18.18 of the Bainbridge Island Municipal Code is amended to read as follows:

18.18.020 Encouraging sustainability.

The site designs of all developments and redevelopments are encouraged to accommodate solar panels, small wind energy generators, ~~and rain garden/swales/bioretention/rain gardens, permeable pavement, and other LID BMPs~~ where ~~feasible/practical~~. (Ord. 2011-02 § 2 (Exh. A), 2011)

18.18.030 Specific design regulations and guidelines.<sup>1</sup>

All development shall comply with the design regulations and guidelines applicable to that type of development as set forth in this section and the reference documents, which are adopted as part of this title by reference. In the event of a conflict between two or more design standards or regulations, the more specific shall apply.

A. Detached Single-Family Residential Developed Using the R-8SF Urban Single-Family Overlay District. Detached single-family residential developed in accordance with the R-8SF urban single-family overlay district transfer of development rights program shall comply with those regulations contained in “Design Guidelines for R-8SF Urban Single-Family Overlay District” if they want to develop at overlay zone densities.

B. Multifamily Residential. Multifamily development in the R-8 and R-14 zones shall comply with those regulations contained in “Design Guidelines for Multifamily”; provided, that applications submitted prior to December 8, 1999, shall not be subject to the requirements of this section.

C. Commercial and Mixed Use – General. Development, redevelopment, and exterior renovation in commercial and mixed use projects in all zoning districts except the B/I district shall comply with the general guidelines in “Guidelines for Commercial and Mixed Use Projects – Including Guidelines for Lynwood Center, Island Center, and Rolling Bay,” as well as any specific guidelines applicable to that type of development in the subsections below.

D. Nonresidential Uses in Residential Zones. Educational, cultural, governmental, religious or health care facilities in residential zones shall comply with the general guidelines in “Design Guidelines for Commercial and Mixed Use Projects – Including Guidelines for Lynwood Center, Island Center, and Rolling Bay.”

E. Mixed Use Town Center and High School Road Districts. Development, redevelopment, or exterior renovation in the Mixed Use Town Center overlay districts and the High School Road districts shall comply with regulations contained in “Design Guidelines for Mixed Use Town Center and High School Road Zoning Districts.”

F. Lynwood Center NSC Design Guidelines. Development, redevelopment, and exterior renovation in the Lynwood Center NSC zone district shall comply with those regulations contained in the Lynwood Center NSC-specific portion of “Guidelines for Commercial

and Mixed Use Projects – Including Guidelines for Lynwood Center, Island Center, and Rolling Bay.”

1. Street trees shall be provided in an amount equivalent to at least one every 30 feet in planting pots or beds covered by a tree grate, pavers or planted area. Structural grid systems with a minimum soil volume ratio of 1 cubic foot of soil per 1 square foot of tree canopy area are encouraged. Trees may be grouped and are encouraged to have a varied meandering effect. Tree size, location and species shall be approved by the city. See street tree diagram in Central Core Overlay District Design Guidelines (subsection E of this section).

G. Island Center NSC Design Guidelines. Development, redevelopment, and exterior renovation in the Island Center NSC zone district shall comply with those regulations contained in the Island Center NSC-specific portion of “Guidelines for Commercial and Mixed Use Projects – Including Guidelines for Lynwood Center, Island Center, and Rolling Bay.”

1. Street trees shall be provided in an amount equivalent to at least one every 30 feet in planting pots or beds covered by a tree grate, pavers or planted area. Structural grid systems with a minimum soil volume ratio of 1 cubic foot of soil per 1 square foot of tree canopy area are encouraged. Trees may be grouped and are encouraged to have a varied meandering effect. Tree size, location and species shall be approved by the city. See street tree diagram in Central Core Overlay District Design Guidelines (subsection E of this section).

H. Rolling Bay NSC Design Guidelines. Development, redevelopment, and exterior renovation in the Rolling Bay NSC zone district shall comply with those regulations contained in the Rolling Bay NSC-specific portion of “Guidelines for Commercial and Mixed Use Projects – Including Guidelines for Lynwood Center, Island Center, and Rolling Bay.”

1. Street trees shall be provided in an amount equivalent to at least one every 30 feet in planting pots or beds covered by a tree grate, pavers or planted area. Structural grid systems with a minimum soil volume ratio of 1 cubic foot of soil per 1 square foot of tree canopy area are encouraged. Trees may be grouped and are encouraged to have a varied meandering effect. Tree size, location and species shall be approved by the city. See street tree diagram in Central Core Overlay District Design Guidelines (subsection E of this section).

**Section 22.** Section 18.36.030 of the Bainbridge Island Municipal Code is amended to read as follows:

18.36.030 Definitions.

37. “Bioretention” means an engineered facilities that store and treat stormwater by passing it through a specified soil profile, and either retain or detain the treated stormwater for flow attenuation.

75. “Dripline” means the outermost circumference of a tree canopy where water drips from and onto the ground.

~~875. “Established vegetation” means mature trees and, shrubs, or groundcovers.~~

~~1542. “Mature trees and shrubs” means “significant trees” as defined in this chapter and/or native shrubs, ferns and forbs established at a density that provides a predominantly continuous cover vegetation with well-established root systems that provide a predominantly continuous cover.~~

~~1553. “Mature vegetation on ridgelines” means all existing vegetation that is well-established and exists along the line formed by the highest points of a ridge and “significant trees” as defined in this chapter located on those dominant natural topographic features that are prominently visible from off-site public rights-of-way and lands.~~

~~1642. “Native forest” means mature trees and shrubs, and groundcovers consisting of native species, but dominated by native trees and providing at least 50 percent tree canopy and plants.~~

~~1664. “Native vegetation” means plant species which are indigenous to the coastal region of the Pacific Northwest and which reasonably could have been expected to naturally occur on the site. Invasive/exotic species shall not be considered native species tree, shrub, or ground cover species that are indigenous to the Central Puget Sound lowland region.~~

~~2075. “Rain garden” means a non-engineered shallow, landscaped depression, with compost-amended native soils and adapted plants. The depression is designed to pond and temporarily store stormwater runoff from adjacent areas, and to allow stormwater to pass through the amended soil profile. “Rain garden/swale” means a ground-level sited design feature designed to detain rainwater for infiltration and reuse consistent with water rights laws.~~

~~2086. “Rainwater harvesting barrels” means a barrel cistern designed for the on-site harvest and storage of rainwater for nonpotable uses such as irrigation, toilet flushing, and laundry used to offset the potable water needs for a building and/or landscape.~~

~~2286. Site. When used in connection with historic preservation, “site” means a place where a significant event or pattern of events occurred. It may be a location of prehistoric or historic occupation or activities that may be marked by physical remains or it may be the symbolic focus of a significant event or pattern of events, although not actively occupied. A site may be the location of a ruined or now nonexistent building or structure if the location itself possesses historic, cultural or archaeological significance. When used in connection with new development or redevelopment, “site” means the area defined by the legal boundaries of a parcel or parcels of land that is (are) subject to new development or redevelopment.~~

~~2520. “Tree canopy” means the total area of the tree or trees where the leaves and outermost branches extend, also known as the “dripline.”~~

**Section 23.** This ordinance shall take effect and be in force five (5) days from its passage, approval, and publication as required by law.

PASSED BY THE CITY COUNCIL this \_\_\_\_ day of XXX, 2016.

APPROVED BY THE MAYOR \_\_\_\_ XX day of XXX, 2016.

\_\_\_\_\_  
Val Tollefson, Mayor

ATTEST/AUTHENTICATE:

\_\_\_\_\_  
Rosalind D. Lassoff, CMC, City Clerk

FILED WITH THE CITY CLERK: XXX, 2016

PASSED BY THE CITY COUNCIL:

PUBLISHED:

EFFECTIVE DATE:

ORDINANCE NUMBER: 2016-28

DRAFT

**Table 2.16.020.Q-3 Innovative Site Development Scoring Method**

TIER	Minimum Site Development Point Requirement	WATER QUALITY & CONSERVATION		LANDSCAPING & OPEN SPACE			TRANSPORTATION	
		REQUIREMENT	VALUE	% OF OPEN SPACE	VALUE	VALUE IF PUBLIC	TRANSPORTATION COMPONENTS	VALUE
		Projects use methods to decrease water usage and improve stormwater runoff quality through an integrated approach to stormwater management such as greywater use, stormwater collection in cisterns, <u>green-vegetated</u> roofs and covered parking. All HDDP projects will follow the <u>WA State DOE 2012 Stormwater Management Manual for Western Washington, as amended in December 2014 adopted stormwater manual in BIMC 15.20</u> .		Project provides well-designed common open space, with at least 5 percent of the gross land area, set aside as open space and designed as an integrated part of the project rather than an isolated element. The common open space must be outside of critical areas and their buffers and required roadside buffers. Appropriate community amenities such as playgrounds, composting and neighborhood gardens promoting the production of locally grown food are encouraged. Resident neighborhood community gardens can be in common open space areas, and shall be appropriately located for solar exposure, and include water availability, soil amenities, and storage for garden tools. Required growing space for neighborhood gardens is 60 square feet per dwelling unit, not including any existing orchard area. Open space dedicated to the public pursuant to the standards of BIMC Sections 17.12.030. A1, A2, A3, A6 & A7 is encouraged.			Project design provides enhanced sensitivity to pedestrian and bicycle travel to promote the people getting around without a car, a reduced carbon footprint, improved health of humans, and lower pollution levels. Project internally preserves existing informal internal connection to external non-motorized facilities, furthering the Island-wide Transportation Plan (IWTP) and using such solutions as woonerfs, green streets, and natural trails and paths. Project reduces reliance on automobiles and trip counts, and promotes alternative transportation, such as integrating parking and charging facilities for electric cars, or bus shelters.	
4	30	Number of dwelling units that integrate greywater reuse components into building design:						
		10%	1	5-10%	2	4	Project preserves, creates or integrates internal and external non-motorized connections.	
		11-20%	2	11-15%	4	6	Provides public walkways, separated paths, or bike lanes. No points for facilities required by IWTP.	
		21-30%	3	16-20%	6	8	On-site car sharing program	
		Over 31%	4	21-25%	8	10	Electric vehicle charging stations for 3% of vehicle parking capacity.	
3	25	Percentage of total roof area qualifying as " <u>green-vegetated</u> roofs":		Greater than 25%			Covered, consolidated bike parking for subdivisions	
		15-30%	2	Incorporates neighborhood garden			Bus Shelter	
		Over 31%	4	Preserves tree that qualifies as a "Heritage tree" under City Program. The tree is not otherwise required to be preserved.			2 per tree	
2	25	Project integrates cisterns: % of total roof area directed to cisterns:		All Private yard areas ≤ 20% turf			4	
		15-30%	2	Project landscaping integrates at least 60% native or drought tolerant plants			4	
		Over 31%	4					
1	14	Percentage of total parking spaces that are covered (i.e. parking garage, carport):						
		5-20%	1					
		21-40%	2					
		41-60%	3					
		61-80%	4					
		Over 81%	5					

## Chapter 13.04

### DEFINITIONS

#### **13.04.060 Compatible pollutant.**

“Compatible pollutant” for wastewater discharges means biochemical oxygen demand, suspended solids, pH, and fecal coliform bacteria, plus additional pollutants identified in an NPDES a National Pollutant Discharge Eliminating system (NPDES) wastewater discharge permit if the publicly owned treatment works is designed to treat such pollutants, and in fact does remove such pollutants to a substantial degree. The term “substantial degree” is not subject to precise definition, but generally means contemplated removals in the order of 80 percent or greater. Examples of the additional pollutants which may be considered compatible include:

- A. Chemical oxygen demand;
- B. Total organic carbon;
- C. Phosphorus and phosphorus compounds;
- D. Nitrogen and nitrogen compounds;
- E. Fats, oils and greases of animal or vegetable origin, except as prohibited where these materials would interfere with the operation of the publicly owned treatment works. (Ord. 82-20 § A(30), 1982)

#### **13.04.110 Incompatible pollutant.**

“Incompatible pollutant” for wastewater discharges means any pollutant which is not a compatible pollutant as defined in “compatible pollutant.” (Ord. 82-20 § A(31), 1982)

## Chapter 13.24

### STORM AND SURFACE WATER UTILITY

Sections:

- 13.24.010 Purpose.
- 13.24.020 Utility established.
- 13.24.030 Jurisdiction.
- 13.24.040 Transfer of property.
- 13.24.050 Definitions.
- 13.24.060 Storm and surface water utility fund.
- 13.24.070 Utility rate policy.
- 13.24.080 Powers and authority.
- 13.24.090 Fee imposed.
- 13.24.110 Utility service charge calculation.
- 13.24.120 Undeveloped real property.
- 13.24.130 Service charges.
- 13.24.131 Rate reductions.
- 13.24.132 Property exempt from service charges.
- 13.24.140 Billing and payment.
- 13.24.150 Service charge adjustments and appeals.
- 13.24.170 Lien for service – Interest.
- 13.24.180 Inspections – Right of entry – Emergency.

#### 13.24.050 Definitions.

The following words when used in this chapter shall have the following meanings, unless the context clearly indicates otherwise:

A. “Agricultural land” means land primarily devoted to agricultural operations.

B. “Agricultural operation” means any facility or activity for the production or intent of production for commercial or family use purposes of dairy, apiary, livestock, camelids, ratites, vegetable or animal products, and crop products including, but not limited to, ornamental crops. Incidental vegetable gardening, landscaping and keeping common pets by single-family residential properties are not defined as agriculture.

C. “Commercial use” means the providing of goods or services for compensation.

D. “Developed” shall mean that condition of real property altered from its natural state by the creation or addition to or construction on such property of impervious ground cover – hard surfaces; expansion of a building footprint, addition or replacement of a building or other structures; structural development including construction, installation or expansion of a building or other structure; replacement of hard surface that is not part of a routine maintenance activity; and land disturbing activities or other manmade physical improvements (such as clearing or grading) ~~such that the hydrology of the property or portion thereof is affected.~~

E. “Hard surface” shall mean an impervious surface, a permeable pavement or a vegetated roof.

~~FE. “Impervious ground cover” shall mean those hard surfaces which either prevent or impede the entry of water into the soil in the manner that such water entered the soil under natural conditions preexistent to development, or which cause water to run off the surface in greater quantities or at an increased rate of flow than that present under natural conditions preexistent to development, including, without limitation, such surfaces as rooftops, greenhouses, asphalt or concrete sidewalks, paving, unnamed ways of travel, driveways and parking lots, walkways, artificial turf playing fields, patio areas, storage areas, and gravel, oiled macadam or other surfaces which similarly affect the natural infiltration or runoff patterns that existed prior to development.~~

~~Lawns, pastures, agricultural land, and landscaped areas, including playgrounds with soft ground cover, are not impervious ground cover. Permeable pavement areas that have been designed to completely infiltrate water applied~~

~~to the surface, as documented in a drainage report submitted and approved for the developed property, are not impervious ground cover.”~~Impervious Surface” shall mean a non-vegetated surface area which either prevents or retards the entry of water into the soil mantle as under natural conditions prior to development. A non-vegetated surface area which causes water to run off the surface in greater quantities or at an increased rate of flow from the flow present under natural conditions prior to development. Common impervious surfaces include, but are not limited to, roof tops, walkways, patios, driveways, parking lots or storage areas, concrete or asphalt paving, gravel roads, packed earthen materials, and oiled, macadam or other surfaces which similarly impede the natural infiltration of stormwater.”

GF. “Impervious surface unit (ISU)” shall mean the average estimated amount of impervious ground cover on a single-family residential parcel. For the purposes of calculating the service charges in BIMC 13.24.130, an ISU shall be defined as 3,000 square feet of impervious ground cover and is the unit of measurement used by the utility in assessing service charges.

GH. “Landscaped areas” shall mean those areas of any property type that are planted with trees, shrubs, or other vegetation, including the soil or bedding material areas associated with the plantings.

HI. “Low intensity development” shall mean any development, excluding single family, which creates or utilizes less than 15 percent impervious ground cover on a parcel.

IJ. “Multifamily” shall mean any residential structure designed for occupancy by multiple-family households in rented or leased apartments.

K. “On-site stormwater management facility” means low impact development best management practices as defined in BIMC 15.20.

LJ. “Other developed property” shall mean all property developed for other than single-family residential uses. Such other developed properties include apartments, municipal, commercial, retail, industrial, manufacturing, maintenance, utility, recreation, agriculture, park, school, marina, religious, convalescent center, and any other private or public purposes, including properties with commercial operations that may also contain one or more residences.

MK. “Service charge” shall mean the fee levied by the utility.

NL. “Single-family residential” shall mean individual single-family homes, mobile homes, condominiums and duplex homes.

OM. “Structure” means any manmade assemblage of materials extending above or below the surface of the earth and affixed or attached thereto.

PN. “Undeveloped” shall mean that condition of real property unaltered by construction on, or addition to, such property of impervious ground cover or physical manmade improvements of any kind in excess of 100 square feet that change the hydrology of the property from its natural state.

QE. “Utility” means the storm and surface water utility established by Ordinance No. 86-27, passed on July 16, 1986.

RP. “Way-of-travel” means a roadway of whatever sort, including, but not limited to, avenues, boulevards, circles, courts, roads, drives, lanes, loops, places, tracts and ways, which is capable of carrying vehicular traffic. (Ord. 2015-20 § 2, 2015)

#### **13.24.131 Rate reductions.**

For parcels, excluding single-family residential properties:

A. The service charge charged to a parcel for impervious ground cover areas consisting of compacted gravel surfaces used by vehicles as a way-of-travel or parking shall be 75 percent of the rate for impervious ground cover areas set forth in BIMC 13.24.130;

B. For any property other than a single-family residence (including mobile homes, condominiums and duplexes), if the property owner (1) has been required by either the city or Kitsap County since January 1, 1985, to construct an on-site storm water mitigation facility as a condition of the property's development or (2) has constructed voluntarily since January 1, 1985, an on-site ~~storm water mitigation~~stormwater management facility serving the property and exceeding city standards at the time of construction, the city may at its sole discretion reduce by up to 50 percent of the storm and surface water service fee charged for the property pursuant to BIMC 13.24.130. For parcels that construct qualifying green infrastructure (low impact development) as on-site ~~storm water mitigation~~stormwater management in accordance with Chapter 15.20 BIMC, the city may, at its sole discretion, reduce the utility service fee charged by up to 50 percent for the property pursuant to BIMC 13.24.130.

C. Any low intensity parcel that creates or utilizes less than 15 percent impervious ground cover will be eligible for a rate reduction of 50 percent, in the city's sole discretion.

D. In order to obtain a rate reduction pursuant to subsections B and C of this section, the parcel owner must submit the required forms that demonstrate the qualification for a rate reduction by November 15th of a given year. If approved, the rate reduction will become effective in the calendar year following the request.

E. The rate reduction authorized by this section shall not reduce the total storm and surface water utility service fee to less than 50 percent of the fee required pursuant to BIMC 13.24.130, and shall not be used in conjunction with any other rate reduction authorized by this title. The minimum ISU charge per developed property shall be one ISU.

F. Low income senior and disabled citizens who meet the requirements set forth in Chapter 13.16 BIMC shall be entitled to a reduction in service charges as established by city resolution. (Ord. 2015-20 § 2, 2015)

## Chapter 15.20

### **SURFACE ~~WATER~~ AND ~~STORM WATER~~STORMWATER MANAGEMENT<sup>1</sup>**

Sections:

- 15.20.010 Purpose.
- 15.20.020 Definitions.
- 15.20.030 General provisions.
- 15.20.040 Regulated activities and allowed activities.
- 15.20.050 General requirements.
- 15.20.060 Approval standards.
- 15.20.070 Administration.
- 15.20.080 Enforcement.
- 15.20.090 Repealed.
- 15.20.100 Repealed.

#### **15.20.010 Purpose.**

The provisions of this chapter are intended to establish regulation for all new development, redevelopment or construction activities within the city that will or may impact surface ~~water~~ or ~~storm waters~~stormwater. The provisions of this chapter establish the minimum requirements that must be met to permit a property to be developed, redeveloped or proceed with construction activities within the city. The purpose of this chapter is to:

- A. Preserve and enhance the suitability of waters for contact recreation, fishing, and other beneficial uses;
- B. Minimize water quality degradation and sedimentation in streams, ponds, lakes, wetlands and other water bodies;
- C. Minimize the impact of increased runoff, erosion and sedimentation caused by land development and poor maintenance practices;
- D. Maintain and protect ground water resources;
- E. Minimize adverse impacts from projects on ground and surface water quantities, locations and flow patterns;
- F. Decrease potential landslide, flood and erosion damage to public and private property;
- G. Promote site planning and construction practices that are consistent with natural topographical, vegetational and hydrological conditions;
- H. Maintain and protect the city ~~storm water~~stormwater management infrastructure and downstream systems and properties. (Ord. 2009-13 § 1, 2009; Ord. 98-31 § 1, 1999)

#### **15.20.020 Definitions.**

1. "Approval" means the proposed work or completed work conforms to this chapter in the opinion of the director.

~~2. "As graded" means the extent of surface conditions on completion of grading.~~

~~3. "Basin plan" means a plan and all implementing regulations and procedures including but not limited to land use management adopted by ordinance for managing surface and storm water management facilities and features within individual sub-basins.~~

~~4. "Bedrock" means the more or less solid rock in place either on or beneath the surface of the earth. It may be soft, medium, or hard and have a smooth or irregular surface.~~

~~5. "Bench" means a relatively level step excavated into earth material on which fill is to be placed.~~

~~26.~~ “Best management practice (BMP)” means physical, structural, and/or managerial practices that, when used singly or in combination, prevent ~~and/or~~ reduce the release of pollutants and other adverse impacts to waters of Washington State ~~pollution of water. BMPs are listed and described in the manual.~~

~~7.~~ “Certified erosion and sediment control lead (CESCL)” means an individual who has current certification through an approved erosion and sediment control training program that meets the minimum training standards established by the Department of Ecology (see BMP C160 in the manual). A CESCL is knowledgeable in the principles and practices of erosion and sediment control. The CESCL must have the skills to assess site conditions and construction activities that could impact the quality of storm water and the effectiveness of erosion and sediment control measures used to control the quality of storm water discharges. Certification is obtained through an Ecology-approved erosion and sediment control course. Course listings are provided online at Ecology’s web site.

~~38.~~ “City” shall mean the city of Bainbridge Island.

~~9.~~ “Civil engineer” means a professional engineer licensed in the state of Washington in civil engineering who is experienced and knowledgeable in the practice.

~~10.~~ “Civil engineering” means the application of the knowledge of the forces of nature, principles of mechanics and the properties of materials and to the evaluation, design and construction of civil works.

~~44.~~ “Clearing” means the destruction and removal of vegetation by manual, mechanical, or chemical methods.

~~12.~~ “Commercial agriculture” means those activities conducted on lands defined in RCW 84.34.020(2), and activities involved in the production of crops or livestock for wholesale trade. An activity ceases to be considered commercial agriculture when the area on which it is conducted is proposed for conversion to a nonagricultural use or has been idle for more than five years, unless the idle land is registered in a federal or state soils conservation program, or unless the activity is maintenance of irrigation ditches, laterals, canals, or drainage ditches related to an existing and ongoing agricultural activity.

~~513.~~ “Compaction” means densification of a fill by mechanical means.

~~14.~~ “Critical areas” means, at a minimum, areas which include wetlands, areas with a critical recharging effect on aquifers used for potable water, fish and wildlife habitat conservation areas, frequently flooded areas, geologically hazardous areas, including unstable slopes, and associated areas and ecosystems. Reference Chapter 16.20 BIMC.

~~15.~~ “Design storm” means a prescribed hyetograph and total precipitation amount (for a specific duration recurrence frequency) used to estimate runoff for a hypothetical storm for the purposes of analyzing existing drainage, designing new drainage facilities or assessing other impacts of a proposed project on the flow of surface water. (A hyetograph is a graph of percentages of total precipitation for a series of time steps representing the total time during which the precipitation occurs.)

~~646.~~ “Detention” means the release of ~~storm water~~ stormwater runoff from a specific site at a slower rate than it is collected by the ~~storm water~~ stormwater facility system, the difference being held in temporary storage.

~~747.~~ “Detention facility” means an above or below ground facility, such as a pond or tank, that temporarily stores ~~storm water~~ stormwater runoff and subsequently releases it at a slower rate than it is collected by the drainage facility system. There is little or no infiltration of stored ~~storm water~~ stormwater.

~~18.~~ “Drainage basin” means a geographic and hydrologic subunit of a watershed. Further clarification is located in the drainage reconnaissance study or basin assessment.

~~849.~~ “Earth material” means any rock, natural soil or fill and/or any combination thereof.

~~20.~~ “Engineering geologist” means a geologist experienced and knowledgeable in engineering geology.

~~21.~~ “Engineering geology” means the application of geologic knowledge and principles in the investigation and evaluation of naturally occurring rock and soil for use in the design of civil works.

~~922.~~ “Erosion” means the wearing away of the land surface by running water, wind, ice, or other geological agents, including such processes as gravitational creep, or the detachment and movement of soil or rock fragments by water, wind, ice, or gravity.

~~1023.~~ “Excavation” means the mechanical removal of earth material.

~~24.~~ “Experimental BMP” means a BMP that has not been tested, evaluated and approved for general use by the Department of Ecology in collaboration with local governments and technical experts. These include BMPs known as emerging technologies.

~~1125.~~ “Fill” means a deposit of earth material placed by artificial means.

~~1226.~~ “Forest practice” means any activity conducted on or directly pertaining to forest land and relating to growing, harvesting, or processing timber, as defined by RCW 76.09.050.

~~27.~~ “Frequently flooded areas” means the 100-year floodplain designations of the Federal Emergency Management Agency and the National Flood Insurance Program. Reference Chapter 15.16 BIMC.

~~1328.~~ “Geologically hazardous areas” means areas susceptible to significant erosion, sliding, earthquakes, or other geological events. They pose a threat to the public health and safety ~~of citizens~~ when used as sites for incompatible commercial, residential or industrial development. Geologically hazardous areas include erosion hazard areas, landslide hazard areas, and seismic hazard areas. Reference to Chapter 16.20 BIMC.

~~29.~~ “Grade” means the slope of a road, channel, or natural ground, the finished surface of a canal bed, roadbed, top of embankment, or bottom of excavation or any surface prepared for the support of construction such as paving or the laying of a conduit.

~~a.~~ “Existing grade” means the grade prior to grading.

~~b.~~ “Rough grade” means the stage at which the grade approximately conforms to the approved plan.

~~c.~~ “Finish grade” means the final grade of the site which conforms to the approved plan.

~~30.~~ “Grade, to” (“to grade”) means to finish the surface of a canal bed, roadbed, top of embankment or bottom of excavation.

~~31.~~ “Gradient terrace” means an earth embankment or a ridge and channel constructed with suitable spacing and an acceptable grade to reduce erosion damage by intercepting surface runoff and conducting it to a stable outlet at a stable nonerosive velocity.

~~1432.~~ “Ground water” means water in a saturated zone or stratum beneath the surface of land or a surface water body.

~~33.~~ “Hydroperiod” means the seasonal occurrence of flooding and/or soil saturation; it encompasses depth, frequency, duration, and seasonal pattern of inundation.

~~15.~~ “Hard surface” means an impervious surface, a permeable pavement, or a vegetated roof.

~~1634.~~ “~~Illicit~~ Illegal discharge” and “illegal connections” are as defined in BIMC 15.22.020.

~~1735.~~ “Impervious surface” means a hard-non-vegetated surface area which either prevents or retards the entry of water into the soil mantle as under natural conditions prior to development and/or a hard surface area which causes water to run off the surface in greater quantities or at an increased rate of flow from the flow present under natural conditions prior to development. Common impervious surfaces include, but are not limited to, roof tops, walkways, patios, driveways, parking lots or storage areas, concrete or asphalt paving, gravel roads, packed earthen materials, and oiled macadam or other surfaces which similarly impede the natural infiltration of ~~storm water~~stormwater. Open, uncovered retention/detention facilities shall not be considered as impervious surfaces for purposes of

determining whether the thresholds for application of minimum requirements are exceeded. Open, uncovered retention/detention facilities shall be considered impervious surfaces for purposes of runoff modeling.

1836. “Interflow” means that portion of rainfall that infiltrates into the soil and moves laterally through the upper soil horizons until intercepted by a stream channel or until it returns to the surface; for example, in a wetland, spring or seep.

1937. “Land disturbing activity” means any activity that results in ~~movement of earth, or~~ a change in the existing soil cover (both vegetative and nonvegetative) and/or the existing soil topography. Land disturbing activities include, but are not limited to, clearing, grading, filling and excavation. Compaction that is associated with stabilization of structures and road construction shall also be considered a land disturbing activity. Vegetation maintenance practices are not considered land disturbing activity. Stormwater facility maintenance is not considered land disturbing activity if conducted according to established standards and procedures.

20. “Low impact development (LID)” means a stormwater and land use management strategy that strives to mimic pre-disturbance hydrologic processes of infiltration, filtration, storage, evaporation and transpiration by emphasizing conservation, use of on-site natural features, site planning, and distributed stormwater management practices that are integrated into a project design.

21. Low impact development best management practices (LID BMPs)” means distributed stormwater management practices, integrated into a project design, that emphasize pre-disturbance hydrologic processes of infiltration, filtration, storage, evaporation and transpiration. LID BMPs include, but are not limited to: bioretention, rain gardens, permeable pavements, roof downspout controls, dispersion, soil quality and depth, minimal excavation foundations, vegetated roofs, and water re-use.

22. “LID principles” are land use management strategies that emphasize conservation, use of on-site natural features, and site planning to minimize impervious surfaces, native vegetation loss, and stormwater runoff.

2338. “Mitigation” means, in the following order of preference:

- a. Avoiding the impact altogether by not taking a certain action or part of an action;
- b. Minimizing impacts by limiting the degree or magnitude of the action and its implementation, by using appropriate technology, or by taking affirmative steps to avoid or reduce impacts;
- c. Rectifying the impact by repairing, rehabilitating or restoring the affected environment;
- d. Reducing or eliminating the impact over time by preservation and maintenance operations during the life of the action.

~~39.~~ “Natural location” means the location of those channels, swales, and other nonmanmade conveyance systems as defined by the first documented topographic contours existing for the subject property, either from maps or photographs, or such other means as appropriate.

24. “Native vegetation” means plant species that are indigenous to the coastal region of the Pacific Northwest and which reasonably could have been expected to naturally occur on the site. Invasive species and noxious weeds shall not be considered native species.

2540. “New development” land disturbing activities, including Class IV – general forest practices that are conversions from timber land to other uses; structural development, including construction or installation of a building or other structure; creation of ~~impervious hard~~ surfaces; and subdivision, short subdivision and binding site plans, as defined and applied in Chapter 58.17 RCW. Projects meeting the definition of redevelopment shall not be considered new development.

26. “Pervious surface” means any surface material that allows stormwater to infiltrate into the ground. Examples include lawn, landscape, pasture, native vegetation areas, and permeable pavement.

~~2741.~~ “Pollution” shall be construed to mean such contamination or other alteration of the physical, chemical, or biological properties of waters of the state, including change in temperature, taste, color, turbidity, or odor of the waters, or such discharge of any liquids, gaseous, solid, radioactive or other substance into any waters of the state as will or is likely to create a nuisance or render such waters harmful, detrimental or injurious to the public health, safety or welfare, or to domestic, commercial, industrial, agricultural, recreational, or other legitimate beneficial uses, or to livestock, wild animals, birds, fish or other aquatic life; as defined in RCW 90.48.020 as now existing or hereafter amended.

~~28.~~ “Pollution-generating hard surface (PGHS)” means hard surfaces considered to be a significant source of pollutants in stormwater runoff. See the listing of surfaces under pollution-generating impervious surface.

~~29.~~ “Pollution-generating impervious surface (PGIS)” means impervious surfaces considered to be a significant source of pollutants in stormwater runoff. Such surfaces include those which are subject to: vehicular use; industrial activities (as further defined in the glossary of the Stormwater Management Manual for Western Washington (SWMMWW)); storage of erodible or leachable materials, wastes, or chemicals, and which receive direct rainfall or the run-on or blow-in of rainfall; metal roofs unless they are coated with an inert, non-leachable material (e.g., baked-on enamel coating); or roofs that are subject to venting significant amounts of dusts, mists, or fumes from manufacturing, commercial, or other indoor activities.

~~30.~~ “Pollution-generating pervious surfaces (PGPS)” means any non-impervious surface subject to vehicular use, industrial activities (as further defined in the glossary of the Stormwater Management Manual for Western Washington (SWMMWW)); or storage of erodible or leachable materials, wastes, or chemicals, and that receive direct rainfall or run-on or blow-in of rainfall, use of pesticides and fertilizers, or loss of soil. Typical PGPS include permeable pavement subject to vehicular use, lawns, and landscaped areas including: golf courses, parks, cemeteries, and sports fields (natural and artificial turf).

~~3142.~~ “Redevelopment” means, on a site that is already substantially developed (i.e., has 35 percent or more of existing impervious surface coverage), the creation or addition of impervious surfaces; the expansion of a building footprint or addition or replacement of a structure; structural development including construction, installation or expansion of a building or other structure; replacement of impervious surface that is not part of a routine maintenance activity; and land disturbing activities.

~~43.~~ “Regional retention/detention system” means a storm water quantity control structure designed to correct existing excess surface water runoff problems of a basin or sub-basin for two or more properties. The area downstream has been previously identified as having existing or predicted significant and regional flooding and/or erosion problems. This term is also used when a detention facility is used to detain storm water runoff from a number of different businesses, developments or areas within a catchment.

~~44.~~ “Retention/detention facility (R/D)” means a type of drainage facility designed either to hold water for a considerable length of time and then release it by evaporation, plant transpiration, and/or infiltration into the ground; or to hold surface and storm water runoff for a short period of time and then release it to the surface and storm water management system.

~~32.~~ “Replaced hard surface” means, for structures, the removal and replacement of hard surfaces down to the foundation. For other hard surfaces, the removal down to bare soil or base course and replacement.

~~33.~~ “Replaced impervious surface” means, for structures, the removal and replacement of impervious surfaces down to the foundation. For other impervious surfaces, the removal down to bare soil or base course and replacement.

~~3445.~~ “Site” means the area defined by the legal boundaries of a parcel or parcels of land that is (are) subject to new development or redevelopment. For road projects, the length of the project site and the right-of-way boundaries define the site.

~~46.~~ “Slope” means the degree of deviation of a surface from the horizontal, measured as a numerical ratio, percent, or in degrees. Expressed as a ratio, the first number is the horizontal distance (run) and the second is the vertical distance (rise), as 2:1. A 2:1 slope is a 50 percent slope. Expressed in degrees, the slope is the angle from the horizontal plane, with a 90-degree slope being vertical (maximum) and 45 degrees being a 1:1 or 100 percent slope.

~~47. "Soil" means the unconsolidated mineral and organic material on the immediate surface of the earth that serves as a natural medium for the growth of land plants.~~

~~48. "Source control BMP" means a structure or operation that is intended to prevent pollutants from coming into contact with storm water through physical separation of areas or careful management of activities that are sources of pollutants. The manual separates source control BMPs into two types. Structural source control BMPs are physical, structural, or mechanical devices, or facilities that are intended to prevent pollutants from entering storm water. Operational BMPs are nonstructural practices that prevent or reduce pollutants from entering storm water. See Volume IV of the manual for details.~~

~~3549. "Storm waterStormwater" means that portion of precipitation that does not naturally percolate into the ground or evaporate, but flows via overland, interflow, channels or pipes into a defined surface water channel, or a constructed infiltration facility.~~

~~3650. "Storm waterStormwater drainage system" means constructed and natural features which function together as a system to collect, convey, channel, hold, inhibit, retain, detain, infiltrate, divert, treat or filter storm waterstormwater.~~

~~3751. "Storm waterStormwater facility" means a constructed component of a storm waterstormwater drainage system, designed or constructed to perform a particular function, or multiple functions, including but not limited to pipes, swales, ditches, culverts, street gutters, detention basins, retention basins, constructed wetlands, infiltration devices, catch basins, oil/water separators, sediment basins and modular pavement.~~

~~3852. "Storm waterStormwater management manual" or "manual" means the Stormwater Management Manual for Western Washington adopted by reference in BIMC 15.20.050and prepared by Washington State Department of Ecology, dated February 2005, Publication Numbers 05-10-029 through 05-10-033 including subsequent publications which contains BMPs to prevent or reduce pollution.~~

~~53. "Toe of slope" means a point or line of slope in an excavation or cut where the lower surface changes to horizontal or meets the exiting ground slope.~~

~~54. "Top of slope" means a point or line on the upper surface of a slope where it changes to horizontal or meets the original surface.~~

~~55. "Unstable slopes" means those sloping areas of land which have exhibited past and present history of mass movement of earth.~~

~~3956. "Vegetation" means all organic plant life growing on the surface of the earth. Referenee to Chapter 16.22 BIMC.~~

~~57. "Watershed" means a geographic region within which water drains into a particular river, stream, or body of water as identified and numbered by the State of Washington Water Resource Inventory Areas (WRIAs) as defined in Chapter 173-500 WAC.~~

~~40. "Waters of the State" includes those waters as defined as "waters of the United States" in 40 CFR Subpart 122.2 within the geographic boundaries of Washington State and "waters of the state" as defined in chapter 90.48 RCW which includes lakes, rivers, ponds, streams, inland waters, underground waters, salt waters and all other surface waters and water courses within the jurisdiction of the State of Washington.~~

~~4158. "Wetlands" means those areas that are inundated or saturated by surface or ground water at a frequency and duration sufficient to support (and under normal circumstances do support) a prevalence of vegetation typically adapted for life in saturated soil conditions, such as swamps, marshes, bogs, and other similar areas. This definition includes wetlands created, restored or enhanced as part of a mitigation procedure; it does not include constructed wetlands or the following surface waters of the state intentionally constructed from sites that are not wetlands: irrigation and drainage ditches, grass-lined swales, canals, agricultural detention facilities, farm ponds, and landscape amenities. Referenee to Chapter 16.20 BIMC.~~

(Ord. 2009-13 § 2, 2009; Ord. 2005-10 § 1, 2005; Ord. 2003-22 § 22, 2003; Ord. 2001-49 § 1, 2001; Ord. 98-31 § 1, 1999)

**15.20.030 General provisions.**

A. Procedures. The department of public works/engineering department is authorized to adopt written procedures for the purpose of carrying out the provisions of this chapter. Prior to fulfilling the requirements of this chapter, the administrator or assigns shall not grant any approval or permission to conduct a regulated activity, including but not limited to the following:

1. Building permits, commercial or residential;
2. Comprehensive plan amendments;
3. Conditional use permits;
4. Final plats (short/long/large lot);
5. Forest practices;
6. Grading or clearing permits;
- ~~7. Planned unit developments;~~
- ~~7~~8. Plats;
  - a. Subdivide, preliminary and final (short/long/large lot);
- ~~8~~9. Preliminary plats (short, long, large lot);
- ~~9~~10. Reasonable use exceptions;
- ~~10~~11. Right-of-way permits;
- ~~11~~12. Shoreline substantial development permits;
- ~~12~~13. Shoreline variance/shoreline conditional use permits;
- ~~13~~14. Site plan reviews;
- ~~14~~15. Variances;
- ~~15~~16. Zone reclassification (rezones); or
- ~~16~~17. Any subsequently adopted permit or required approval not expressly exempted by this chapter.

B. The following agencies may also require a drainage review to assess a site's impact. Any requirements imposed by these agencies are separate from the city mandates. It is the applicant's sole responsibility to resolve any conflicting issues that may arise from submittal reviews.

1. U.S. Army Corps of Engineers;
2. Washington State Department of Natural Resources;
3. Bremerton-Kitsap County Health District;
4. Washington State Department of Ecology: general permit is required for sites that disturb one acre or more;
5. Washington State Department of Fish and Wildlife;

6. Washington State Department of Transportation. (Ord. 2005-10 § 2, 2005: Ord. 98-31 § 1, 1999)

**15.20.040 Regulated activities and allowed activities.**

A. Regulated Activities. Consistent with the minimum requirements contained in the [stormwater](#) manual, the administrator shall approve or disapprove the following activities:

1. New Development.

- a. Land disturbing activities;
- b. Structural development, including construction, installation or expansion of an existing building or other structure;
- c. Creation of ~~800 square feet or more of~~ new [impervious-hard](#) surfaces ~~greater than 800 square feet~~;
- d. Class IV general forest practices that are conversions from timber land to other uses; and
- e. Subdivision, short subdivision and binding site plans, as defined in RCW 58.17.020.

2. Redevelopment. On an already developed site, the creation or addition ~~800 square feet or more of~~ [impervious-hard](#) surfaces ~~greater than 800 square feet~~; structural development including construction, installation or expansion of a building or other structure; any land disturbing activity, and/or replacement of [impervious-hard](#) surface (that is not part of a routine maintenance activity); and land disturbing activities associated with structural or [impervious-hard surface](#) redevelopment. (Ord. 2009-13 § 3, 2009: Ord. 2005-10 § 3, 2005: Ord. 98-31 § 1, 1999)

**15.20.050 General requirements.**

A. Stormwater Management Manual Adopted. The ~~February 2005 Edition of~~ Washington State Department of Ecology's [2012 Stormwater Management Manual for Western Washington, as amended in 2014](#), is hereby adopted by reference and is hereinafter referred to as the [stormwater](#) manual; provided, that certain provisions of the [stormwater](#) manual are amended as stated in BIMC 15.20.060.

B. ~~Illicit-Illegal~~ discharges and ~~illegal-illicit~~ connections to the ~~storm-water~~[stormwater](#) drainage system are prohibited by ~~Chapter BIMC 15.22-BIMC~~.

C. Low Impact Development Manual Adopted. The ~~2012~~[2009 Edition of the](#) Low Impact Development (LID) [Technical Guidance Manual for Puget Sound—A Practical Guide to LID Implementation in Kitsap County](#) is hereby adopted by reference and is hereinafter referred to as the LID manual ~~for use in meeting the relevant sections of the manual~~. (Ord. 2009-13 § 4, 2009: Ord. 2005-10 § 4, 2005: Ord. 98-31 § 1, 1999)

**15.20.060 Approval standards.**

A. ~~City-specific Standards~~. The City amends the standards adopted as part of the stormwater manual (summarized in [15.20.060.B](#)) as follows:

~~1. Instead of following the Better Site Design BMP (BMP T5.41) in Volume V, Section 5.3.2 of the stormwater manual, the applicant shall conduct a site assessment following the guidance in the LID Manual adopted in BIMC 15.20.050. The applicant shall comply with the City of Bainbridge Island Design and Construction Standards for streets and roadways unless an exception is granted in writing by the administrator.~~

~~2. The hard surface threshold for triggering Minimum Requirements No. 1 through No. 5 for new development and redevelopment is 800 square feet or greater of new plus replaced hard surface area.~~

~~3. Optional Guidance No. 1: Financial Liability, is not adopted.~~

~~4. Optional Guidance No. 2: Off Site Analysis and Mitigation, and Volume I, Section 3.1.3, Perform an Offsite Analysis, is required for projects creating 5,000 square feet or more of hard surface area.~~

~~5. In Volume I, Section 2.7 of the stormwater manual, Adjustments, is not adopted.~~

~~6. In Volume I, Section 2.3 of the manual, Definitions Related to Minimum Requirements, the definition of “The use of threshold discharge areas, as defined by the stormwater manual” is not adopted.~~

~~B. In Volume I, Section 2.5.6 of the manual, Minimum Requirement No. 6 Runoff Treatment, and Section 2.5.7, Minimum Requirement No. 7 Flow Control, and Section 4.2, BMP and Facility Selection Process, references to “threshold discharge area” are deleted.~~

7. In Volume III, Section 3.2 of the stormwater manual, Figure 3.12, Pond Signage, is amended to include the following language:

Developers shall provide the required signage for constructed ponds as a part of the project.

8. In Volume V, Section 4.3, Setbacks, Slopes and Embankments, is amended to include the following language:

All stormwater facilities and infiltration systems constructed within 200 feet of a geologically hazardous area shall have the concurrence of a Geotechnical Engineer.

9. The 2013 Rain Garden Handbook for Western Washington shall be used to supplement the Rain Garden (BMP T5.14A) design guidelines in the stormwater manual for projects triggering Minimum Requirements #1-5. Instead of applying the rain garden sizing chart provided in the Rain Garden Handbook for Western Washington, applicants shall apply the List #1 sizing requirement which is that rain gardens shall have a horizontally projected surface area below the overflow which is at least 5% of the total impervious surface area draining to it. If land/landscape area will also be draining to the rain garden, the horizontally projected surface area below the overflow shall be increased by 2% of the lawn/landscape area.

B. Stormwater Manual Standards. The following is a summary of the general thresholds included in the adopted stormwater manual with modifications based on the City-specific standards provided in 15.20.060.A as well as thresholds related to Minimum Requirements #5, #6, and #7. The adopted stormwater manual should be referred to for additional information and more complete guidance regarding implementation of these thresholds.

1. Overall project thresholds include the following:

<u>Applicable Requirements</u>	<u>New Development</u>	<u>Redevelopment</u>
<u>Minimum Requirement #2 (Construction Stormwater Pollution Prevention)</u>	<u>All projects</u>	<u>All projects</u>
<u>Minimum Requirements #1-5</u>	<u>≥ 800 sf new plus replaced hard surface area<sup>1</sup>, or</u> <u>≥ 7,000 sf land disturbing activity</u>	<u>≥ 800 sf new plus replaced hard surface area<sup>1</sup>, or</u> <u>≥ 7,000 sf land disturbing activity</u>
<u>Minimum Requirements #1-9</u>	<u>≥ 5,000 sf new plus replaced hard surface area, or</u> <u>≥ ¾ acre of vegetation converted to lawn or landscaped areas, or</u> <u>≥ 2.5 acres of native vegetation converted to pasture</u>	<u>≥ 5,000 sf new hard surface area, or</u> <u>≥ ¾ acre of vegetation converted to lawn or landscaped areas, or</u> <u>≥ 2.5 acres of native vegetation converted to pasture</u>  <u>AND</u>

		<u>New hard surface is <math>\geq</math> 50% of the existing hard surface within the project limits (road-related projects), or</u>  <u>Proposed improvements are <math>&gt;</math> 50% of the assessed value of the existing site improvements</u>
<u>Optional Guidance No. 2: Off Site Analysis and Mitigation</u>	<u><math>\geq</math> 5,000 square feet of hard surface area<sup>2</sup></u>	<u><math>\geq</math> 5,000 square feet of hard surface area<sup>2</sup></u>

1 – City-specific threshold for new development and redevelopment

2 – City-specific threshold for Off Site Analysis and Mitigation

2. Thresholds related to Minimum Requirement #5 include the following:

<u>Applicable Requirements</u>	<u>New Development and Redevelopment</u>
<u>List #1</u>	<u>Thresholds for Minimum Requirements #1-5 summarized in 15.20.060.B.1</u>
<u>List #2</u>	<u>Thresholds for Minimum Requirements #1-9 summarized in 15.20.060.B.1</u>
<u>LID Performance Standard</u>	<u>Optional, but can be used instead of List #1 or List #2 as specified above</u>
<u>BMP T5.13 (Post Construction Soil Quality and Depth)</u>	<u>Required for disturbed pervious areas</u>
<u>Direct Discharge to Puget Sound</u>	<u>Project discharging to Puget Sound are not required to evaluate bioretention, rain gardens, permeable pavement, and full dispersion if using List #1 or List #2. However, those project must implement the following, if feasible:</u> <ul style="list-style-type: none"> <li><u>• BMP T.510A, B, or C (Downspout Full Infiltration, Downspout Dispersion Systems, or Perforated Stub-out Connections)</u></li> <li><u>• BMP T5.11 (Concentrated Flow Dispersion)</u></li> <li><u>• BMP T5.12 (Sheet Flow Dispersion)</u></li> <li><u>• BMP T5.13 (Post Construction Soil Quality and Depth)</u></li> </ul>

3. Thresholds related to Minimum Requirement #6 include the following:

<u>Applicable Requirements</u>	<u>New Development and Redevelopment</u>
<u>Stormwater Treatment Facility</u>	<u><math>\geq</math> 5,000 sf pollution-generating hard surfaces (PGHS), or</u> <u><math>\geq</math> <math>\frac{3}{4}</math> acre of pollution-generating pervious surfaces (PGPS)</u>
<u>Oil Control</u>	<u>High-use sites</u>
<u>Phosphorus Treatment</u>	<u>Not applicable at this time</u>
<u>Enhanced Treatment</u>	<u>Industrial project sites, commercial project sites, multi-family project sites, and high AADT roads<sup>1</sup> that:</u>

	<p><u>1) Discharge directly to fresh waters or conveyance systems tributary to fresh waters designated for aquatic life use or that have an existing aquatic life use; or</u></p> <p><u>2) Use infiltration strictly for flow control – not treatment – and the discharge is within ¼ mile of a fresh water designated for aquatic life use or that has an existing aquatic life use.</u></p>
<u>Basic Treatment</u>	<p><u>All sites meeting the stormwater treatment facility thresholds listed above for PGHS or PGPS that:</u></p> <p><u>1) Discharge to the ground, but do not meet the soil suitability criteria for infiltration treatment or alternative pretreatment specified in the stormwater manual</u></p> <p><u>2) Are residential projects</u></p> <p><u>3) Discharge directly (or indirectly through a municipal separate storm sewer system) to Puget Sound</u></p> <p><u>4) Project sites that drain to fresh water that is not designated for aquatic life use, and does not have an existing aquatic life use; and project sites that drain to waters not tributary to waters designated for aquatic life use or that have an existing aquatic life use;</u></p> <p><u>5) Landscaped areas of industrial, commercial, and multi-family project sites, and parking lots of industrial and commercial project sites that do not involve pollution-generating sources (e.g., industrial activities, customer parking, storage of erodible or leachable material, wastes or chemicals) other than parking of employees' private vehicles.</u></p>

1 – High AADT roads are designated as ≥ 15,000 (fully controlled and partially controlled limited access highways) and ≥ 7,500 (all other roads).

4. Thresholds related to Minimum Requirement #7 include the following:

<u>Applicable Requirements</u>	<u>New Development and Redevelopment</u>
<u>Flow Control Facility</u>	<p><u>≥ 10,000 sf effective impervious surface, or</u></p> <p><u>≥ ¾ acre of vegetation converted to lawn or landscaped areas, or</u></p> <p><u>≥ 2.5 acres of native vegetation converted to pasture, or</u></p> <p><u>≥ 0.15 cfs increase in the 100-year flow frequency (using a 15-minute time step)</u></p>
<u>Direct Discharge Exemption</u>	<u>Flow control facilities are not required for direct discharges to Puget Sound.</u>

C. In Volume I, Section 2.4.1 of the manual, New Development is amended to read as follows:

All new development that shall be required to comply with Minimum Requirement No. 2. In addition, new development that exceeds certain thresholds shall be required to comply with additional Minimum Requirements as follows. The following new development shall comply with Minimum Requirements No. 1 through No. 5:

- 1. Creates or adds 800 square feet, or greater, of new, replaced, or new-plus-replaced impervious surface area, or

~~2. Has land disturbing activity of 7,000 square feet or greater.~~

~~The following new development shall comply with Minimum Requirements Nos. 1 through 10:~~

- ~~1. Creates or adds 5,000 square feet, or more, of new impervious surface area, or~~
- ~~2. Converts 3/4 acres, or more, of native vegetation to lawn or landscaped areas, or~~
- ~~3. Converts 2.5 acres, or more, of native vegetation to pasture.~~

~~D. In Volume I, Section 2.4.2 of the manual, Redevelopment, is amended to read as follows:~~

~~All redevelopment shall be required to comply with Minimum Requirement No. 2. In addition, all redevelopment that exceeds certain thresholds shall be required to comply with additional Minimum Requirements as follows:~~

~~The following redevelopment shall comply with Minimum Requirements No. 1 through No. 5 for the new and replaced impervious surfaces and the land disturbed:~~

- ~~1. The new, replaced, or total of new plus replaced impervious surfaces is 800 square feet or more, or~~
- ~~2. 7,000 square feet or more of land disturbing activities.~~

~~The following redevelopment shall comply with Minimum Requirements Nos. 1 through 10 for the new impervious surfaces and converted pervious areas:~~

- ~~1. Adds 5,000 square feet or more of new impervious surfaces, or~~
- ~~2. Converts 3/4 acres, or more, of native vegetation to lawn or landscaped areas, or~~
- ~~3. Converts 2.5 acres, or more, of native vegetation to pasture.~~

~~If the runoff from the new impervious surfaces and converted pervious surfaces is not separated from runoff from other surfaces on the project site, the stormwater treatment facilities must be sized for the entire flow that is directed to them.~~

~~The administrator may allow the Minimum Requirements to be met for an equivalent (flow and pollution characteristics) area within the same site. For public roads projects, the equivalent area does not have to be within the project limits, but must drain to the same receiving water.~~

#### ~~Additional Requirements for the Project Site~~

~~For road related projects, runoff from the replaced and new impervious surfaces (including pavement, shoulders, curbs, and sidewalks) shall meet all the Minimum Requirements if the new impervious surfaces total 5,000 square feet or more and total 50% or more of the existing impervious surfaces within the project limits. The project limits shall be defined by the length of the project and the width of the right of way.~~

~~Other types of redevelopment projects shall comply with all the Minimum Requirements for the new and replaced impervious surfaces if the total of new plus replaced impervious surfaces is 5,000 square feet or more, and the valuation of proposed improvements—including interior improvements—exceeds 50% of the assessed value of the existing site improvements.~~

~~E. In Volume I, Section 2.5.10 of the manual, Minimum Requirement No. 10: Operation and Maintenance, is amended to read as follows:~~

~~An operation and maintenance manual that is consistent with BIMC 15.21 and the provisions in Volume V of this manual shall be provided for all proposed private stormwater facilities and BMPs, and the party (or parties) responsible for maintenance and operation shall be identified. For private facilities, a copy of the manual shall be provided to the city prior to occupancy and a copy retained onsite or within reasonable access to the site, and shall be transferred with the property to subsequent owners. The copy of the manual shall be retained in the Public Works Department. A log of maintenance activity that indicates what actions were taken shall be kept and be available for inspection by the Administrator.~~

~~F. In Volume I, Section 2.6.1 of the manual, Financial Liability/Bonding, is not adopted.~~

~~G. In Volume I, Section 2.6.2 of the manual, Optional Guidance No. 2: Off Site Analysis and Mitigation, and Volume I, Section 3.1.3, Perform an Offsite Analysis, are adopted by reference and established for projects creating 5,000 square feet or more of impervious area.~~

~~H. In Volume I, Section 2.6.2 of the manual, Optional Guidance No. 2: Off Site Analysis and Mitigation Development, is amended to read as follows:~~

~~Development projects that discharge stormwater off site shall submit an off site analysis report that assesses the potential off site water quality, erosion, slope stability, and drainage impacts associated with the project and that proposes appropriate mitigation of those impacts. An initial qualitative analysis shall extend downstream for the entire flow path from the project site to the receiving water or up to one mile, whichever is less.~~

~~If a receiving water is within one quarter mile, the analysis shall extend within the receiving water to one quarter mile from the project site. The analysis shall extend one quarter mile beyond any improvements proposed as mitigation. The analysis must extend upstream to a point where any backwater effects created by the project cease. Upon review of the qualitative analysis, the local administrator may require that a quantitative analysis be performed.~~

~~The existing or potential impacts to be evaluated and mitigated shall include:~~

- ~~1. Conveyance system capacity problems;~~
- ~~2. Localized flooding;~~
- ~~3. Upland erosion impacts, including landslide hazards;~~
- ~~4. Stream channel erosion at the outfall location;~~
- ~~5. Violations of surface water quality standards as identified in a Basin Plan or a TMDL (Water Clean up Plan); or violations of ground water standards in a wellhead protection area.~~

~~Projects shall be required to initially submit, with the permit application, a qualitative analysis of each downstream system leaving a site. The analysis should accomplish four tasks:~~

~~Task 1—Define and map the study area.~~

~~Submission of a site map showing property lines; a topographic map (at a minimum a USGS 1:24000 Quadrangle Topographic map) showing site boundaries, study area boundaries, downstream flowpath, and potential/existing problems.~~

~~Task 2—Review all available information on the study area.~~

~~This should include all available basin plans, ground water management area plans, drainage studies, floodplain/floodway FEMA maps, wetlands inventory maps, Critical Areas maps, stream habitat reports, salmon distribution reports, etc.~~

~~Task 3—Field inspect the study area.~~

~~The design engineer should physically inspect the existing on and off site drainage systems of the study area for each discharge location for existing or potential problems and drainage features. An initial inspection and investigation should include:~~

- ~~1. Investigate problems reported or observed during the resource review;~~
- ~~2. Locate existing/potential constrictions or capacity deficiencies in the drainage system;~~
- ~~3. Identify existing/potential flooding problems;~~
- ~~4. Identify existing/potential overtopping, scouring, bank sloughing, or sedimentation;~~
- ~~5. Identify significant destruction of aquatic habitat (e.g., siltation, stream incision);~~
- ~~6. Collect qualitative data on features such as land use, impervious surface, topography, soils, presence of streams, wetlands;~~
- ~~7. Collect information on pipe sizes, channel characteristics, drainage structures;~~
- ~~8. Verify tributary drainage areas identified in Task 1;~~
- ~~9. Contact the local government office with drainage review authority, neighboring property owners, and residents about drainage problems; and~~
- ~~10. Note date and weather at time of inspection.~~

~~Task 4—Describe the drainage system, and its existing and predicted problems.~~

~~For each drainage system component (e.g., pipe, culvert, bridges, outfalls, ponds, vaults) the following should be covered in the analysis: location, physical description, problems, and field observations. All existing or potential problems (e.g., ponding water, erosion) identified in tasks 2 and 3 above should be described. The descriptions should be used to determine whether adequate mitigation can be identified, or whether more detailed quantitative analysis is necessary. The following information should be provided for each existing or potential problem:~~

- ~~1. Magnitude of or damage caused by the problem;~~
- ~~2. General frequency and duration;~~
- ~~3. Return frequency of storm or flow when the problem occurs (may require quantitative analysis);~~
- ~~4. Water elevation when the problem occurs;~~
- ~~5. Names and concerns of parties involved;~~
- ~~6. Current mitigation of the problem;~~
- ~~7. Possible cause of the problem; and~~
- ~~8. Whether the project is likely to aggravate the problem or create a new one.~~

~~Upon review of this analysis, the administrator may require mitigation measures deemed adequate for the problems, or a quantitative analysis, depending upon the presence of existing or predicted flooding, erosion, or water quality problems, and on the proposed design of the onsite drainage facilities. The analysis should repeat Tasks 3 and 4 above, using quantitative field data including profiles and cross sections.~~

~~The quantitative analysis should provide information on the severity and frequency of an existing problem or the likelihood of creating a new problem. It should evaluate proposed mitigation intended to avoid aggravation of the existing problem and to avoid creation of a new problem.~~

~~I. In Volume I, Section 2.7 of the stormwater manual, Adjustments, is not adopted.~~

~~J. In Volume III, Section 3.1.2 of the stormwater manual, Downspout Dispersion Systems, is modified for use with additional standard details as prescribed by the city.~~

~~K. In Volume III, Section 3.1.3 of the stormwater manual, Perforated Stub Out Connections, is modified for use with additional standard details as prescribed by the city.~~

~~L. In Volume III, Section 3.2 of the stormwater manual, Figure 3.12, Pond Signage, is amended to include the following language:~~

~~Developers shall provide the required signage for constructed ponds as a part of the project.~~

~~M. In Volume V, Section 4.3, Setbacks, Slopes and Embankments, is amended to include the following language:~~

~~All stormwater facilities and infiltration systems constructed within 200 feet of a geologically hazardous area shall have the concurrence of a Geotechnical Engineer.~~

~~N. In Volume V, Section 5.3.1 of the manual, BMP T 5.10 Downspout Dispersion, is modified for use with additional standard details as prescribed by the city.~~

~~O. In Volume V, Section 5.3.2 of the manual, BMP T 5.21 Better Site Design: Build Narrower Streets, is amended to include the following language:~~

~~Streets and roadways must, however, comply with city of Bainbridge Island Design and Construction Standards and Specifications unless an exception is granted in writing by the administrator.~~

~~P. In Volume V, Section 5.3.3 of the manual, BMP T 5.30 Full Dispersion, is modified for use with additional standard details as prescribed by the city.~~

~~Q. In Volume V of the manual, Chapter 12, Emerging Technologies, is not adopted.~~

~~R. In Volume I, Section 2.5.2 of the manual, Minimum Requirement No. 2: Construction Storm Water Pollution Prevention (SWPPP), the following language replaces or amends the language found in Section 2.5.2 of the manual:~~

~~All new development and redevelopment projects are responsible for preventing erosion and discharge of sediment and other pollutants into receiving waters.~~

~~Sediment and erosion control BMPs shall be consistent with the BMPs contained in Chapters 3 and 4 of Volume II of the manual.~~

~~The SWPPP shall include a narrative and drawings. All BMP's shall be clearly referenced in the narrative and marked on the drawings. The SWPPP narrative shall include documentation to explain and justify the pollution prevention decisions made for the project.~~

~~1. Volume I, Section 2.5.2 of the manual, Minimum Requirement No. 2: Construction Storm Water Pollution Prevention Plan (SWPPP) Elements. The following language replaces or amends the language found in the Elements Section 2.5.2 of the manual:~~

~~Element 2.c. Wheel wash or tire baths shall be located on site, if the stabilized construction entrance is not effective in preventing sediment from being tracked onto public roads.~~

~~Element 6.c. Temporary pipe slope drains shall handle the expected peak 10 minute flow velocity from a type 1A, 10 year, 24 hour frequency storm for the developed condition.~~

~~The hydrologic analysis shall use the existing land cover condition for predicting flow rates from tributary areas outside the project limits. For tributary areas on the project site, the analysis shall use the temporary or permanent project land cover condition, whichever will produce the highest flow rates. If using the Western Washington Hydrology model to predict flows, bare soil areas should be modeled as “landscaped area.”~~

#### ~~Element 7. Protect Drain Inlets~~

~~All sediment and street wash water shall not be allowed to enter storm drains without prior and adequate treatment unless treatment is provided before the storm drain discharges to waters of the State.~~

#### ~~Element 9. Control Pollutants~~

~~Permittees shall require construction site operators obtain written approval from the Department prior to using chemical treatment other than CO<sub>2</sub> or dry ice to adjust pH.~~

~~S. Volume I, Section 2.5.6 of the manual, Minimum Requirement No. 6 Runoff Treatment. The following language replaces the language found in Section 2.5.6 of the manual:~~

#### ~~Project Thresholds~~

~~The following require construction of stormwater treatment facilities (see Table below):~~

- ~~• Projects in which the total of effective, pollution-generating impervious surface (PGIS) is 5,000 square feet or more in a threshold discharge area of the project, or~~
- ~~• Projects in which the total of pollution-generating pervious surfaces (PGPS) is three-quarters (3/4) of an acre or more in a threshold discharge area, and from which there is a surface discharge in a natural or man-made conveyance system from the site.~~

#### ~~Treatment Type Thresholds~~

##### ~~1. Oil Control:~~

~~Treatment to achieve Oil Control applies to projects that have “high use sites.” High use sites are those that typically generate high concentrations of oil due to high traffic turnover or the frequent transfer of oil. High use sites include:~~

- ~~a. An area of a commercial or industrial site subject to an expected average daily traffic (ADT) count equal to or greater than 100 vehicles per 1,000 square feet of gross building area;~~
- ~~b. An area of a commercial or industrial site subject to petroleum storage and transfer in excess of 1,500 gallons per year, not including routinely delivered heating oil;~~

~~c. An area of a commercial or industrial site subject to parking, storage or maintenance of 25 or more vehicles that are over 10 tons gross weight (trucks, buses, trains, heavy equipment, etc.);~~

~~Treatment  
Requirements  
by Threshold  
Discharge  
Area~~

<del>&lt;3/4</del>	<del>&gt;3/4</del>	<del>&lt;</del>	<del>&gt;</del>
<del>acres</del>	<del>acres</del>	<del>5,000</del>	<del>5,000</del>
<del>of</del>	<del>of</del>	<del>sf of</del>	<del>sf of</del>
<del>PGPS</del>	<del>PGPS</del>	<del>PGIS</del>	<del>PGIS</del>

~~Treatment  
Facilities~~

~~X~~

~~X~~

~~Onsite  
Stormwater  
BMPs~~

~~X~~

~~X~~

~~X~~

~~X~~

~~PGPS =  
pollution  
generating  
pervious  
surfaces~~

~~PGIS =  
pollution  
generating  
impervious  
surfaces~~

~~sf = square  
feet~~

~~d. A road intersection with a measured ADT count of 25,000 vehicles or more on the main roadway and 15,000 vehicles or more on any intersecting roadway, excluding projects proposing primarily pedestrian or bicycle use improvements.~~

~~2. Phosphorus Treatment:~~

~~The requirement to provide phosphorus control is determined by the local government with jurisdiction (e.g., through a lake management plan), or the Department of Ecology (e.g., through a waste load allocation). The local government may have developed a management plan and implementing ordinances or regulations for control of phosphorus from new/redevelopment for the receiving water(s) of the stormwater drainage. The local government can use the following sources of information for pursuing plans and implementing ordinances and/or regulations:~~

~~a. Those waterbodies reported under section 305(b) of the Clean Water Act, and designated as not supporting beneficial uses due to phosphorus;~~

~~b. Those listed in Washington State's Nonpoint Source Assessment required under section 319(a) of the Clean Water Act due to nutrients.~~

~~3. Enhanced Treatment:~~

~~Enhanced treatment for reduction in dissolved metals is required for the following project sites that discharge to fish bearing streams, lakes, or to waters or conveyance systems tributary to fish bearing streams or lakes:~~

~~Industrial project sites,~~

~~Commercial project sites,~~

~~Multi family project sites, and~~

~~High AADT roads as follows:~~

~~Within Urban Growth Management Areas:~~

- ~~• Fully controlled and partially controlled limited access highways with Annual Average Daily Traffic (AADT) counts of 15,000 or more~~
- ~~• All other roads with an AADT of 7,500 or greater~~

~~Outside of Urban Growth Management Areas:~~

- ~~• Roads with an AADT of 15,000 or greater unless discharging to a 4th Strahler order stream or larger;~~
- ~~• Roads with an AADT of 30,000 or greater if discharging to a 4th Strahler order stream or larger (as determined using 1:24,000 scale maps to delineate stream order).~~

~~However, such sites listed above that discharge directly (or, indirectly through a municipal storm sewer system) to Basic Treatment Receiving Waters (Appendix I C of the manual), and areas of the above listed project sites that are identified as subject to Basic Treatment requirements, are also not subject to Enhanced Treatment requirements. For developments with a mix of land use types, the Enhanced Treatment requirement shall apply when the runoff from the areas subject to the Enhanced Treatment requirement comprise 50% or more of the total runoff within a threshold discharge area.~~

~~4. Basic Treatment:~~

~~Basic Treatment generally applies to:~~

- ~~• Project sites that discharge to the ground, UNLESS:~~
  - ~~1) The soil suitability criteria for infiltration treatment are met; (see Chapter 3 of Volume III of the manual for soil suitability criteria) or~~
  - ~~2) The project uses infiltration strictly for flow control—not treatment—and the discharge is within 1/4 mile of a phosphorus sensitive lake (use a Phosphorus Treatment facility), or within 1/4 mile of a fish bearing stream, or a lake (use an Enhanced Treatment facility).~~
- ~~• Residential projects not otherwise needing phosphorus control as designated by USEPA, the Department of Ecology, or by the Permittee; and~~
- ~~• Project sites discharging directly to salt waters, river segments, and lakes listed in Appendix I C of the manual; and~~

~~• Project sites that drain to streams that are not fish-bearing, or to waters not tributary to fish-bearing streams;~~

~~• Landscaped areas of industrial, commercial, and multi-family project sites, and parking lots of industrial and commercial project sites that do not involve pollution-generating sources (e.g., industrial activities, customer parking, storage of erodible or leachable material, wastes or chemicals) other than parking of employees' private vehicles. For developments with a mix of land use types, the Basic Treatment requirement shall apply when the runoff from the areas subject to the Basic Treatment requirement comprise 50% or more of the total runoff within a threshold-discharge area.~~

#### ~~Treatment Facility Sizing~~

~~Water Quality Design Storm Volume: The volume of runoff predicted from a 24-hour storm with a 6-month return frequency (a.k.a., 6-month, 24-hour storm). Wetpool facilities are sized based upon the volume of runoff predicted through use of the Natural Resource Conservation Service curve number equations in Chapter 2 of Volume III of the manual, for the 6-month, 24-hour storm. Alternatively, the 91st percentile, 24-hour runoff volume indicated by an approved continuous runoff model may be used.~~

#### ~~Water Quality Design Flow Rate~~

##### ~~1. Preceding Detention Facilities or when Detention Facilities are not required:~~

~~The flow rate at or below which 91% of the runoff volume, as estimated by an approved continuous runoff model, will be treated. Design criteria for treatment facilities are assigned to achieve the applicable performance goal at the water quality design flow rate (e.g., 80% TSS removal).~~

##### ~~2. Downstream of Detention Facilities:~~

~~The water quality design flow rate must be the full 2-year release rate from the detention facility. Alternative methods may be used if they identify volumes and flow rates that are at least equivalent. That portion of any development project in which the above PGIS or PGPS thresholds are not exceeded in a threshold-discharge area shall apply On-site Storm Water Management BMPs in accordance with Minimum Requirement #5.~~

#### ~~Treatment Facility Selection, Design, and Maintenance~~

~~Stormwater treatment facilities shall be:~~

- ~~• Selected in accordance with the process identified in Chapter 4 of Volume I of the manual;~~
- ~~• Designed in accordance with the design criteria in Volume V of the manual, and~~
- ~~• Maintained in accordance with the maintenance schedule in Volume V of the manual.~~

#### ~~Additional Requirements~~

~~The discharge of untreated stormwater from pollution-generating impervious surfaces to ground water is not authorized, except for the discharge achieved by infiltration or dispersion of runoff from residential sites through use of On-site Stormwater Management BMPs.~~

(Ord. 2009-13 § 5, 2009: Ord. 2005-10 § 5, 2005: Ord. 2001-49 § 2, 2001; Ord. 98-31 § 1, 1999)

**15.20.070 Administration.**

A. Administrator. The public works director or a designee shall administer this chapter and shall be referred to as the administrator. The administrator shall have the authority to develop and implement administrative procedures to administer and enforce this chapter.

B. Review and Approval. The administrator may approve, conditionally approve or deny an application for activities regulated by this chapter.

C. Enforcement Authority. The administrator shall enforce this chapter.

D. Inspection. All activities regulated by this chapter shall be inspected by the administrator. The administrator shall inspect projects at various stages of the work requiring approval to determine that adequate control is being exercised. Stages of work requiring inspection include, but are not limited to,

1. Prior to clearing and construction (preconstruction) for all sites that have a high potential for sediment transport to ensure that clearing limits, sensitive areas and their buffers, and trees that are to be preserved have been clearly marked

2. During construction to verify proper installation and maintenance of erosion and sediment control BMPs, maintenance of clearing limits, and protection of trees that are to be preserved

3. Every 6 months during construction for new residential development until 90% of the lots are constructed (or when construction has stopped and the site is fully stabilized) to identify maintenance needs for permanent stormwater facilities

4. Upon completion of construction and prior to final approval or occupancy to ensure proper installation of, ~~land-disturbing activities, installation of utilities, permanent storm water~~ stormwater control facilities, landscaping, retaining walls and completion of project.

5. Ongoing annual inspections of permanent stormwater facilities designed to meet Minimum Requirement #6 (Runoff Treatment) and/or Minimum Requirement #7 (Flow Control) per BIMC 15.21.

When required by the administrator, a special inspection and/or testing shall be performed. (Ord. 2009-13 § 6, 2009: Ord. 2005-10 § 6, 2005: Ord. 98-31 § 1, 1999)

**15.20.080 Enforcement.**

A. Failure to Comply. It is unlawful for any person to violate any provision or fail to comply with any of the requirements of this chapter.

B. Emergency Access and Reparation. In the event the violation constitutes an immediate danger to public health or safety, the administrator is authorized to enter upon the subject private property, without giving prior notice, to take any and all measures necessary to abate the violation and/or restore the property. Any expense related to such remediation undertaken by the city shall be fully reimbursed by the property owner and/or responsible party. Any relief obtained under this section shall not prevent the city from seeking further relief or applying other penalties as provided in this chapter.

C. Civil Infraction. Except as provided in subsection D of this section, conduct made unlawful by this chapter shall constitute a civil infraction and is subject to enforcement and fines as provided in BIMC 1.26.035. A civil infraction under this section shall be processed in the manner set forth in Chapter 1.26 BIMC.

D. Misdemeanor. Any person who again violates this chapter within 12 months after having been found by the Bainbridge Island municipal court to be in violation of this chapter, commits a misdemeanor and any person who is convicted thereof shall be punished as provided in BIMC 1.24.010.A.

E. Civil Penalty. In addition to any civil infraction fine, criminal penalty, and/or other available sanction or remedial procedure, any person engaging in conduct made unlawful by this chapter shall be subject to a cumulative civil penalty in the amount of \$1,000 per day for each violation from the date set for compliance until the date of compliance. Any such civil penalty shall be collected in accordance with BIMC 1.26.090.

F. Additional Remedies.

1. In addition to any other remedy provided by this chapter or under the Bainbridge Island Municipal Code, the city may initiate injunction or abatement proceedings or any other appropriate action in courts against any person who violates or fails to comply with any provision of this chapter to prevent, enjoin, abate, and/or terminate violations of this chapter and/or to restore a condition which existed prior to the violation. In any such proceeding, the person violating and/or failing to comply with any provisions of this chapter shall be liable for the costs and reasonable attorneys' fees incurred by the city in bringing, maintaining and/or prosecuting such action.

2. Any person who violates any provision of this chapter may also be in violation of the Federal Clean Water Act, NPDES Phase II permit, and/or Chapter 90.48 RCW and may be subject to sanctions including civil and criminal penalties. Any enforcement action authorized under this chapter shall also include written notice to the violator of such potential liability. (Ord. 2009-13 § 7, 2009: Ord. 2005-10 § 7, 2005: Ord. 98-31 § 1, 1999)

**15.20.090 Exceptions and appeals.**

*Repealed by Ord. 2009-13. (Ord. 2005-10 § 8, 2005: Ord. 2003-25 § 6, 2003; Ord. 98-31 § 1, 1999)*

**15.20.100 Severability.**

*Repealed by Ord. 2003-24. (Ord. 98-31 § 1, 1999)*

## Chapter 15.21

### ~~STORM WATER~~STORMWATER FACILITIES MAINTENANCE PROGRAM

Sections:

- 15.21.010 Purpose.
- 15.21.020 Definitions.
- 15.21.030 General provisions.
- 15.21.040 General requirements.
- 15.21.050 Administration.
- 15.21.060 Inspection program.
- 15.21.070 Enforcement.
- 15.21.080 Repealed.

#### **15.21.010 Purpose.**

The purpose of this chapter is to ensure maintenance of all ~~storm water~~stormwater facilities within the city and to set minimum standards for the inspection and maintenance of ~~storm water~~stormwater facilities. The provisions of this chapter are intended to:

- A. Provide for inspection and maintenance of ~~storm water~~stormwater facilities in the city to provide for effective and functional ~~storm water~~stormwater drainage systems.
- B. Authorize the city, through the public works department, to require that ~~storm water~~stormwater facilities be operated, maintained and repaired in conformance with this chapter.
- C. Establish the minimum level of compliance.
- D. Guide and advise all who conduct inspection and maintenance of ~~storm water~~stormwater facilities. (Ord. 98-42 § 1, 1999)

#### **15.21.020 Definitions.**

For the purposes of this chapter, the following definitions shall apply:

A. “Best management practice (BMP),” means physical, structural, and/or managerial practices that, when used ~~in-~~the singular~~singly~~ or in combination, prevent ~~and/or~~ reduce ~~pollution of water, and have been approved by the-~~Washington State Department of Ecology. BMPs are listed and described in the storm water management manual~~the~~release of pollutants and other adverse impacts to waters of Washington State.

B. *Repealed by Ord. 2003-22.*

C. “Property owner” means any person having title to and/or responsibility for, a building or property, including a lessee, guardian, receiver or trustee, and the owner’s duly authorized agent.

~~DC.~~ “Storm water~~Stormwater~~” means that portion of precipitation that does not naturally percolate into the ground or evaporate, but flows via overland, interflow, channels or pipes into a defined surface water channel, or a constructed infiltration facility.

~~ED.~~ “Storm water~~Stormwater~~ drainage system” means constructed and natural features which function together as a system to collect, convey, channel, hold, inhibit, retain, detain, infiltrate, divert, treat or filter ~~storm-~~water~~stormwater~~.

~~FE.~~ “Storm water~~Stormwater~~ facility” means a constructed component of a ~~storm water~~stormwater drainage system, designed or constructed to perform a particular function, or multiple functions, including but not limited to, pipes, swales, ditches, culverts, street gutters, detention basins, retention basins, constructed wetlands, infiltration devices, catchbasins, oil/water separators, sediment basins and modular pavement.

~~GF. “Storm water~~Stormwater management manual (stormwater manual)” means the manual adopted in BIMC 15.20~~by reference and prepared by the Washington State Department of Ecology which contains BMPs to prevent or reduce pollution.~~

H. “Waters of the State” includes those waters as defined as "waters of the United States" in 40 CFR Subpart 122.2 within the geographic boundaries of Washington State and "waters of the state" as defined in chapter 90.48 RCW which includes lakes, rivers, ponds, streams, inland waters, underground waters, salt waters and all other surface waters and water courses within the jurisdiction of the State of Washington.

(Ord. 2003-22 § 23, 2003; Ord. 98-42 § 1, 1999)

#### **15.21.040 General requirements.**

A. Maintenance Required. All ~~storm water~~stormwater facilities shall be maintained in accordance with this chapter and the ~~storm water~~stormwater management manual. Systematic, routine preventive maintenance is preferred.

B. Minimum Standards. The following are the minimum standards for the inspection and maintenance of ~~storm-~~water~~stormwater~~ facilities:

1. Stormwater fFacilities designed to meet Minimum Requirements #6 and/or #7 in the stormwater manual shall be inspected annually by a qualified third party contractor and cleared of debris, sediment and vegetation when they effect the functioning and/or design capacity of the facility.
2. Property owners shall be responsible for clearing debris, sediment and vegetation from their stormwater facility when they affect the functioning and/or design capacity.
3. Grassy Biofiltration swales ~~and other biofilters~~ shall be inspected monthly and mowed or replanted as necessary. Clippings are to be removed and properly disposed of. Additional maintenance criteria are included in the stormwater manual.
4. Bioretention/rain garden routine maintenance shall include weeding, removal of noxious weeds, clearing vegetation within 1 foot of inlets/outlets, replenishment of mulch, and irrigation during the summer months and as needed during prolonged dry periods. Additional maintenance criteria are included in the stormwater manual.
5. Permeable pavement routine maintenance shall include cleaning surface debris at a minimum of once or twice per year. Additional maintenance criteria are included in the stormwater manual.
6. Where lack of maintenance is causing or contributing to a water quality problem, immediate action shall be taken to correct the problem. Within one month, after initial recognition of problem, the city inspector or designee shall revisit the facility to assure that the problem has been rectified.

C. Disposal of Waste from Maintenance Activities. Disposal of waste from maintenance activities shall be conducted in accordance with the minimum Functional Standards for Solid Waste Handling, Chapter 173-304 WAC, guidelines for disposal of waste materials from ~~storm water~~stormwater maintenance activities, and where appropriate, the Dangerous Waste Regulations, Chapter 173-303 WAC.

D. Compliance. Property owners are responsible for the inspection, maintenance, operation and/or repair of ~~storm-~~water~~stormwater~~ drainage systems and BMPs located on their property. Property owners shall inspect, maintain, operate and repair these facilities in compliance with the requirements of this chapter and the storm water-management-stormwater manual. Property owners shall hire, at the owner’s expense, a qualified third party contractor to conduct inspections and submit annual inspection reports to the City for any stormwater facilities designed to meet Minimum Requirements #6 and/or #7. (Ord. 98-42 § 1, 1999)

#### **15.21.050 Administration.**

A. Director. The public works director, and/or designee, shall administer this chapter and shall be referred to as the director. The director shall have the authority to develop and implement administrative procedures to administer and enforce this chapter.

B. Inspection Authority. The director is directed and authorized to develop an inspection program for ~~storm-~~waterstormwater facilities in the city.

C. Enforcement Authority. The director shall enforce this chapter. (Ord. 98-42 § 1, 1999)

**15.21.060 Inspection program.**

A. Inspection. Whenever implementing the provisions of the inspection program, or whenever there is cause to believe that a violation of this chapter has been or is being committed, the inspector is authorized to inspect all ~~storm-~~waterstormwater drainage systems within the city in accordance with Chapter 1.16 BIMC.

B. Procedures. The method of entry onto property to perform duties imposed by this chapter shall be in accordance with Chapter 1.16 BIMC.

C. Inspection Schedule. The director shall establish a master inspection and maintenance schedule to inspect appropriate ~~storm-~~waterstormwater facilities that are not owned or operated by the city. Inspections shall be annual. Critical ~~storm-~~waterstormwater facilities may require a more frequent inspection schedule.

D. Inspection and Maintenance Records. As existing ~~storm-~~waterstormwater facilities are encountered, they shall be added to the master inspection and maintenance schedule. Records of new ~~storm-~~waterstormwater facilities shall include the following:

1. As-built plans and locations;
2. Findings of fact from any exemption granted by the local government;
3. Operation and maintenance requirements and records of inspection maintenance actions and frequencies;
4. Declaration of covenant associated with maintenance and operation of storm drainage facilities. See “Exhibit A” following this chapter; and
5. Engineering reports, as appropriate.

E. Reporting Requirements. The inspector shall report annually to the director of public works about the status of the ~~storm-~~waterstormwater facilities inspections. The annual report may include, but not be limited to, the proportion of the components found in and out of compliance, the need to upgrade components, enforcement actions taken, compliance with the inspection schedule, the resources needed to comply with the schedule, and comparisons with previous years. (Ord. 2003-28 § 3, 2003; Ord. 98-42 § 1, 1999)

**15.21.070 Enforcement.**

A. General. Enforcement action, as provided by Chapter 1.26 BIMC, shall be taken whenever a person has violated any provision of this chapter.

B. Orders. The director or designee shall have the authority to issue to an owner or person in control of a ~~storm-~~waterstormwater facility deemed to be in violation of this chapter, an order to maintain or repair a component of a ~~storm-~~waterstormwater facility or BMP to bring it in compliance with this chapter, the ~~storm-~~waterstormwater ~~management~~ manual and/or city regulations. The order shall include:

1. A description of the specific nature, extent and time of the violation and the damage or potential damage that reasonably might occur;
2. A notice that the violation or potential violation cease and desist and, in appropriate cases, the specific corrective actions to be taken;
3. A reasonable time to comply, depending on the circumstances.

C. Civil Penalty. A person who fails to comply with the requirements of this chapter or who fails to conform to the terms of an approval or order issued shall be subject to civil penalties as provided for in the BIMC 1.26.090. (Ord. 98-42 § 1, 1999)

**15.21.080 Severability.**

*Repealed by Ord. 2003-24. (Ord. 98-42 § 1, 1999)*

EXHIBIT A

DECLARATION OF COVENANT ASSOCIATED WITH MAINTENANCE  
AND OPERATION OF STORM DRAINAGE FACILITIES

Grantor: \_\_\_\_\_ Additional Grantor: \_\_\_\_\_

Grantee: \_\_\_\_\_ Additional Grantee: \_\_\_\_\_

Legal Description \_\_ 1/4 \_\_ 1/4sec \_\_ T \_\_ R \_\_ W.M. Additional Legal:  
\_\_\_\_\_

Assessor's Tax Parcel #: \_\_\_\_\_ Additional #: \_\_\_\_\_

Reference Auditor File #: \_\_\_\_\_ Additional #: \_\_\_\_\_

Whereas the city of Bainbridge Island, a political subdivision of the State of Washington, has rights under city ordinances, codes, and Washington State statutes to regulate ~~storm-~~  
~~water~~stormwater drainage, and The City of Bainbridge Island, Department of Public Works has issued a permit number \_\_\_\_\_ for the development known as \_\_\_\_\_ which contains on-site stormwater facilities.

The Grantors, hereinafter known as the owner(s) of the real property situated in the City of Bainbridge Island, State of Washington, and legally described as follows:

The owner(s), their heirs, successors or assigns, hereby covenant and agree that:

1. The City of Bainbridge Island, or its designee, shall have the right to ingress and egress over the above described property for the purpose of inspecting, sampling and monitoring stormwater facility components and discharges.
2. If, at any time, the City of Bainbridge Island reasonably determines that maintenance or repair work is required to be done to the existing and accepted stormwater facilities installed on the property described above (which will mean repair or clean out existing facilities only to the same standards as originally installed and accepted), the COBI City Engineer or his/her designee shall give the Owner(s) seven (7) days' notice that the City intends to perform such maintenance or repair work, or to have them performed by others. If the owner(s) have not completed or are not diligently pursuing the maintenance or repair work to the facilities and it becomes necessary for the City to perform the work, the Owner(s) will assume responsibility for the cost of such maintenance or repair work and will reimburse the City within thirty (30) working days of receipt of the invoice for any such work performed. Overdue payments will require payment of interest at the current legal rate for liquidated judgments, and any costs or fees incurred by the City, should any legal action be required to collect such payments, will be borne by the parties responsible for said reimbursements.
3. If, at any time, the City reasonably determines that the existing and accepted stormwater facilities installed on the property described above poses a hazard to life and limb, or endangers property, or adversely affects the safety and operations of public way, due to failure, damage or non-maintenance, and that the situation is so adverse as to preclude written notice to the Owner(s), the City Engineer may take the measures necessary to eliminate the hazardous situation (which will mean repair or clean out of the existing facilities only to the same standards as originally installed and accepted), provided the Director, or his/her designee, has first made a reasonable effort to locate said Owner(s) before acting. The Owner(s) will assume responsibility for the cost of such maintenance or repair work and will reimburse the City within thirty (30) days of receipt of the invoice for any such work

performed. Overdue payments will require payment of interest at the current legal rate for liquidated judgments, and any costs or fees incurred by the City, should any legal action be required to collect such payments, will be borne by the parties responsible for said reimbursements.

4. The Owner(s) will keep the City informed at all times as to the name, address and telephone number of the contact person responsible for the performance of maintenance or repair work to the storm drainage facilities.

5. The Owner(s) agree to hold harmless and indemnify the City or its designee from any and all claims arising from any activity the City undertakes on the property described above if it becomes necessary for the City to conduct maintenance or repair work.

These covenants are intended to protect the value and desirability of the real property described above, and to benefit all the citizens of the City of Bainbridge Island. They shall run with the land and be binding on all parties having or acquiring from the Owner(s), their heirs, successors or assigns, any right, title or interest in the property or any part thereof. They shall inure to the benefit of each present or future successor in interest of said property or any part thereof, or interest therein, and to the benefit of all the citizens of the City of Bainbridge Island.

\_\_\_\_\_

Owner    Date

\_\_\_\_\_

Owner    Date